



# Huntsville, Alabama

305 Fountain Circle  
Huntsville, AL 35801

## Cover Memo

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**Meeting Type:** City Council Regular Meeting **Meeting Date:** 12/18/2025

**File ID:** TMP-6379

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**Department:** Planning

**Subject:**

**Type of Action:** Approval/Action

Public hearing on the rezoning of 380.93 acres of land lying on the south side of AL Hwy 20 and east of I-65 from Residence 2-B District and Highway Business C-4 District to Planned Development - Housing District, LUI 56; and the consideration of an Ordinance pertaining to the same. (Set on November 20, 2025, Regular Council Meeting)

Ordinance No. 25-879

**Does this item need to be published?** Yes

If yes, please list preferred date(s) of publication: Resolution: November 26, December 3 & 10, 2025/  
Ordinance: December 24, 2025

**Finance Information:**

**Account Number:** n/a

**City Cost Amount:** \$ 0

**Total Cost:** \$ 0

**Special Circumstances:**

**Grant Funded:** \$ 0

**Grant Title - CFDA or granting Agency:** n/a

**Resolution #:** n/a

**Location:**

**Address:**

**District:** District 1 ☐ District 2 ☐ District 3 ☐ District 4 ☐ District 5 ☒

**Additional Comments:** Recommended by Planning Commission

## **ORDINANCE NO. 25-**

### **AN ORDINANCE TO AMEND THE ZONING ORDINANCES OF THE CITY OF HUNTSVILLE, ALABAMA**

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**WHEREAS**, an application for a Planned Development amendment to the Zoning Ordinance of the City of Huntsville, Alabama (“the Zoning Ordinance”), has been submitted for the Westmoore Landing development pursuant to Article 30 of the Zoning Ordinance; and

**WHEREAS**, pursuant to Section 30.5.5 of the Zoning Ordinance, the Planning Department has prepared a report containing findings and making recommendations; and

**WHEREAS**, the Planning Commission has considered said application and report and recommended that the proposed Planned Development amendment be approved as proposed; and

**WHEREAS**, said report has been provided to and considered by the City Council, which includes such findings and recommendations of the Planning Department and Planning Commission; and

**WHEREAS**, based on the report, recommendations, and documentation in support of the same, the City Council is of the opinion that that the proposed Planned Development amendment should be granted in accord with PD and other regulations applicable including certain modifications of PD and other applicable regulations as set out in the report and provided for in 30.5.5(5) of the Zoning Ordinance.

**NOW, THEREFORE**, the foregoing premises considered, the public welfare requiring it, and under authority granted by Section 11-52-78 of the Code of Ala. 1975, **BE IT ORDAINED** by the City Council for the City of Huntsville, Alabama, as follows:

1. That the findings of the Report on Westmoore Landing Planned Development-Housing Zoning, and documentation in support of the same to the extent not inconsistent with such report, are adopted as the findings of the City Council as if set forth fully herein and a copy of such report is attached hereto as Exhibit “A” and incorporated herein by reference as if set out herein fully.

2. That the preliminary concept plan as it may have been changed during earlier procedures, a copy which is attached hereto as Exhibit “B” and incorporated herein by reference as if set out herein fully, is approved, and by such approval shall be binding in determinations concerning final development plans in accordance with Section 30.5.6 of the Zoning Ordinance. Consistent with such section, the development shall be required to be in accord with final development plans meeting the requirements of the Planned Development and other regulations, as supplemented or modified by the report approved by this amendment to the Zoning Ordinance and the development shall further conform to any time or priority limitations established by the report on beginning and completion of the development as a whole, or in specified stages.

3. That the findings, contents, conditions, waivers, and/or modifications as set out in the Report on Westmoore Landing Planned Development-Housing Zoning and documentation submitted in support of such report, including application for Planned Development amendment; the Westmoore Landing Planned Development Rezoning Submittal including the General Development Plan; Open Space Plan; the Typical Lot Block Patterns; the Illustrations of Typical Building Designs; Landscaping Plan; the Boundary and Topographic Survey, support this amendatory action, including the grant of the Planned Development amendment in accord with PD and other regulations applicable including those certain modifications of PD and other applicable regulations as set out in the report and provided for in 30.5.5(5) of the Zoning Ordinance.

4. That the Council has made careful examination and consideration of the public purposes furthered by the Planned Development regulations and other regulations as well as the proposed modifications from such regulations in the Report on Westmoore Landing Planned Development-Housing Zoning and hereby finds and determines that the regulations approved for modification by this Ordinance either: 1) do not serve public purposes to a degree at least equivalent to such general zoning, subdivision, or other regulations or requirements where there are conflicts between the special PD regulations in Articles 30 and 31 and the general zoning, subdivision or other regulations or requirements, or 2) serve the public purposes furthered by the underlying regulation to an equal or greater degree where the actions, designs, or solutions proposed by the Report on Westmoore Landing Planned Development-Housing Zoning are not literally in accord with otherwise applicable PD or general regulations, as set forth in the Report on Westmoore Landing Planned Development-Housing Zoning and as required by Section 30.2 of the Zoning Ordinance in each particular case.

5. That pursuant to Section 31.2.3 of the Zoning Ordinance the scale, character, and location of the Westmoore Landing Planned Development as set forth in the Report on Westmoore Landing Planned Development-Housing Zoning, and documentation submitted in support of such report to the extent not inconsistent with the same, justify additional principal and accessory structures and uses as described therein and are hereby specifically authorized, subject to the requirements and limitations set forth in such Section.

6. That the following area, lying on the south side of AL Hwy 20 and east of I-65, which area is now shown and classified on the Official Zoning Maps of the City of Huntsville, Alabama, Residence 2-B and Highway Business C-4 Districts, shall be shown and classified on the Official Zoning Maps of the City of Huntsville, Alabama, as Planned Development - Housing District, LUI 56, in the Zoning Ordinances of the City of Huntsville, Alabama, said property being particularly described as follows:

All that part of Sections 07 and 18, Township 05 South, Range 03 West of the Huntsville Meridian, Limestone County, Alabama more particularly described as beginning at a point located at the Northwest Corner of said Section 07; said point is further described as the Point of Beginning; thence from the Point of Beginning, South 88 Degrees 03 Minutes 53 Seconds East 2066.50 feet to a point; thence South 04 Degrees 11 Minutes 10 Seconds West 220.41 feet to a point; thence South 00 Degrees 33 Minutes 02 Seconds West 290.67 feet to a point; thence South 03

Degrees 47 Minutes 43 Seconds West 168.97 feet to a point; thence South 23 Degrees 20 Minutes 55 Seconds East 97.07 feet to a point; thence South 29 Degrees 06 Minutes 18 Seconds East 16.20 feet to a point; thence South 30 Degrees 44 Minutes 58 Seconds East 16.20 feet to a point; thence South 28 Degrees 04 Minutes 52 Seconds East 293.31 feet to a point; thence South 87 Degrees 23 Minutes 31 Seconds East 239.89 feet to a point; thence South 00 Degrees 18 Minutes 22 Seconds West 268.82 feet to a point; thence South 89 Degrees 08 Minutes 40 Seconds East 230.48 feet to a point; thence South 24 Degrees 22 Minutes 09 Seconds East 1247.71 feet to a point; thence South 01 Degree 37 Minutes 21 Seconds West 521.37 feet to a point; thence North 88 Degrees 52 Minutes 17 Seconds West 527.77 feet to a point; thence South 02 Degrees 30 Minutes 36 Seconds West 51.62 feet to a point; thence South 01 Degree 58 Minutes 33 Seconds West 51.62 feet to a point; thence South 01 Degree 26 Minutes 25 Seconds West 51.62 feet to a point; thence South 00 Degrees 54 Minutes 20 Seconds West 51.62 feet to a point; thence South 00 Degrees 22 Minutes 15 Seconds West 51.62 feet to a point; thence South 00 Degrees 09 Minutes 50 Seconds East 51.62 feet to a point; thence South 04 Degrees 38 Minutes 29 Seconds East 15.44 feet to a point; thence South 02 Degrees 59 Minutes 21 Seconds East 15.44 feet to a point; thence South 01 Degree 20 Minutes 17 Seconds East 15.44 feet to a point; thence South 00 Degrees 18 Minutes 51 Seconds West 15.44 feet to a point; thence South 01 Degree 57 Minutes 51 Seconds West 15.44 feet to a point; thence South 03 Degrees 37 Minutes 02 Seconds West 15.44 feet to a point; thence South 05 Degrees 16 Minutes 06 Seconds West 15.44 feet to a point; thence South 06 Degrees 55 Minutes 12 Seconds West 15.44 feet to a point; thence South 08 Degrees 34 Minutes 18 Seconds West 15.44 feet to a point; thence South 13 Degrees 25 Minutes 34 Seconds West 9.50 feet to a point; thence South 16 Degrees 20 Minutes 38 Seconds West 9.50 feet to a point; thence South 19 Degrees 15 Minutes 40 Seconds West 9.50 feet to a point; thence South 22 Degrees 10 Minutes 20 Seconds West 9.50 feet to a point; thence South 25 Degrees 05 Minutes 16 Seconds West 9.50 feet to a point; thence South 28 Degrees 00 Minutes 15 Seconds West 9.50 feet to a point; thence South 30 Degrees 55 Minutes 05 Seconds West 9.50 feet to a point; thence South 33 Degrees 50 Minutes 02 Seconds West 9.50 feet to a point; thence South 36 Degrees 45 Minutes 03 Seconds West 9.50 feet to a point; thence South 39 Degrees 39 Minutes 56 Seconds West 9.50 feet to a point; thence South 42 Degrees 34 Minutes 43 Seconds West 9.50 feet to a point; thence South 45 Degrees 29 Minutes 38 Seconds West 9.50 feet to a point; thence South 45 Degrees 50 Minutes 27 Seconds West 11.07 feet to a point; thence South 48 Degrees 11 Minutes 10 Seconds West 11.07 feet to a point; thence South 50 Degrees 31 Minutes 51 Seconds West 11.07 feet to a point; thence South 52 Degrees 52 Minutes 38 Seconds West 11.07 feet to a point; thence South 55 Degrees 13 Minutes 21 Seconds West 11.07 feet to a point; thence South 57 Degrees 34 Minutes 02 Seconds West 11.07 feet to a point; thence South 59 Degrees 54 Minutes 44 Seconds West 11.07 feet to a point; thence South 62 Degrees 15 Minutes 23 Seconds West 11.06 feet to a point; thence South 64 Degrees 36 Minutes 15 Seconds West 11.07 feet to a point; thence South 61 Degrees 28 Minutes 35 Seconds West 55.22 feet to a point; thence South 61 Degrees 59



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 Degrees 22 Minutes 07 Seconds West 134.78 feet to a point; thence South 14  
 Degrees 22 Minutes 07 Seconds West 249.57 feet to a point; thence South 14  
 Degrees 22 Minutes 07 Seconds West 262.67 feet to a point; thence South 89  
 Degrees 51 Minutes 27 Seconds West 281.17 feet to a point; thence South 02  
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 Degrees 07 Minutes 23 Seconds East 306.23 feet to a point; thence North 01 Degree  
 52 Minutes 28 Seconds East 87.86 feet to a point; thence North 72 Degrees 48  
 Minutes 56 Seconds West 199.21 feet to a point; thence South 01 Degree 47  
 Minutes 46 Seconds West 78.51 feet to a point; thence North 88 Degrees 05  
 Minutes 59 Seconds West 114.01 feet to a point; thence North 01 Degree 32  
 Minutes 13 Seconds East 971.23 feet back to the Point of True Beginning and  
 containing 380.93 acres more or less.

7. The boundaries of the above district as described and defined above are hereby established and shall be shown on the Official Zoning Maps of the City of Huntsville, Alabama, on file in the Planning Department of the Urban Development Department, in accordance with the various classifications herein enumerated and hereafter the regulations governing said district under the Zoning Ordinance of the City of Huntsville, Alabama, shall apply to the area so described.

8. This ordinance shall take effect from and after the date of publication.

**ADOPTED** this the \_\_\_\_ day of \_\_\_\_\_, 2025.

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President of the City Council of  
the City of Huntsville, Alabama.

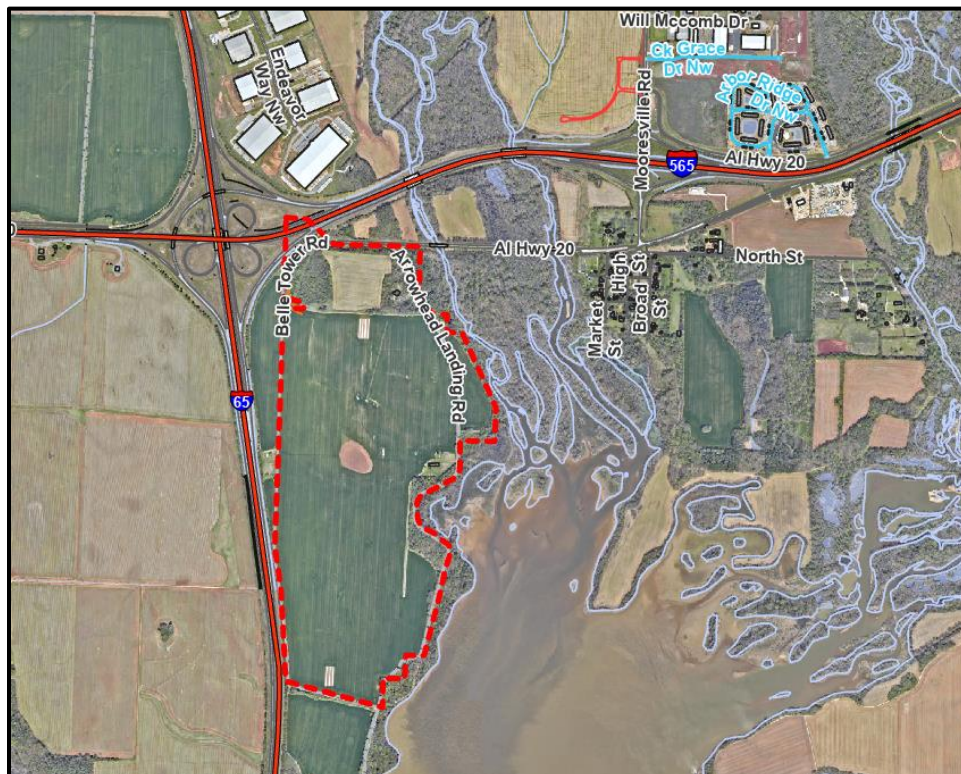
**APPROVED** this the \_\_\_\_ day of \_\_\_\_\_, 2025.

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Mayor of the City of Huntsville, Alabama

NOTICE OF CONSIDERATION OF A PROPOSED ORDINANCE REZONING LAND LYING ON THE SOUTH SIDE OF AL HWY 20 AND EAST OF I-65 FROM R2B AND C4 TO PD-H PURSUANT TO ALABAMA CODE SECTIONS 11-52-77 AND 11-52-78 AND THE ZONING ORDINANCE OF THE CITY OF HUNTSVILLE, ALABAMA

The City Council of the City of Huntsville, Alabama, will hold a meeting at 5:30 P.M. on December 18, 2025, at Huntsville City Hall located at 305 Fountain Circle SW, Huntsville, Alabama 35801, in the City Council Chambers located on the 2<sup>nd</sup> floor at which the Council shall consider an ordinance rezoning of certain property located within the city limits of the City of Huntsville described herein from Residence 2-B District and Highway Business C-4 District to Planned Development-Housing District, LUI 56. A copy of the proposed ordinance and report to the Planning Commission with supporting documents is available for public inspection in the Planning Department at Huntsville City Hall. A public hearing shall be held at the meeting prior to consideration of the rezoning ordinance at which members of the public are invited to attend and provide comments in opposition to or in favor of the proposed rezoning. The property for which rezoning is to be considered is generally located at the southeast corner of the intersection of Interstate 65 and Interstate 565 and bound on the north by Old Highway 20, on the west by Interstate 65 and Belle Tower Road, and on the east and south by the Wheeler Wildlife Refuge, said property being commonly known as the McCrary Farm. A map of the property proposed to be rezoned is included below.





## REPORT TO THE CITY COUNCIL ON THE WESTMOORE LANDING PLANNED DEVELOPMENT-HOUSING DISTRICT

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### I. Request:

The request is to rezone 380.93 acres (16,593,310.80 square feet) of land from Highway Business C-4 District (C4) and Residence 2-B District (R2B) to Planned Development-Housing District (PD-H) with a Land Use Intensity (LUI) rating of 56 with proposed Residential Floor Area for the development of 9,719,560 square feet. The proposed Open Space requirement for the development is 12,342,169 square feet.

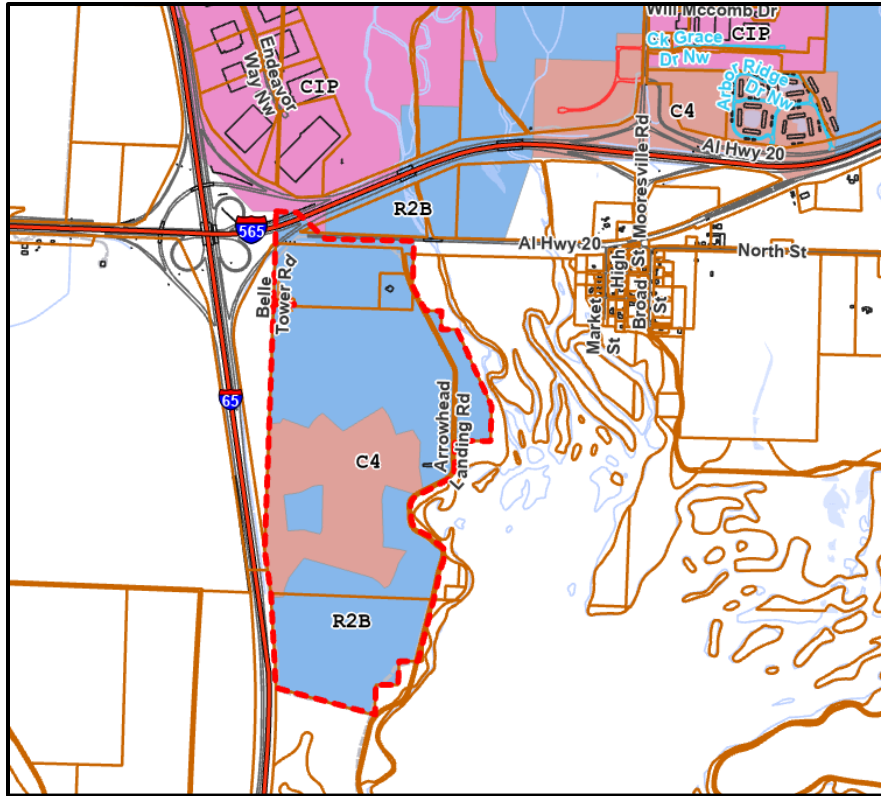
### II. Location:

The land (*outlined in blue on the image below*) lies generally on the east side of Interstate 65 and south of Interstate 565.



**Vicinity Map**

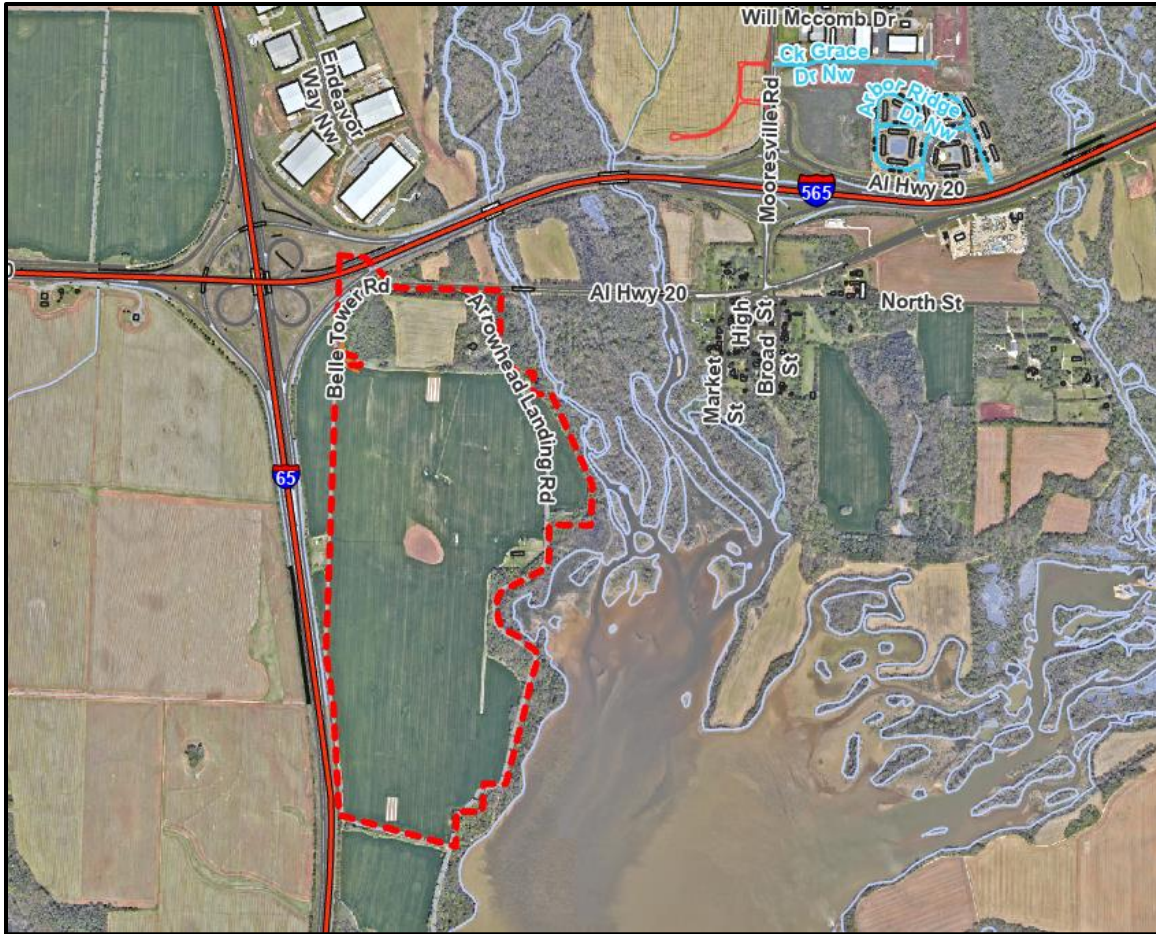
The subject property is currently zoned 98.08 acres of C4 and 282.85 acres of R2B. It is adjacent and in proximity with properties zoned Commercial Industrial Park, Highway Business C-4, and Residence 2-B Districts.



**Zoning Map**

The property has been generally used as agricultural farmland. The Singing River Trail House and two (2) cell towers exist within the site. To the east of the property approximately ½ mile off AL Hwy 20 is the Historic Mooresville Town. The property is surrounded by the Wheeler Wildlife Refuge that includes the Singing River Trails, Limestone Creek Boat Ramp, and river deltas formed by the Tennessee River. Properties to the east, west, and south of the subject property are outside Huntsville city limits. Developments within the Huntsville city limits, which is to the north of the property across I-565, include Southpointe Industrial Park, Limestone Creek Apartment Homes, and various industrial and warehousing businesses.





**Aerial Map**

### **III. Development:**

The proposed Westmoore Landing Planned Development (PD) is a mixed-use development that adopts the Traditional Neighborhood Development (TND) approach. The design concept adopted within this proposed development aims to integrate various land uses in a compact manner with shorter distances between homes, businesses, and public spaces with pedestrian-friendly streets and sidewalks that promote walkability. The proposed development also comprises a variety of housing types and scales to accommodate diverse housing needs by offering a range of housing options, which include single-family detached and attached dwellings, cluster housing, and apartments.

The proposed PD utilizes the concept of a "transect", a geographical cross-section revealing a sequence of environments, to guide urban planning and development. This approach, also known as Form-Based Code or SmartCode, emphasizes urban form, walkability, and mixed uses, unlike conventional zoning that separates land uses. It helps organize growth by defining transitions from rural to urban areas. The PD-H application document illustrates typical natural and rural-urban transects with zones ranging from natural to urban core and special districts.



The proposed PD plan outlines the distribution of public open spaces to enhance community quality of life by providing areas for recreation, exercise, and relaxation. These spaces act as community hubs, encouraging social interaction. The plan aims to ensure residents have access to various programmed and unprogrammed open spaces within a 1/4 mile pedestrian shed, offering diverse gathering spaces for community events and outdoor activities.

The proposed Westmoore Landing PD will be accessed from AL Hwy 20, which is classified as a major collector road. It will have two main access points off AL Hwy 20, which will connect to its street network detailed in the PD's **Street Network Plan**, which shows varying right-of-way (R.O.W.) dimensions and parking conditions, including:

- Mixed-Use with Greenway Trail - Dedicated Parking - 70' R.O.W.
- Mixed-Use - Dedicated Parking - 57' R.O.W.
- Mixed-Use One Way - Dedicated Parking - 30.5' R.O.W.
- Mixed-Use Angled Parking - Dedicated Parking - 42' R.O.W.
- Neighborhood Boulevard with Greenway Trail - Unmarked Parking - 73' R.O.W.
- Neighborhood Boulevard - Unmarked Parking - 70' R.O.W.
- Neighborhood Primary Street - Unmarked Parking - 53' R.O.W.
- Neighborhood Secondary Street - Unmarked Parking - 49' R.O.W.
- Neighborhood One Way Street - Unmarked Parking - 29' R.O.W.
- Neighborhood Collector with Greenway Trail - Dedicated Parking - 60' R.O.W.
- Neighborhood Collector - Dedicated Parking - 57' R.O.W.
- Residential Alley - No Parking - 26' R.O.W.
- Mixed-Use Alley - No Parking - 30' R.O.W.

The PD-H application document also provides Street Sections with detailed diagrams and specifications for various street sections, outlining sidewalk widths, furnishing zones, parking dimensions, and pavement widths for different street types. Dedicated access to Arrowhead Landing, TVA land, and federal interstate bridges will be provided via a realigned and improved Bell Tower Road. Coordination with the City of Huntsville and the Singing River Trail organization will occur for areas where the Singing River Trail and Arrowhead Landing Road intersect with the project boundary.

The proposed Westmoore Landing PD is divided into several transect zones with specified acreage:

- T3-Neighborhood: 151.75 acres
- T3-Hamlet: 27.13 acres
- T4-Neighborhood: 75.56 acres
- T4-Village: 28.41 acres
- T5-Neighborhood: 80.98 acres
- T5-Town Center: 17.10 acres

### **Transect & Community Types Overview**

- **T3-Neighborhood (T3-N):** Designed for a gradual transition from less dense natural environments to denser urban ones. These areas feature moderate development with varied residential uses and integrated open spaces. Vegetation defines curvilinear streets and parks, with a balance leaning towards landscape over buildings.
- **T3-Hamlet (T3-H):** Complements T3-N residential patterns with appropriately scaled commercial/live-work/mixed-use areas. T3-H areas are pedestrian-oriented with attached residential uses and open spaces for neighborhood gatherings. Buildings and hardscapes have a more significant presence to define activity nodes and formal open spaces.
- **T4-Neighborhood (T4-N):** Intended for more intense, urban residential development. Buildings may be taller and closer to the street, creating a denser streetscape. These neighborhoods integrate single-family, attached, and multi-family housing within walking distance of commercial areas and neighborhood-scaled open spaces, featuring connected street systems with sidewalks and bikeways.
- **T4-Village (T4-V):** Complements T4-N with commercial/mixed-use Village Centers scaled to the surrounding residential neighborhood. T4-V areas are pedestrian-oriented with various attached, multi-family, and commercial uses, and open spaces for community gatherings. Buildings and hardscapes have significant presence, with mid-rise heights and zero setback build-to zones for vibrant commercial activity.
- **T5-Neighborhood (T5-N):** The densest residential transect, intended as a vibrant neighborhood with diverse buildings that directly engage active streetscapes. It

provides population density to support adjacent T5-TC commercial, and office uses. T5-N buildings are typically multi-story, mid-rise with parapet roofs, and may include structured parking. Buildings generally front streetscapes with shallow setbacks and residential stoops or commercial ground floors to encourage pedestrian activity.

- **T5-Town Center (T5-TC):** A regional gathering place for living, working, shopping, and connecting. It serves as a center for commerce, civic, and recreational activities on a grander scale. Buildings are predominantly urban, mixed-use, with ground-floor commercial and upper-floor residential, office, and hospitality uses. Building forms are typically mid-rise with parapet wall roofs and street-level storefronts, built to the sidewalk with active pedestrian zones. Parking options include head-in angle, parallel, structured, and concealed surface lots.

**Block Diagram.** The block structure in the proposed Westmoore Landing PD is designed to be flexible, allowing for a variety of building types to support diverse housing, income ranges, and age demographics. Block dimensions could be adjusted to create special lots, addresses, street setbacks, and common open spaces. The block diagram illustrates the potential for various building types within the same block, creating a transition of densities from single-family dwellings to mixed-use multi-family buildings. It also shows the relationship between different frontages like porches, stoops, and storefronts. Blocks are proposed to have perimeters that do not exceed 3,200 linear feet along the street right-of-way. Blocks are proposed to be wide enough for at least two tiers of lots, with additional tiers permitted for cottage clusters.

**Permitted Uses.** The PD-H application document includes tables detailing permitted land uses by transect, including dwellings and other principal and accessory uses that are allowed generally under the PD-H regulations, R2B and C4 districts of the city.

The T3-Neighborhood (T3-N), T4-Neighborhood (T4-N), and T5-Neighborhood (T5-N) transects account for 81% of the total land area within the proposed PD. Permitted uses within these transects would mainly be residential. Meanwhile, the remaining 19% of the total land area within the PD would be classified as the T3-Hamlet (T3-H), T4-Village (T4-V), and T5-Town Center (T5-TC) transects, wherein residential uses would be permitted together with commercial, medical, and small-scale production uses.

**Conditions on Uses.** Specific conditions are outlined for various uses, including Animal Boarding Facility, Auction Gallery, Automobile Repair Garage, Mechanical and Body, Bakery, Commercial Heliport/Helipad/Helistop, Dry Cleaning Establishments, Light Manufacturing & Assembly.

If the subject property were to be developed strictly according to its existing zoning districts, the R2B would generally permit uses including, but not limited to,

single-family detached dwellings, attached dwellings, and multifamily, offices, and hotels/ motels. Meanwhile, permitted uses in the C4 district provide for retailing of goods and the furnishing of major services, selected trade shops, and automotive repairs. Uses not explicitly allowed in the C4 district include the following:

***Residential Uses***

- Semi-Detached Dwellings
- Single Family Attached Dwellings
- Single Family Detached Dwellings
- Two Family Dwellings
- Zero Lot Line Dwellings
- Home Occupation

**Building Type Overview.** The Westmoore Landing PD proposes the following building types by category:

- **Single Family Detached:**
  - Cluster Housing
  - Single Family Rear Entry
  - Single Family Front Entry
  - Accessory Dwelling Unit (ADU)
- **Single Family Attached:**
  - Duplex
  - Townhouse
- **Multifamily:**
  - Carriage House Flat
  - Manor House
  - Stacked Flats
- **Other Categories (including commercial, mixed-use, and civic structures):**
  - Mixed-Use
  - Live-Work
  - Commercial

- Civic
- Amenities

The PD-H application document also includes typical floor plans and visual representations for many of these building types to illustrate their design and floor layout. Each building type can house any use permitted within its respective transect zone; the name of the building type does not limit or expand the allowed uses.

Generally, each lot is permitted one primary building. Exceptions exist for multiple primary buildings on a lot if a site plan ensures compliance with all standards, or within an approved fee simple condominium lot. This entails establishing a legal structure that would allow for the separate ownership of individual dwelling units within a single lot, while also establishing shared ownership of common areas. Accessory structures and Accessory Dwelling Units (ADUs) are permitted in addition to a primary building. Notwithstanding anything to the contrary, all lots shall have legal access to a public street. The minimum required street, including alleys, access width of a lot shall be at least the minimum lot width requirement set out in the Building Type Standards, or if no standard is set out at least 20 feet.

**Building Types.** This section of the PD-H application document provides standards on the physical forms of buildings within each Transect to create variation in the built environment within an appropriate scale. These regulations supplement the development standards for each zoning district, with more restrictive regulations prevailing in case of conflict.

- **Applicability:** Any building construction or alteration must comply with the standards for each building type and other applicable standards. Applicants select an allowed building frontage type with the chosen building. Each building type can house any use permitted in its transect, and building type names do not limit or expand uses.
- **Number of Buildings:** Generally, each lot has one primary building type, with exceptions for multiple primary buildings allowed if a site plan ensures compliance with standards or within an approved fee simple condominium lot. Accessory structures and ADUs are permitted in addition to a primary building.
- **Building Orientation:** The primary facade must be oriented towards the street or open space.

The PD-H application document lists permitted building types by transect, including Single Family Detached (Cluster Housing, Single Family Rear Entry, Single Family Front Entry, Accessory Dwelling Unit), Single Family Attached (Duplex, Townhouse), Multifamily (Carriage House Flat, Manor House, Stacked Flats), Mixed-Use, Live-Work, Commercial, Civic, and Amenities.

**Typical Floor Plans** The PD-H application document includes typical floor plans for Cluster Housing, Single Family Front Entry, Single Family Rear Entry, Accessory

Dwelling Unit, Duplex, Townhouse, Carriage House Flat, Manor House, Stacked Flats, Mixed-Use, Live-Work, Commercial, and Civic (example fire station).

For comparison, the following are the typical single-family residential building types in the T3-Neighborhood (T3-N), T4-Neighborhood (T4-N), and T5-Neighborhood (T5-N) transects, which reflect the intent of each transect to generally allow for residential uses:

Cluster Housing			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	18 ft. min.	18 ft. min.	16 ft. min.
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback (C)	10 ft.	10 ft.	0 or 3 ft.
Minimum Rear Setback (D)	0 or 10 ft.	0 or 10 ft.	0 or 10 ft.
Maximum Height (E)	2 stories	3 stories	4 stories
Separation between Buildings (F)	0 or 10 ft. min.	0 or 6 ft. min.	0 or 6 ft. min.
Paseo (Public Access Easement) Width (G)	20 ft. min.	15 ft. min.	15 ft. min.
Minimum Open Space Setback (H)	5 ft.	5 ft.	5 ft.

**Notes:**

**Building Orientation:** The fronts of units may not face the rear of another unit.

**Parking Access & Location:** Rear loaded with no street access is allowed if Alley Access is available. Side and/or Rear Surface Parking is also allowed.

**Open Space:** 150 square feet of Common Open Space shall be provided per unit.

**Cluster Size:** Cluster Housing can consist of 3-36 units per cluster.

**Distance Buffer:** Clusters shall have a minimum distance buffer of 400 ft. between any two clusters.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



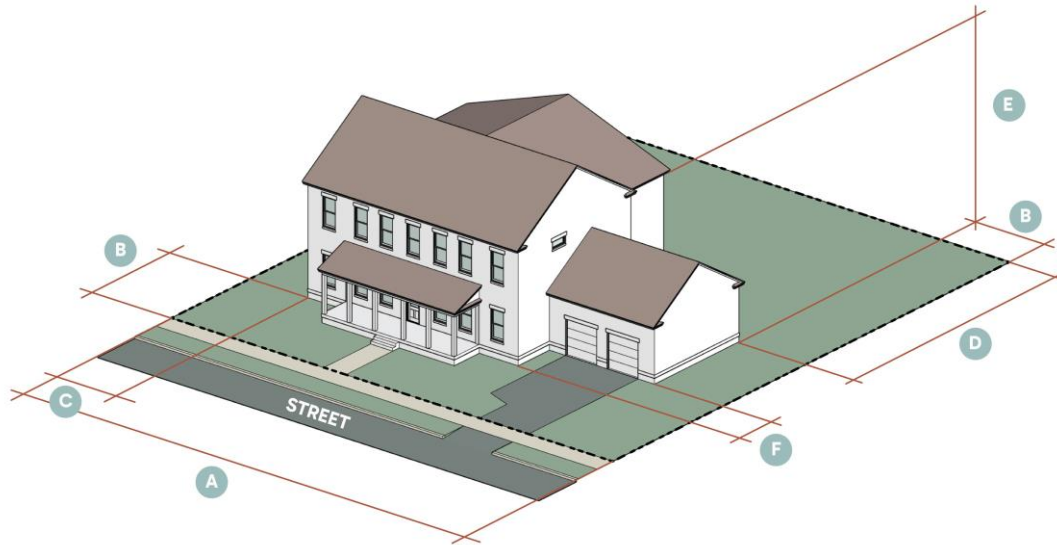
**Cluster Housing**

Single Family Front Entry			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	55 ft. min.	55 ft. min.	not applicable
Minimum Street Setback (B)	15 ft.	10 ft.	
Minimum Side Setback	5 ft.	5 ft.	
Minimum Rear Setback (D)	0 or 15 ft. min.	0 or 15 ft. min.	
Maximum Height (E)	3 stories	4 stories	
Garage Setback (F)	10 ft. min.	10 ft. min.	

**Notes:**

**Parking Access & Location:** Front or side loaded with street access. Detached or attached garages are permitted.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



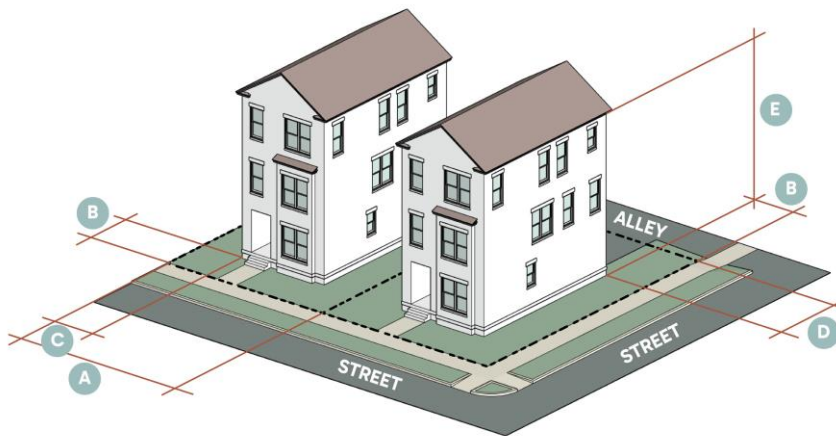
**Single-Family Front Entry**

Single Family Rear Entry			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	25 ft. min.	25 ft. min.	25 ft. min.
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback	5 ft.	5 ft.	5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	0 or 15 ft. min.	0 or 15 ft. min.
Maximum Height (E)	3 stories	4 stories	4 stories

**Notes:**

**Parking Access & Location:** Rear loaded with Alley Access, when available. Detached or Attached Garages are permitted.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



**Single Family Rear Entry**

Accessory Dwelling Unit (ADU)			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	N/A	N/A	N/A
Minimum Street Setback (B)	N/A	N/A	N/A
Minimum Side Setback	0 or 5 ft.	0 or 5 ft.	0 or 3 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	0 or 15 ft. min.	0 or 15 ft. min.
Maximum Height (E)	3 stories	3 stories	3 stories
Separation between Buildings (F)	10 ft. min.	10 ft. min.	10 ft. min.



**Notes:**

**Permitted Primary Building Types:** Single Family Rear Entry, Single Family Front Entry, Duplex, Townhouse, Manor House, and Live-Work.

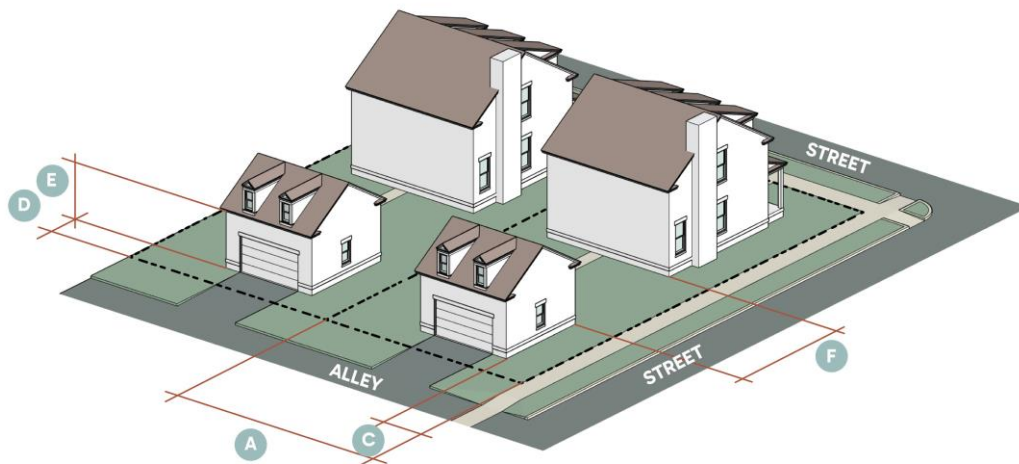
**Parking Access & Location:** Shall be located underneath, behind, or beside the building only on interior lot lines.

**ADU Size:** The size of an ADU is not to exceed 60% of the primary building's habitable floor area floor area.

**ADU Location:** ADUs may be attached, semi-detached, or detached structures. Separation between buildings does not apply when ADU is attached to primary building. Where applicable, open-air breezeways may encroach on separation between buildings. ADU's shall be subordinate in mass, size, and height to the primary building. The architectural style shall be consistent or complementary in design, character, and style to the primary building.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.

**Prohibition:** ADUs are not permitted on fee simple cluster lots.



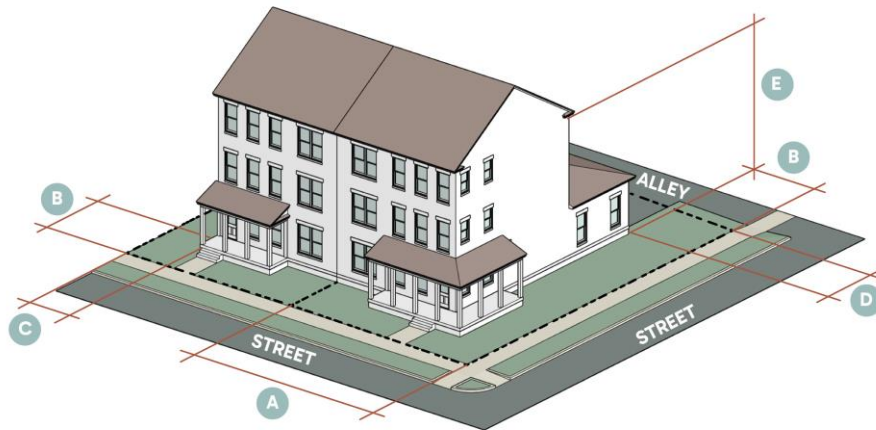
**Accessory Dwelling Unit (ADU)**

Duplex			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	16 ft. min. per unit	16 ft. min. per unit	16 ft. min. per unit
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback	0 or 5 ft.	0 or 5 ft.	0 or 5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	0 or 15 ft. min.	0 or 15 ft. min.
Maximum Height (E)	3 stories	4 stories	4 stories

**Notes:**

**Parking Access & Location:** Rear loaded with alley access. A surface lot or detached/attached garage is permitted.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



**Duplex**

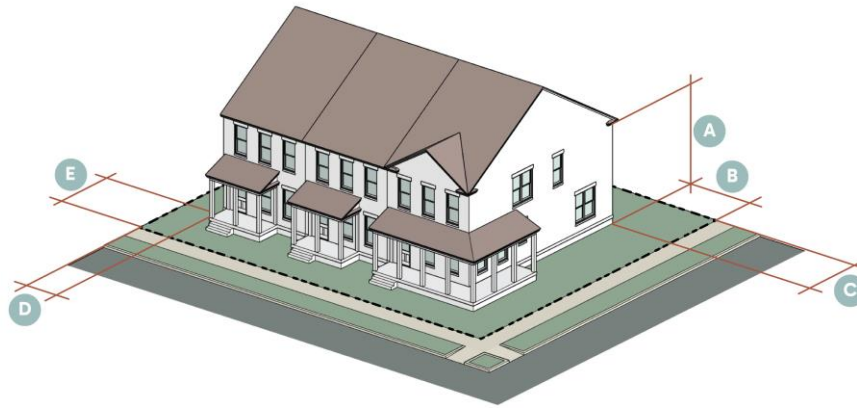
Townhouse			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	16 ft. min. per unit	14 ft. min. per unit	11 ft. min. per unit
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback	0 or 5 ft.	0 or 5 ft.	0 or 5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	5 or 20 ft.	5 or 20 ft.
Maximum Height (E)	3 stories	4 stories	4 stories

**Notes:**

**Number of Units:** Townhouses can have 3 to 8 units in a building.

**Parking Access & Location:** Rear loaded with alley access. A surface lot or detached/attached garage is permitted.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



### Townhouse

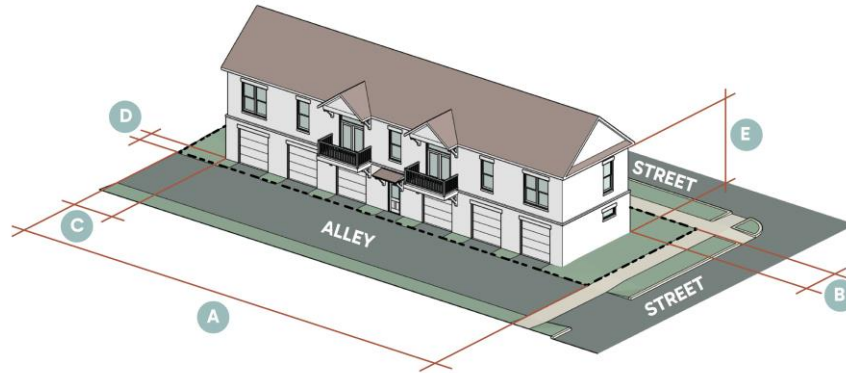
The proposed PD allows for multifamily dwellings in all transects with varying density to reflect the intent of each of the transects. The following are the typical multiple family residential building types:

Carriage House Flat			
Standard	T3-N	T4-N	T5-N
Lot Width (A)	24 ft. min.	24 ft. min.	24 ft. min.
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback	0 or 5 ft.	0 or 5 ft.	0 or 5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	5 or 20 ft.	5 or 20 ft.
Maximum Height (E)	3 stories	3 stories	3 stories

**Notes:**

**Parking Access & Location:** Rear loaded with alley access. Garage parking is located below the unit on the ground floor.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



**Carriage House Flat**

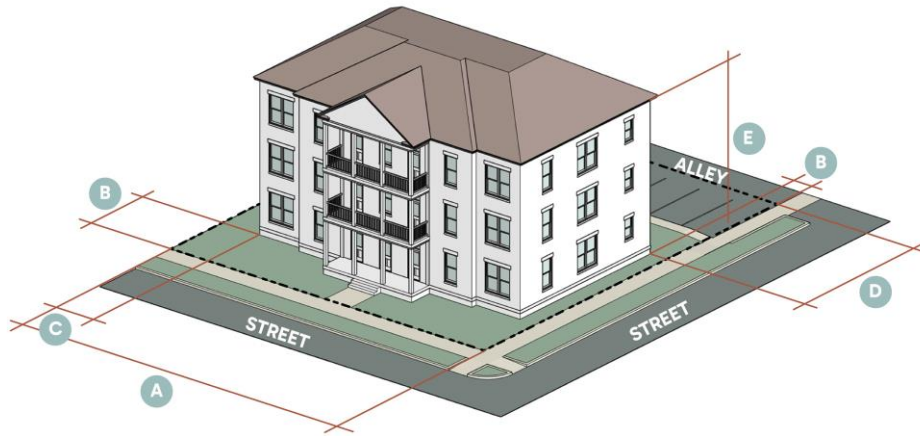
<b>Manor House</b>			
<b>Standard</b>	<b>T3-N</b>	<b>T4-N</b>	<b>T5-N</b>
Lot Width (A)	40 ft. min.	40 ft. min.	40 ft. min.
Minimum Street Setback (B)	15 ft.	10 ft.	5 ft.
Minimum Side Setback	5 ft.	5 ft.	5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	5 or 20 ft.	5 or 20 ft.
Maximum Height (E)	3 stories	4 stories	4 stories
<b>Manor House</b>			
<b>Standard</b>	<b>T3-H</b>	<b>T4-V</b>	<b>T5-TC</b>
Lot Width (A)	40 ft. min.	40 ft. min.	40 ft. min.
Minimum Street Setback (B)	10 ft.	5 ft.	5 ft.
Minimum Side Setback	5 ft.	5 ft.	5 ft.
Minimum Rear Setback (D)	0 or 15 ft. min.	5 or 20 ft.	5 or 20 ft.
Maximum Height (E)	3 stories	4 stories	4 stories

**Notes:**

**Number of Units:** Manor Houses can have 2 to 14 units per building.

**Parking Access & Location:** Rear loaded with no street access if alley access is available, and side or rear surface parking is allowed.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



**Manor House**

<b>Stacked Flats</b>				
<b>Standard</b>	<b>T4-N</b>	<b>T4-V</b>	<b>T5-N</b>	<b>T5-TC</b>
Lot Width (A)	70 ft. min.	70 ft. min.	70 ft. min.	70 ft. min.
Minimum Street Setback (B)	10 ft.	5 ft.	10 ft.	5 ft.
Minimum Side Setback (C)	5 ft.	5 ft.	5 ft.	5 ft.
Minimum Rear Setback (D)	5 ft.	5 ft.	5 ft.	5 ft.
Maximum Height (E)	4 stories	4 stories	8 stories	8 stories

**Notes:**

**Number of Units:** Stacked Flats can have 12 or more units per building.

**Parking Access & Location:** Rear loaded with no street access if alley access is available, and side or rear surface parking is allowed.

**Raised Foundations:** A minimum of 18" is required for raised foundations, with screening required for foundations exceeding 36" along public streets, easements, and open spaces.



**Stacked Flats**

Mixed-Use			
Standard	T4-V	T5-N	T5-TC
Lot Width (A)	N/A	N/A	N/A
Minimum Street Setback (B)	0 ft.	0 ft.	0 ft.
Minimum Side Setback (C)	0 or 5 ft.	0 or 5 ft.	0 or 5 ft.
Minimum Rear Setback (D)	0 ft.	0 ft.	0 ft.
Maximum Height (E)	4 stories	8 stories	8 stories

**Notes:**

**Parking Access & Location:** Rear loaded with no street access if alley access is available, and side or rear surface parking is allowed.

**Frontage:** Frontages may encroach into a required front or corner setback.



**Mixed-Use**

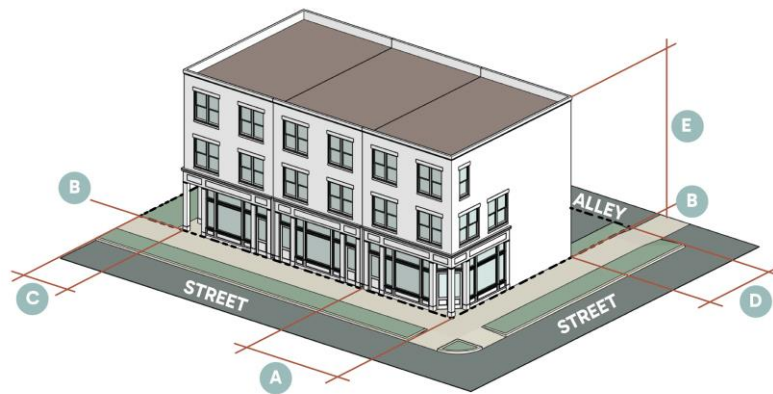


Live-Work			
Standard	T4-V	T5-N	T5-TC
Lot Width (A)	18 ft. min. per unit	18 ft. min. per unit	18 ft. min. per unit
Minimum Street Setback (B)	0 ft.	0 ft.	0 ft.
Minimum Side Setback (C)	0 or 5 ft.	0 or 5 ft.	0 or 5 ft.
Minimum Rear Setback (D)	0 ft.	0 ft.	0 ft.
Maximum Height (E)	4 stories	4 stories	4 stories

**Notes:**

**Parking Access & Location:** Rear loaded with no street access is allowed if alley access is available, and side or rear surface parking is permitted.

**Frontage:** Frontages may encroach into a required front or corner setback.



**Live-Work**

#### IV. Findings

Planned Development – Housing (PD-H) Districts are planned development districts principally for dwellings, and related uses and facilities. Planned Development (PD) Districts are intended for specialized purposes to be situated on tracts suitable in location, area and character for the uses and structures proposed, and are to be planned and developed on a unified basis. Plans for the PD are to provide comfort and convenience of occupants, facilitate protection of the character of surrounding neighborhoods, and provide facilities appropriate to the needs of the city.

The application for PD Amendments as required in Section 30.5.2 includes plans, maps, studies or reports reasonably necessary to the city for the purpose of making required determinations. These materials as submitted including a preliminary development concept plan, shall be hereon referred to as the proposed Planned Development (PD) Submittal Packet.

Based on Section 30.5.5 of the Zoning Ordinance, the following were determined regarding the submitted plans for the proposed PD-H:

1. *As to the suitability of the tract for the general type of PD zoning proposed, physical characteristics of the land, and relation of the proposed development to surrounding areas and existing and probable future development.*

The subject property is currently zoned R2B and C4. The C4 District is intended to be utilized primarily by those commercial uses which cater to the traveling public, tourists, trucks, and heavy automobile traffic. Meanwhile, R2B Districts are intended to cater residential needs with a higher density allowed and some commercial uses that complement the permitted residential uses such hotels and offices.

The proposed PD is accessible from multiple major highways including I-65, I-565, and AL Hwy 20. To the north of the property are the Southpoint and Greenbrier Industrial Parks. It is also within proximity to the Jetplex and Lowe Industrial Parks. These are expanding employment centers that have broad transportation connectivity to the surrounding areas. The adjacent areas, including the proposed PD property, have been identified in the Big Picture Comprehensive Plan of the city as a prime area for housing to provide ready access for employees of these employment centers.

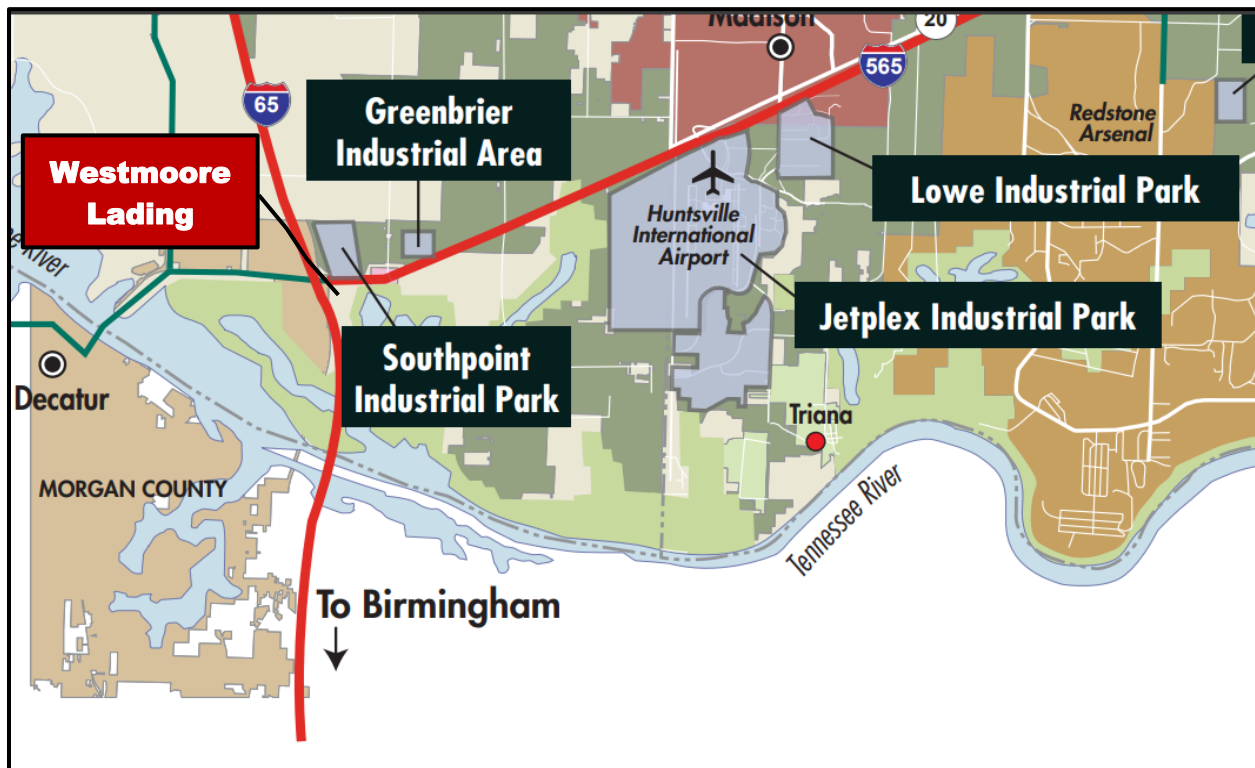
Westmoore Landing PD with its plan to build a mixture of residential, commercial, medical, and small-scale production uses align with the vision specifically for the Southwest Subarea of the Big Picture plan, which would be for services and quality-of-life amenities to have been added throughout the district as new neighborhoods have developed. The proposed PD Submittal Packet would realize the following principles including but not limited to those as set forth by the Big Picture plan:

- a. *Aging in Place / Healthy Communities* – as the proposed PD plans to construct a variety of housing types, these would create housing options for individuals to live out their lives either in the home they've known, or within their familiar neighborhood where they can lead a life of reasonable independence.
- b. *Complete Streets* - several neighborhood nodes are being proposed throughout the proposed development that would support mixed use product types. The streets incorporate various elements including sidewalks, planting strips, and furnishings that serve as focal points for residents to access services, foster community interaction, and support neighborhood identity.
- c. *Design Guidelines* – while the proposed PD plan does establish general design standards, it is the intent of this development to also establish an Architectural Review Board under its Homeowner Association that would review the aesthetic concerns of the community including but not limited



to what colors and materials could be used for the structures to be constructed.

- d. *Employment Diversification* – as there would be a mixture of permitted uses within this proposed PD, this would provide a diversity of goods and services to appeal to the widest customer base feasible. Not only would this provide for a sound economic base for the community but would also contribute to its immediate vicinity, which would help ensure consistency and stability in the face of fluctuations in the economy.
- e. *Housing Choice* – the proposed PD would allow for a mixed housing products and a variety of price points within the same area. This diversity in housing stock would help encourage a pattern of life cycle housing.
- f. *Mixed-Use Development* – as this proposed PD combines various components of housing, offices, and retail this would help ensure increased walkability, reduced traffic congestion, enhanced economic activity, and a stronger sense of community.



- 2. *As to relation to, and access from, major or collector roads, and to utilities and other facilities and services.*

The subject property is accessed off AL Hwy 20, which would be classified as major collector in the 2025 Major Street Plan. Major Collector streets

as defined in the Subdivision Regulations are streets of reasonable continuity that channel traffic between arterials and from other collector streets to the arterial system. As these types of streets may carry relatively high traffic volumes, thus it may sustain retail or other commercial establishments along its route. The intensity of the permitted uses within the proposed PD as it relates to traffic volume could be appropriately addressed for an optimized traffic flow.

The proposed PD Submittal Packet to establish primary connections that would act as the main arteries of the development. These connections provide essential access to homes, community amenities, and key gathering spaces. The thoroughfare within the proposed development is intended for a wider pedestrian walkway, bike paths, or even roads, which would connect key areas like neighborhoods, commercial centers, and main parks. The proposed PD Submittal Packet also intends to have a network of pathways, paseos, and smaller roads that would offer more intimate access to neighborhoods, parks, and natural areas.

There are adequate utilities and services connections in the area for this proposed development to tie into.

3. *As to adequacy of evidence on unified control and suitability of any proposed agreements, contracts, deed restrictions, sureties, dedications, contributions, guarantees, or other instruments, or the need for such instruments, or for amendments in those proposed.*

A duly executed unified control report has been submitted.

4. *As to the suitability of plans proposed, or the desirability of changes, with reasons therefor.*

The proposed PD Submittal Packet meets the Land Use Intensity requirements based on the existing zoning district from which this proposed PD-H District will be established. The computed values for the proposed development as it meets the required ratios are as follows:

	Required under R2-B & Other Districts		Westmoore Landing PD-H Estimated Values	
<b>Minimum Open Space Ratio</b>	0.700	11,615,318 sf	0.744	12,342,169 sf
<b>Minimum Livability Space Ratio</b>	0.400	6,637,324 sf	0.500	8,304,460 sf
<b>Minimum Recreation Space Ratio</b>	0.065	1,078,565 sf	0.105	1,749,746 sf
<b>Maximum Floor Area Ratio</b>	0.606	10,055,546 sf	0.586	9,719,560 sf

The proposed PD Submittal Packet is subject to and shall meet the following PD-H standards as per Sections 31.4 and 31.5 of the Zoning Ordinance:

1. Site Planning, External Relationships

- a. *Principal Vehicular Access Points* - The proposed PD Submittal Packet provides detailed diagrams and specifications for various street sections, outlining sidewalk widths, furnishing zones, parking dimensions, and pavement widths for different street types. The plan proposes the following street types with varying right-of-way (R.O.W.) dimensions and parking conditions:
  - Mixed-Use with Greenway Trail & Dedicated Parking - 70' R.O.W.
  - Mixed-Use with Dedicated Parking - 57' R.O.W.
  - Mixed-Use One Way with Dedicated Parking - 30.5' R.O.W.
  - Mixed-Use with Dedicated Angled Parking - 42' R.O.W.
  - Neighborhood Boulevard with Greenway Trail & Unmarked Parking - 73' R.O.W.
  - Neighborhood Boulevard with Unmarked Parking - 70' R.O.W.
  - Neighborhood Primary Street with Unmarked Parking - 53' R.O.W.
  - Neighborhood Secondary Street with Unmarked Parking - 49' R.O.W.
  - Neighborhood One Way Street with Unmarked Parking - 29' R.O.W.
  - Neighborhood Collector with Greenway Trail with Dedicated Parking - 60' R.O.W.
  - Neighborhood Collector with Dedicated Parking - 57' R.O.W.
  - Residential Alley with No Parking - 26' R.O.W.
  - Mixed-Use Alley with No Parking - 30' R.O.W.
- b. *Access for Pedestrians and Bicycles* - Access for pedestrians and cyclists entering or leaving the district shall be by safe and convenient routes. Submitted plans indicate installation of a minimum of 5 feet sidewalks along all streets within the development except on alleys and those that are adjacent to open spaces/ parks. Certain streets design also includes a minimum of 4 feet furnishing zone, which would allow street furniture and amenities, such as lighting, benches, utility poles, tree pits, and bicycle parking.

- c. *Protection of Visibility - Automotive Traffic, Cyclists, and Pedestrians* - Protection of visibility for automotive traffic, cyclists, and pedestrians shall be as generally provided at Section 73.10 – Corner Visibility. Submitted plans provide adequate corner visibility.
- d. *Uses Adjacent to Single-Family Residence Districts* – The property has been generally used as agricultural farmland. The Singing River Trail House and two (2) cell towers exist within the site. To the east of the property approximately ½ mile off AL Hwy 20 is the Historic Mooresville Town. The property is surrounded by the Wheeler Wildlife Refuge that includes the Singing River Trails, Limestone Creek Boat Ramp, and river deltas formed by the Tennessee River. Adjacent properties to the east, west, and south of the subject property are outside Huntsville city limits.
- e. *Yards, Fences, Walls, or Vegetative Screening at Edges of PD-H Districts* – Although the subject property is largely surrounded by the Wheeler Wildlife Refuge, submitted plans indicate installation of landscaping along the perimeter of the proposed development. The proposed PD plan also intends to coordinate with the City of Huntsville and the Singing River Trail organization to appropriately address the areas where the Singing River Trail intersect with the project boundary.
- f. *Height Limitations at Edges of PD-H Districts* – Adjacent properties to the east, west, and south of the subject property are outside Huntsville City limits.
- g. *Signs Permitted* – The proposed PD will generally adopt the Sign Control Regulations of the Zoning Ordinance and will allow the following permitted signages:
  - i. Residential Subdivision Signs in the T3-N, T4-N, T5-N transects;
  - ii. Apartment Complexes Signs in the T3-N, T4-N, T5-N transects;
  - iii. Apartment Buildings Signs in the T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC transects;
  - iv. Temporary Signs in the T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC transects;
  - v. Permanent Signs in General Business C-3, Central Business C-B, Research Park 2, and Village Business C-6 Districts in the T3-V, T4-V, T5-TC transects.

## 2. Site Planning, Internal Relationships

- a. *Streets, Drives, Parking and Service Areas* – Submitted PD plans propose streets, drives, parking and service areas that would provide safe and convenient access to dwelling units and general facilities, and for service and emergency vehicles.
  - b. *Vehicular Access to Streets* - Vehicular access to other streets or portions of streets from off-street parking and service areas as proposed in the submitted PD plans would be combined, limited, located, designed, and controlled as to channel traffic from and to such areas conveniently, safely, and in a manner that minimizes marginal traffic friction and promotes free flow of traffic on streets without excessive interruption. The proposed development provides for vehicular access from off-street parking and services area directly to the street from the sites to individual dwelling units.
  - c. *Ways for Pedestrians: Use by Emergency or Service Vehicles* – Submitted plans proposes a minimum of 5' sidewalk along both sides of the rights-of-way except on alleys and those that are adjacent to open spaces/ parks. This would provide connectivity and access to all the amenities of the development as well as safe pedestrian access within the development and to the adjacent Wheeler Wildlife Refuge. The plan ensures residents have access to various programmed and unprogrammed open spaces within a 1/4 mile pedestrian shed, offering diverse gathering spaces for community events and outdoor activities. Proposed PD plans intend to construct roadways that would allow for the use by emergency and service vehicles.
  - d. *Protection of Visibility - Automotive Traffic, Cyclists, and Pedestrians* - Protection of visibility for automotive traffic, cyclists, and pedestrians shall be as provided at Section 31.4.3. Submitted plans provide adequate corner visibility.
5. *As to the desirable specific modifications in generally applicable regulations or officially adopted plans, as applicable in the particular case, based on determinations that such modifications are necessary or justified in the particular case by demonstration that the public purposes of PD or other regulations would be met to at least an equivalent degree by proposals of the applicant.*

The following are the code modifications requested for the Westmoore Landing PD and the provided justifications in the submittal. Approval of the requested modifications shall not exempt any portion of the development from any applicable rules and regulations that promote public health, safety, and general welfare, examples of which include, but are not limited to, Fire and Building Codes. Any portion of the Zoning and Subdivision

Regulations not modified through the Planned Development approval process shall still be deemed applicable and enforceable. Below includes the analysis and recommendations on the proposed modifications and justifications as stated in the PD Submittal Packet.

## 1. Street Cross Sections, Minimum Right-of-Way (ROW) and Pavement Widths:

- **Requirement:** The Subdivision Regulations of the city require sidewalks must be improved according to the city's Engineering Department construction standards. A median strip of grassed or landscaped area at least 3.5 feet wide must separate all sidewalks from adjacent curbs. Minimum ROW widths of 50 ft for local streets & 30 ft for rear service roads. Minimum ROW of 40 ft. Minimum pavement widths of 26 ft for local streets & 20 ft for rear service roads. Minimum pavement width of 21 ft.
- **Modifications:** Street cross sections are in accordance with the Street Sections (pages 21 - 27) per the PD-H submittal packet. Where the aerial apparatus staging area is not provided on-street, it may be provided off the alley in the rear of the property.

Modification of minimum design standards.

- 1) Minimum pavement widths, including travel lanes and on-street parking vary and are prescribed in the Street Sections. Minimum pavement widths to include travel lanes and on-street parking when applicable.
  - 2) Minimum R.O.W. varies and is prescribed in the Street Sections.
- **Justification:** These modifications would still promote safe streets by creating an environment that encourages drivers to slow down and be watchful for pedestrians, a technique common in Traditional Neighborhood Development (TND) neighborhoods. The streets are to be sufficiently designed to accommodate standard garbage and fire trucks. Varying ROWs support a walkable context, creating diverse streetscape experiences. The modified pavement widths promote walkable, safe streets that align with TND standards, as existing subdivision regulations often result in pavement widths wider than ideal for desired vehicle speeds on walkable residential streets. Pavement width corresponds to whether parking is required/dedicated/marked or not required/unmarked temporary.
  - **Analysis and Recommendation** – The proposed modifications combined with the overall design of the development shifts the priority from vehicular movement and separation of uses (the conventional minimums) to walkability, compactness, and integrated ecology (the TND goals). Within the proposed PD, as the requested modifications would be on the applicable Subdivision Regulations, these have been reviewed combined with the overall design of the development, which has been found to accomplish the purposes of the applicable regulation to an equivalent degree as it promotes economical and efficient land use, creating a creative design that shall meet the intent of existing standards and regulations. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified

planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## **2. Road Grading, Improvements, and Curbs:**

- **Requirement:** The city's Subdivision Regulations require roads to be graded and improved according to construction plans submitted prior to final plat approval. All roads must have standard "L" curbs and gutters with appropriate underground drainage facilities on both sides.
- **Modifications:** Streets with ribbon curb shall be allowed in T3 Neighborhood & T3 Hamlet as identified within the Westmoore Landing PD street sections as curbless as an alternative. Mountable, header (ribbon) and/or stand up (post) curbs shall be allowed where streets abut parks, common open space, and/or alleys in all Transects and may be used when design conditions permit. Alleys, as shown in the Westmoore Landing PD street sections, may have an inverse crown, ribbon curbing, and a 5 ft furnishing zone beyond the back of the curb, with final layout and design developed during the construction plan phase.
- **Justification:** These modifications allow for varying street character to match the surrounding building character, with T3-N elements nodding to Mooresville's aesthetic. Inverse crowns in alleys simplify utility layout and maintenance. The diversity in curb types accommodates fire access, site conditions, and promotes more durable construction for alleys.
- **Analysis and Recommendation** – The purpose of grading, road improvements, and curb standards is to ensure proper drainage, vehicle sightlines, and structural stability of the road surface. The PD design as it adheres to the TND model would allow for more natural, curvilinear grades to reduce land disturbance, provided stormwater is managed via bioswales instead of curbs. The performance goals of the model would prioritize slower speeds and safer pedestrian environments and better integrate green infrastructure (LID) like swales. The proposed modifications of the applicable Subdivision Regulations combined with the overall design of the development shall meet the intent of existing standards and regulations as it provides an improved level of amenities, and appropriate and harmonious variety that would create a better environment. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## **3. Access and Easements:**

- **Requirement:** The city's Zoning Ordinance and Subdivision Regulations require each lot to be provided with adequate access to a public street. No structure shall be erected on a lot that does not abut on and have legal access to at least one street for the minimum frontage distance required by the zoning district; if no street frontage is specified, then the minimum street frontage shall be twenty (20) feet.

Easements shall be dedicated to the local government for the purpose of maintaining utility and drainage facilities and courses and containing the various systems which lie outside of dedicated rights-of-way, including any easements necessary outside the boundaries of the proposed subdivision. Easements shall be provided along front, side, and rear lot lines as necessary for the provision and maintenance of drainage and utility systems.

- **Modifications:** Pedestrian ways and paseos shall provide access and will be labeled on the final plat. These private easements for ingress, egress, and public access will exclude any portion of a lot but can include utility easements. All paseos, whether they provide private or public access, will be maintained by the Westmoore Landing Owners Association.

Public utility, public drainage, and private easements for ingress, egress, and public access will serve as both access and utility corridors to lot frontages facing common open space / paseos within the Westmoore Landing PD-H.

Fee simple buildings may be accessed from public streets or public alleys as defined in the Westmoore Landing PD-H Document, as long as Fire and Building Codes are met.

- **Justification:** These changes allow lots to have access to public access corridors that can include utilities. This also encourages walkability and communal open spaces while decreasing pavement/roads.
- **Analysis and Recommendation** – The purpose of the existing standards and regulations on access and easements is to ensure people and services can get in and out safely from individual lots, and the vital infrastructure that serves developments (water, power, drainage) can function and be maintained forever. The proposed modifications combined with the overall design of the development shall meet the intent of existing standards and regulations as unscheduled development on individual lots would still be controlled, and economical and efficient land use would still be promoted. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet. However, this should not be construed to exempt any lot to be created within the proposed development from having legal access to a public street. The minimum required street, including alleys, access width of a lot shall be at least the minimum lot width requirement set out in the Building Type Standards, or if no standard is set out at least 20 feet.

#### 4. Intersection Radii:

- **Requirement:** The city's Subdivision Regulations require a minimum 35 ft radius at intersections.



- **Modification:** Intersections are allowed to have 15 ft radii that encourage traffic to slow while promoting pedestrian safety and traffic calming.
- **Justification:** Excessive turning radii allow vehicles to make turns without slowing down sufficiently to look for pedestrians, thus the reduction in radius promotes safety.
- **Analysis and Recommendation** – The conventional minimum radius standards prioritize the movement and accommodation of vehicles. The TND model adopted by the proposed PD encourages smallest radius that can still accommodate the design vehicle to prioritize the pedestrian environment. The proposed modification combined with the overall design of the development shall meet the intent of existing standards and regulations as it prioritizes safety, walkability, and quality of place, favoring long-term property values and community health, discouraging high-speed traffic. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 5. Street Materials and Traffic Calming:

- **Requirement:** The city's Subdivision Regulations require roads to be graded and improved and conform to the construction standards of the City of Huntsville.
- **Modifications:** Raised medians using different materials (brick, stamped concrete, etc.), brick crosswalks, brick/paver sidewalks, etc., will be permitted within the public ROW in T3-HC, T4 VC, and T5-TC for the purpose of promoting and encouraging traffic calming and enhanced pedestrian safety.
- **Justification:** High-traffic pedestrian areas often require additional traffic calming devices and/or changes in materials to encourage slower speeds. Historically, town centers also used varied sidewalk materials to connect public and private realms.
- **Analysis and Recommendation** – The purpose of standards requiring roads to be graded and improved and conform to construction standards is to ensure the longevity of public assets, guarantees public safety, and protects private investment by managing the natural environment. The proposed modifications combined with the overall design of the development would still meet the intent of existing standards and regulations as it prioritizes safety, walkability, and quality of place. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 6. Lot Line Orientation:

- **Requirement:** The city's Subdivision Regulations require side lot lines to be at right angles to street lines or radial to curving street lines.
- **Modification:** Non-radial side lot lines are permitted without the necessity of approval by the Planning Commission.
- **Justification:** This allows for cluster lots and more efficient land layout.
- **Analysis and Recommendation** – The Subdivision Regulations allow for a waiver on this rule if it would produce a better street or lot plan upon review and approval of the Planning Commission. Given that the Street Network Plan of the proposed PD provides an improved and harmonious environment that prioritizes safety, walkability, and quality of place, the proposed modification combined with the overall design of the development shall meet the intent of existing standard and regulation. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 7. Traffic Control Devices:

- **Requirement:** The city's Subdivision Regulations requires traffic control devices installed on public streets or public ROW to conform to the AL Manual of Uniform Traffic Control Devices.
- **Modification:** Traffic Control Devices, streetlights, and street name signs may deviate from Huntsville standards as long as the Westmoore Landing Owner's Association maintains such and, if applicable, MUTCD standards must be followed, otherwise the City of Huntsville will replace with standard materials.
- **Justification:** This allows for upgraded, more traditional, and timeless traffic control devices.
- **Analysis and Recommendation** – The purpose of the standards and regulations is safety through predictability and uniformity, and efficiency and cost management. The proposed modification would allow installation of Traffic Control Devices, streetlights, and street name signs to complement the aesthetics and overall design of the development, which would be predictable and uniform throughout the development. These devices and objects would still have to meet safety standards. The proposed modification combined with the overall design of the development shall meet the intent of existing standards and regulation. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 8. Perimeter Landscaping:

- **Requirement:** The city's Zoning Ordinance requires perimeter landscaping and lighting to be installed for all PVAS. Landscape is required for all PVA per Section 71.
- **Modifications:** Eliminate the requirement for perimeter landscaping along common lot lines for parking areas where the common lot lines fall within or adjacent to the shared parking areas.
- **Justification:** Shared Parking serves as back-of-house parking, loading, and trash collection areas, and allowing efficiency between lots keeps these areas smaller with less duplicative vehicular circulation paths.
- **Analysis and Recommendation** – The intent of the perimeter landscaping and lighting requirement of the Zoning Ordinance includes improving the appearance of paved and unpaved off-street parking and vehicular use areas (PVAs); and insulate public rights-of-way and adjoining properties from noise, glare, and other distractions originating from off-street PVAs. Perimeter landscaping along common lot lines within shared parking areas run would be a barrier for efficient flow of traffic within the area. Required landscaping internal to the shared parking area would still have to be met. The proposed modification combined with the overall design of the development shall meet the intent of existing standards and regulation. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 9. Single-Family Lot and Building Standards:

- **Requirement:** The city's Zoning Ordinance provides required setbacks, lot areas, lot frontages, building areas, and heights for each proposed building type, with averaged front yards when infilling blocks, corner lots in residential districts, and office building setbacks in R2B.

Not less than four or more than eight single family attached dwellings shall be contiguous. Also, no two contiguous single family attached dwellings shall be built with the same front line. The required difference in front line shall be a minimum of three (3) feet. Each single family attached dwelling shall provide not less than six hundred (600) square feet of yard space exclusive of private access ways and off-street parking. If the yard space is divided, one space shall not contain less than four hundred (400) square feet of open yard space, and the remaining two hundred (200) square feet shall be secluded at ground level from view from streets or neighboring property. Locating the secluded yard space in the required front yard is prohibited.

Accessory Dwelling Units are not permitted.

- **Modifications:** Lot sizes, lot widths, yard setbacks, lot coverage, lot frontage, building setbacks, density, open space requirements, and height regulations are modified to meet those standards as defined for each Transect and Building Type as identified in the Building Type Standards in place of provisions of City of Huntsville Zoning Ordinance and the Subdivision Regulations. Corner lot front yard setback will be determined by the front door location, thereby designating the secondary frontage as a side yard.

Townhouse regulations per City of Huntsville Zoning Ordinance (13.2.3) are modified to meet those standards as defined for the Townhouse in the Building Type Standards. Townhouses may be 3-8 units in a row. Townhouses may have a contiguous front facade. There shall be no min. private open space requirements for Townhouses meeting the 1/4 mile radius of a formal open space type.

Accessory dwelling units: each single family rear loaded, single family front loaded, townhouse, manor house, and live/work lot may contain one accessory dwelling unit in addition to the principal dwelling, with full plumbing, not to exceed 60% of primary building habitable floor area in a separate outbuilding and shall have no minimum street frontage requirements, and no max. lot coverage in the rear yard unless there is no alley, in which a 10' minimum setback must be maintained. Accessory structures shall be subservient in style and massing to the primary dwelling and follow the bulk and height standards in the Westmoore Landing PD-H document.

- **Justification:** The lot standards for each transect and building type are designed to traditional neighborhood development standards to reflect the character of each transect. These standards focus on the form of the building from the public realm to create engaging streetscapes that encourage neighbor interaction.

Historically, townhouses were built with a consistent front façade line across all units within a building. The variation of front setbacks is a suburban requirement to force each unit to read individually rather than as a cohesive building. 1403 Prince St in Alexandria that is a good example. There are many examples up and down the eastern United States. Townhouses may function in the same manner as cluster housing to front open spaces. Common open spaces may be used in place of private open space requirements. Common open spaces are larger and suited to a broader variety of activities than small, individual spaces.

The standards for ADUs fits within the lot type standards within Westmoore Landing. ADUs promote intergenerational living and allow for alternative, attainable housing within the neighborhood fabric.

- **Analysis and Recommendation** – Lot and building standards including the lot sizes, lot widths, yard setbacks, lot coverage, lot frontage, building setbacks, density, open space requirements, and height regulations for the proposed PD provides support for a variety of housing types, income ranges, and age demographics within Westmoore Landing. This addresses the purpose of the

city's PD district regulations, which intends for adopted regulations to promote economical and efficient land use, an improved level of amenities, appropriate and harmonious variety, creative design, conservation of resources, and a better environment. The proposed modifications combined with the overall design of the development shall meet the intent of existing standards and regulations in ensuring orderly development while still aligning with the overall visions and needs of the City of Huntsville as noted in its Big Picture plan. Safety would also still be addressed as necessary Building and Fire Code requirements would have to be met by any and all structures within the Westmoore Landing. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

#### **10. Multi-Family Building Standards:**

- **Requirement:** The city's Zoning Ordinance requires that the closest permitted distance between any two apartment buildings is 14 ft for 2+ stories. Minimum distance from an apartment building to a parking space is 8 ft. Minimum distance from side or rear of building to residential district is 25 ft. Windows in all apartment buildings must have not less than a 12 ft view and a 20 ft view if the building is over 25 ft.
- **Modifications:** Multi-family buildings must follow the Building Type Standards of the Westmoore Landing PD, eliminating window view setbacks, and density requirements.
- **Justification:** Multi-family lot standards are modified to be consistent with residential district lot standards, encouraging the intermixing of product types without obvious setback differences. This re-establishes their important place in the street and provides attainable housing options.
- **Analysis and Recommendation** – Bulk and general standards including the regulatory setbacks for the proposed PD provides support for multiple family dwellings within Westmoore Landing. The proposed modification combined with the overall design of the development shall meet the intent of existing standards and regulations in ensuring orderly development while still aligning with the overall visions and needs of the City of Huntsville as noted in its Big Picture plan. Safety would also still be addressed as necessary Building and Fire Code requirements would have to be met by any and all structures within the Westmoore Landing. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 11. Parking Requirements:

- **Requirements:** The city's Zoning Ordinance provides for off-street parking requirements (Article 70) and requires all motorized vehicles to be parked off-street (Section 73.14.3). Minimum distance from parking areas to any street ROW shall be 20 ft. Within the C4 district, the Zoning Ordinance allows multi-tenant, mixed use, and multi-family developments, to adopt shared parking and loading requirements as set forth in the Shared Parking Factor matrix of Article 27.
- **Modifications:** Parking Requirements are modified to meet the standards defined in the Supplementary Design Standards - Parking Requirements of the Westmoore Landing PD Submittal Packet, replacing provisions of the City of Huntsville Zoning Ordinance. Required parking spaces may be reduced according to Table G.103.05.A. One or more required parking spaces may be located on-street adjoining the lot frontage. Parking is permitted in the first 5 ft of a rear yard when accessed from an alley. Shared parking is encouraged and can be further refined by a professional parking study. On-street loading in marked, signed, and designated areas qualifies as a minimum required loading area for off-hour deliveries. The maximum walking distance in the open between dwelling units and parking areas, delivery areas, and refuse and garbage storage areas per Section 31.5.3. is defined as the closest point between the edge of any of these facilities for building types that have shared facilities.
- **Justification:** Parking requirements are reduced to align with walkable, mixed-use community needs. Shared parking based on peak demands reduces overall parking area to meet realistic conditions of varied maximum demand. This also reduces off-street impervious surfaces and allows for convenient loading areas for small commercial tenants. The clarification for walking distance measurement helps in defining shared facility accessibility.
- **Analysis and Recommendation** – The Zoning Regulations require for single-family residences in other districts and two-family residence one (1) space for each dwelling unit. The dwelling units as proposed would provide garages, which shall serve the purpose of the parking requirement. Surface parking, parking garage requirements and the shared parking standards proposed by the PD would also address the parking requirements for other uses within the development. Shared parking allows different uses (e.g., office space, residential, retail) with complementary peak demand times (daytime vs. nighttime) to utilize the same parking spaces, which reduces impervious surfaces, preserves the natural resources and ecosystems, mitigates urban heat island effect, and encourages non-vehicular transportation. The proposed modifications combined with the overall design of the development shall meet the intent of existing standards and regulations as it promotes economical and efficient land use, an improved level of amenities, appropriate and harmonious variety, creative design, conservation of resources, and a better environment. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the

project's continued and complete compliance with the approved overarching PD Submittal Packet.

## 12. Permitted Land Uses:

- **Requirement:** The PD-H district regulations within the city's Zoning Ordinance permit primarily uses and structures that are residential, complementary non-residential, and accessory to the primary uses and structures. Additional principal or accessory structures and uses are allowed in the PD-H provided the scale, character and/or location of a particular planned development justify it. Both R2B and C4 Districts are the existing zoning classification for the subject property. R2B primarily permits residential types and minimal commercial use, which are mostly for lodging and office purposes, whereas C4 District allows for commercial and some light manufacturing uses and limits residential uses to apartments.

Accessory Dwelling Units are not permitted.

- **Modifications:** Permitted land uses are modified to meet the standards defined for each Transect Community Type, replacing provisions of the City of Huntsville Zoning Ordinance.

Accessory dwelling units: each single family rear loaded, single family front loaded, townhouse, manor house, and live/work lot may contain one accessory dwelling unit in addition to the principal dwelling, with full plumbing, not to exceed 60% of primary building habitable floor area in a separate outbuilding and shall have no minimum street frontage requirements, and no max. lot coverage in the rear yard unless there is no alley, in which a 10' minimum setback must be maintained. Accessory structures shall be subservient in style and massing to the primary dwelling and follow the bulk and height standards in the Westmoore Landing PD-H document.

- **Justification:** This allows uses compatible with mixed-use, walkable communities, and a variety of non-residential uses in the hamlet, village, and town center transects.

The standards for ADUs fits within the lot type standards within Westmoore Landing. ADUs promote intergenerational living and allow for alternative, attainable housing within the neighborhood fabric.

- **Analysis and Recommendation** - The PD-H district allows for additional principal or accessory structures and uses - where the scale, character and/or location of particular planned developments justify additional principal or accessory structures and uses. Considering that the proposed PD is 15 miles away from commercial areas within the city limits, principal structures and uses other than residential would be justified to be permitted within the proposed PD as these would provide the necessary services and goods essential to meet the daily needs of the residents of the community. This meets the purpose of PD districts as



intended by the Zoning Ordinance, which should provide for comfort and convenience of occupants, facilitate protection of the character of surrounding neighborhoods, and provide facilities appropriate to the needs of the city. The Zoning Ordinance furthers that unified planning and development within the PD districts should promote economical and efficient land use, an improved level of amenities, appropriate and harmonious variety, creative design, conservation of resources, and a better environment. Mixed-use development as proposed within Westmoore Landing would increase convenience, enhanced walkability, reduced transportation needs, fosters a sense of community, boosts local economies, and promote environmental sustainability. In addition, the proposed land uses are those that would have been allowed if the subject property had been developed under the current R2B and C4 zoning districts. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

### 13. Frontage Types:

- **Requirement:** The city's Zoning Ordinance provides that front porches as it is part of a building are not allowed to encroach on setbacks.
- **Modifications:** Frontage Types are modified to meet the standards defined for each Transect and Building Type and identified in the Frontage Type Standards and associated Supplementary Design Standards. Covered porches having a roof, bay windows, and other architectural elements may encroach within the yard setbacks per the Frontage Type Standards. Temporary Uses have been expanded to include temporary activities as outlined within the definitions.
- **Justification:** Proposed modification stated above sets minimum sizes for usable frontage types that encourage outdoor living and social interaction. Allowing encroachment encourages construction as it doesn't consume potential building area. The expanded definition of temporary uses accommodates activities not within the City of Huntsville Zoning Ordinance.
- **Analysis and Recommendation** – The intent of the proposed development by locating structural elements closer to the streets is to increase walkability and foot traffic. Economically, this design has been strongly linked to increased commercial property values and rent. The increase in foot traffic allows for more retail sales opportunities, which encourages local businesses to thrive and in turn results in a reduction in commercial vacancies. This design as it promotes walkability reduces the reliance on cars, which resolves associated problems of traffic and parking congestion. Walkability improves health by integrating physical activities into daily routines and enhances social interaction by providing spaces for social interaction and community engagement. Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD,

contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

#### 14. General Application of Standards:

- **Requirement:** The PD district regulations within the city's Zoning Ordinance require that when a PD is to be constructed in stages, the nature and timing of each stage must be indicated. Underlying base zoning districts for each Transect Community Type apply to any standards, regulations, and requirements not specifically shown in the Westmoore Landing PD Submittal Packet or included as a condition of Commission or Council approval.
- **Modifications:** A Phasing Plan for development is not required as part of the PD submittal packet.
- **Justification:** An overall development timeline of 30+ years makes accurately predicting a phasing plan nearly impossible.
- **Analysis and Recommendation** – The PD regulations provides that where development is to be in stages, nature and timing of each stage shall be indicated. The purpose of a phasing plan is to manage the rate and sequence of construction to ensure that public infrastructure and services keep pace with private development. As stated in the modification request, the proposed PD development is projected to progress for more than 30 years. Considering the extent of the site and proposed development, this timeline would still be comparable to the progress of typical residential neighborhoods and large-scale commercial developments. The variability in the housing and financial market also would have to be accounted for, which in this current environment compounded by the fact that this proposed PD has not yet received approval, it would not be reasonable to provide timing of each construction stage. The Zoning Ordinance further provides though that the final plan for the first stage be provided within ninety (90) days after Council approval of the PD-H rezoning. This timeline would complement the subsequent steps this proposed development would have to take after its approval. This has been typically the case and would be a more acceptable timeline.

For these reasons the PD regulations would not serve public purposes to a degree at least equivalent to the generally applicable requirements that do not require phasing plans.

Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

#### 15. Vehicular Access to Streets:

- **Requirement:** The PD-H district regulations within the city's Zoning Ordinance require that if the street or portion thereof serves fifty (50) or fewer dwelling or

lodging units, vehicular access from off-street parking and service areas may be directly to the street from the sites of individual dwelling or lodging units. Determination of number of units served shall be based on normal routes of traffic anticipated in the development.

- **Modifications:** The Westmoore Landing PD-H street network shall all be considered as local streets. Outside traffic will be encouraged on the local street sections labeled as mixed-use, boulevard, and collector per the Westmoore Landing PD-H street network plan. Streets serving over 50 dwelling units may have vehicular access to off-street parking directly to the street from the sites of individual dwelling units, provided the buildings follow the building type standards per transect.
- **Justification:** All streets have been reviewed by Schoel Engineering and Skipper Consulting (Traffic Engineering) on their functionality and safety. The adjacent properties that the street network leads to cannot be developed further, so streets would not need to accommodate significant additional traffic. The majority of lots will have vehicular access from alleys, with front-loaded products occurring where alleys are not required.
- **Analysis and Recommendation** – The proposed Westmoore Landing design aims to increase convenience, enhanced walkability, reduced transportation needs, foster a sense of community, boost local economies, and promote environmental sustainability. This design proposed by the PD intends to establish human-scale proximity, which would promote comfortable and safe distances between buildings for easily accessible range for pedestrians to navigate daily. This will also improve walkability and increase visibility and accessibility of commercial uses and employment sites within the proposed PD. The proposed modification combined with the overall design of the development shall meet the intent of existing standard and regulation at a higher degree as these will promote economical and efficient land use, an improved level of amenities, appropriate and harmonious variety, creative design, conservation of resources, and a better environment.

In this specific instance, the PD regulations would not serve public purposes to a degree at least equivalent to the generally applicable requirements that would allow access on streets serving more than 50 dwelling units.

Approval of the proposed amendments to the existing regulations is recommended, which shall be part of the regulations adapted to such unified planning within the proposed PD, contingent upon the project's continued and complete compliance with the approved overarching PD Submittal Packet.

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DOCUMENTS SUBMITTED TO THE PLANNING COMMISSION IN  
COMPLIANCE WITH ZONING ORDINANCE REGULATIONS

Vicinity/Zoning Map  
Illustrations of Typical Building Designs  
Boundary and Topographic Survey  
Unified Ownership Document

Street Trees and Planting Standards  
Physical Features Map  
Traffic Impact Study  
Chickasaw Plum Assessment Report

*Notwithstanding anything to the contrary contained in the Planned Development Submittal Packet as listed above (collectively “Submittals”), the provisions of this report shall apply. In the case of a conflict between the Submittals and this Report, this Report shall apply.*

*Applicable laws and regulations that are not amended by the enactment of the PD-H zoning district for Westmoore Landing shall remain in full force and shall apply to the development.*

*Situations not covered by the adopted ordinance and supplemental documents shall be regulated by appropriate existing Huntsville ordinances and regulations.*

*Failure to comply with or otherwise fail to satisfy the conditions under which approval is granted for the PD-H rezoning may be grounds for actions as provided by Section 30.6 of the Zoning Ordinance.*



# WESTMOORE

— LANDING —

**PD-H AMENDMENT SUBMITTAL**

SUBMITTAL DATE: 11/07/2025



# CREDITS

## DEVELOPERS

Rochford Land Innovations, LLC

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## SPECIAL THANKS TO

City of Huntsville  
Town of Mooresville  
Wheeler Wildlife Refuge



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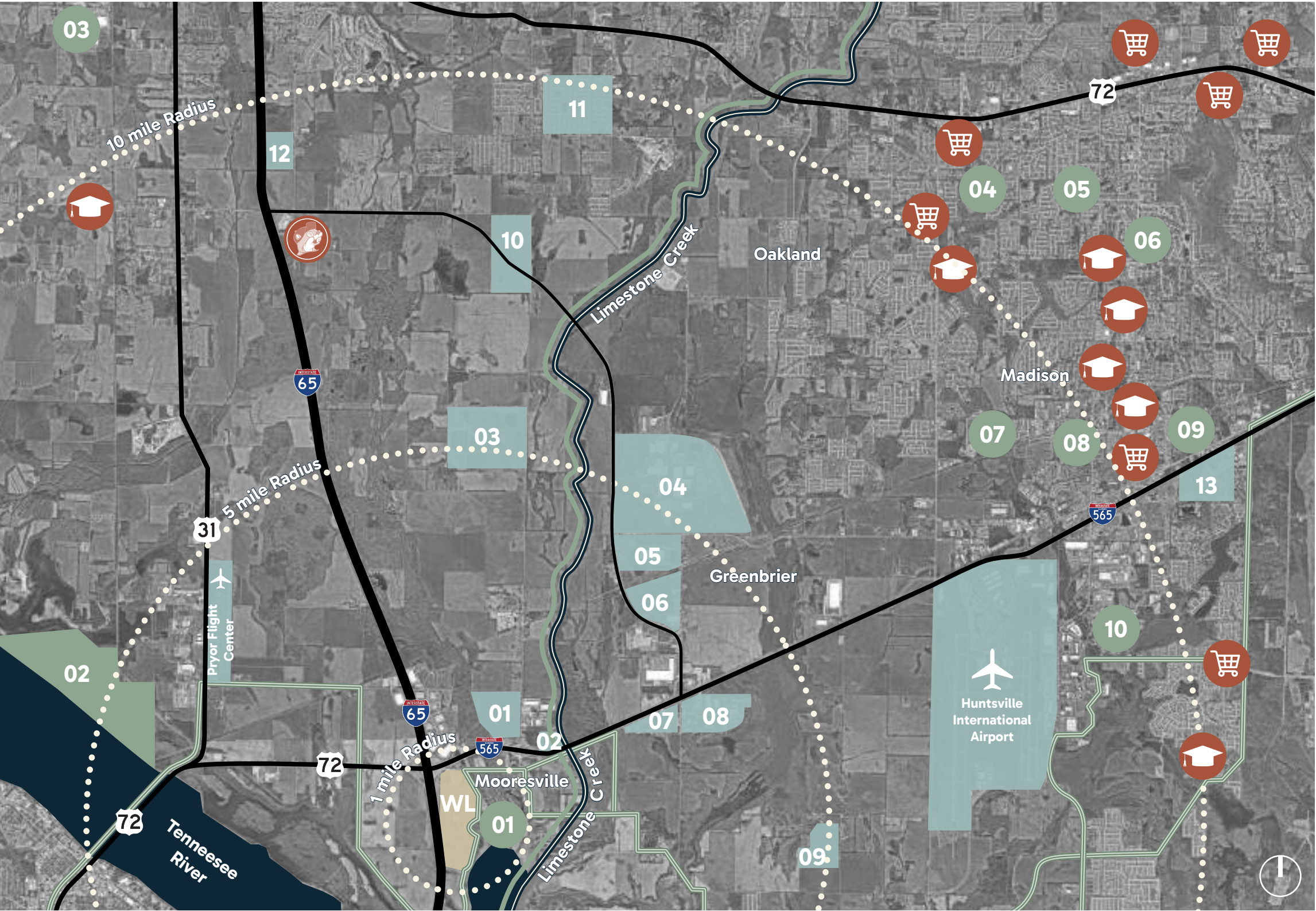


# INTRODUCTION

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# CONTEXT ANALYSIS



Other Developments	
01	Future Development
02	Limestone Creek Apartments
03	Tennessee Valley Research
04	Mazda Toyota Manufacturing
05	Amazon Fulfillment
06	Huntsville West Industrial Park
07	Greenbrier Patch
08	Chase Farms at Greenbrier
09	Greenbrier Preserve
10	Greenbrier Town Square
11	Legacy Grove
12	Anderson Farm
13	Town Madison

Parks & Greenspace	
	Limestone Creek Trail
	Singing River Trail
01	Wheeler Wildlife Refuge / Limestone Bay
02	Swan Creek Wildlife Management Area
03	Robert Allen Tinnon Park
04	Joe Phillips & Boston Harbor Park
05	Brass Oak Park
06	Leathertree Park
07	Palmer Park
08	Governors Estates Park
09	Westgate Park
10	Dallas W. Fanning Nature Preserve



# FRAMEWORK

The concept for Westmoore Landing draws inspiration from the natural structure and function of a river delta, an essential element of North Alabama’s ecosystem, particularly evident around the Wheeler Wildlife Refuge. River deltas, such as those formed by the Tennessee River, create dynamic networks where waterways branch into smaller streams, fostering rich and diverse habitats that sustain abundant wildlife and lush vegetation. Similarly, the development draws from traditional town planning practices that have historically created comfortable, familiar environments in small southern towns, known for their warmth, hospitality, and enduring community spirit.

This organic pattern of connectivity and flow inherent in river deltas provides the foundational framework for Westmoore Landing’s civic and urban planning design. By overlaying ecological principles onto human-scale town planning, the development thoughtfully integrates open and outdoor spaces with pedestrian-friendly streets, accessible public areas, and inviting communal spaces. This integration ensures ecological patterns influence not only the natural landscape but also the arrangement, movement, and interactions within the built environment, promoting both sustainability and genuine community engagement.

Central to this vision is the hierarchy of primary, secondary, and tertiary connections inspired by the branching nature of a river delta, guiding the design of spaces, pathways, and community interactions. These connections enhance walkability, foster social interactions, and encourage a sense of belonging, reminiscent of traditional southern towns. Elements such as front porches, town squares, community parks, and small-scale retail spaces are strategically incorporated to nurture frequent social interactions and meaningful community ties.

Through this intentional blending of ecological design principles and traditional southern town-planning concepts, Westmoore Landing aims to revive the authenticity of neighborhood life. Residents will find themselves naturally drawn to gather, engage in meaningful conversations, and build lasting relationships, cultivating an atmosphere where true community spirit thrives and neighbors genuinely know and care for one another. The result is a harmonious, vibrant, and interconnected community, deeply reflective of its natural surroundings and rich southern heritage.





# PRIMARY CONNECTIVITY

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The primary connections act as the main arteries of the development, much like a river's main channel, guiding the primary movement through Westmoore Landing. These connections provide essential access to homes, community amenities, and key gathering spaces, much like how a river delivers water to the surrounding landscape.





# PRIMARY CONNECTIVITY - RIVERS OF MOVEMENT

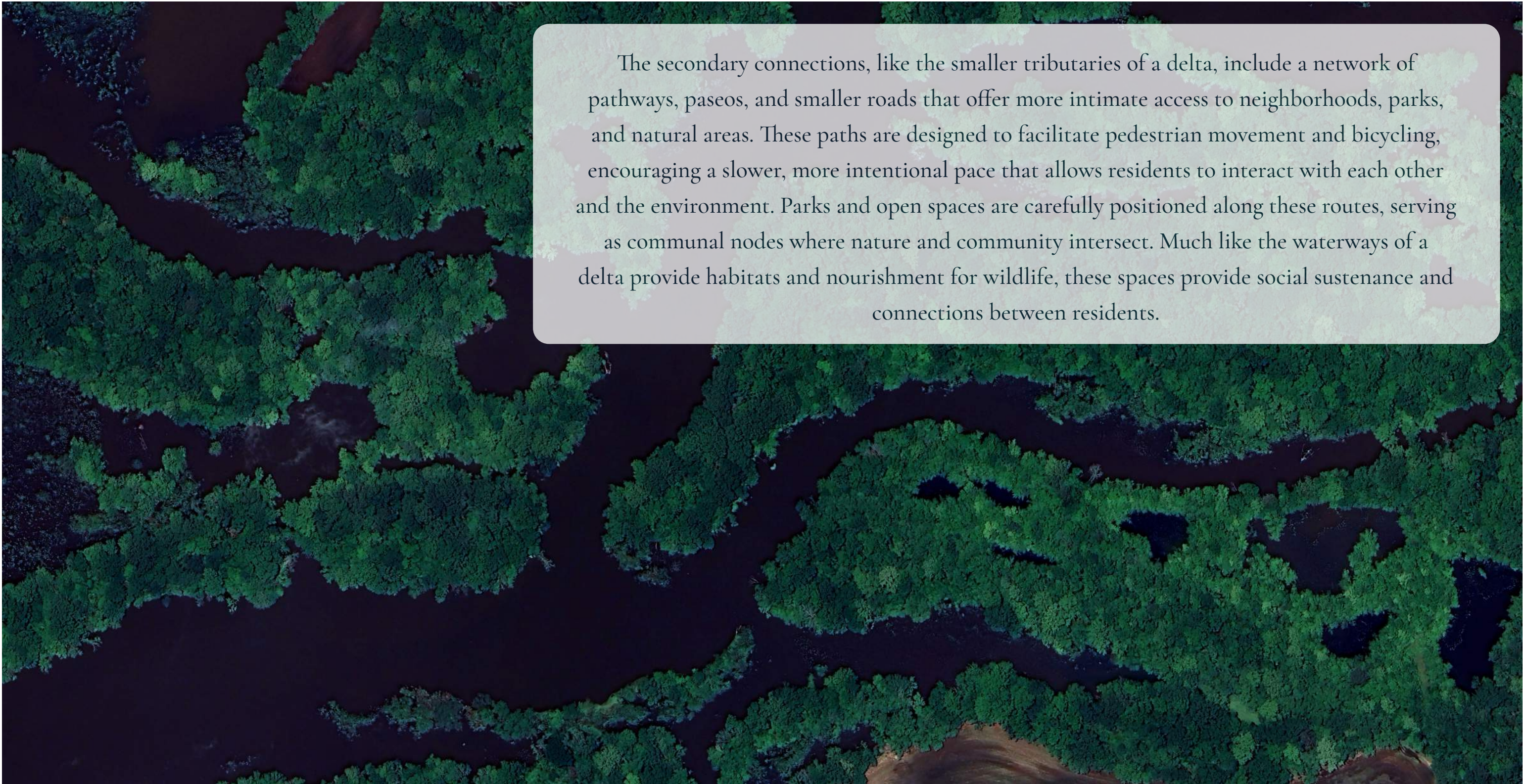
Primary connections are the main thoroughfares, designed for higher traffic and connecting key community areas. They likely include wider pedestrian walkways, bike paths, or even roads, connecting key areas like neighborhoods, commercial centers, and main parks. In Westmoore Landing, the primary connections will also serve as stormwater conveyance to the Wheeler Refuge.





# SECONDARY CONNECTIVITY

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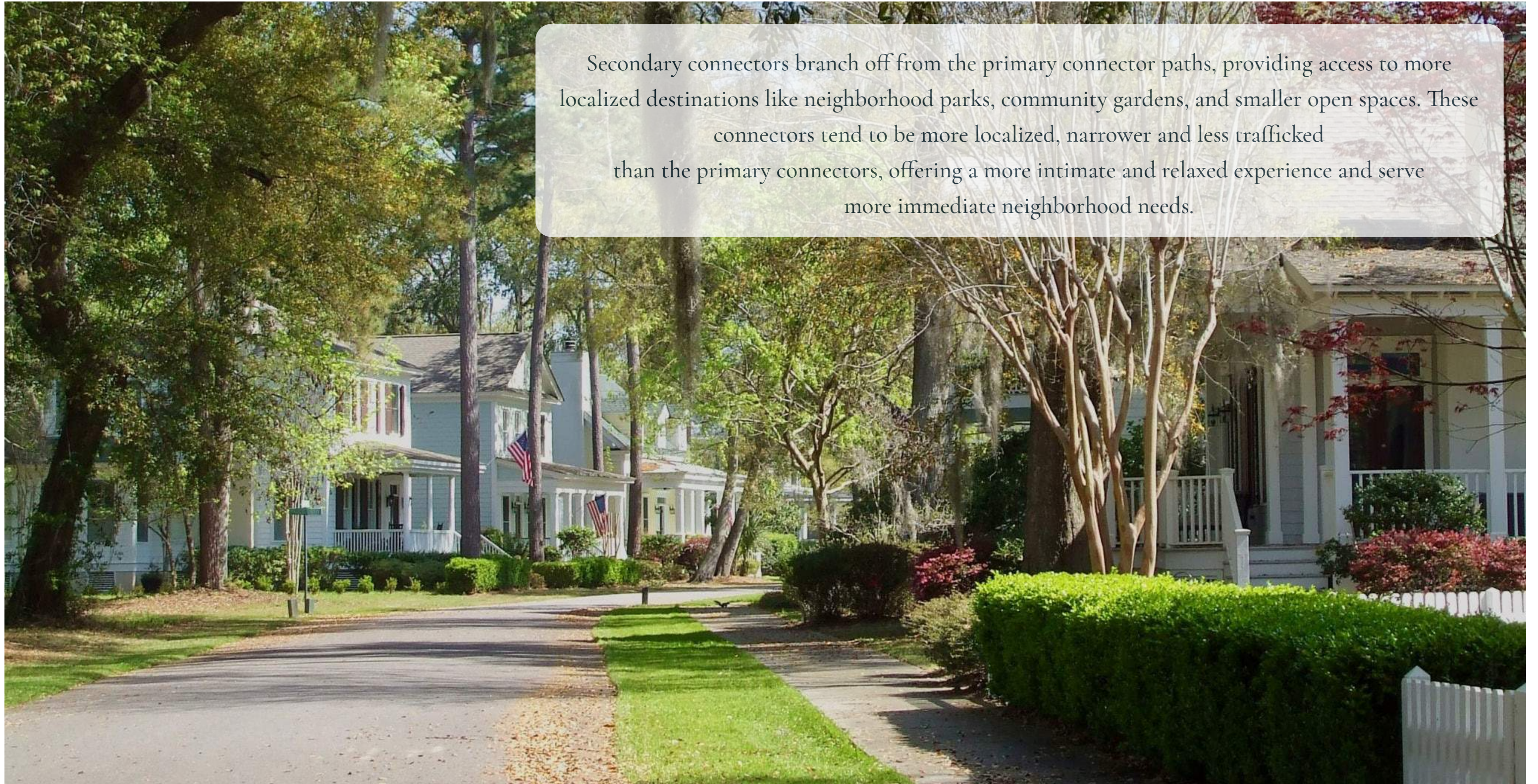
An aerial photograph of a river delta, showing a complex network of waterways branching out into a dense forest. The water is a dark, rich brown, contrasting with the vibrant green of the trees. The branching pattern of the waterways is reminiscent of a tree or a neural network. A semi-transparent white rectangular box with rounded corners is positioned in the upper right quadrant of the image, containing text.

The secondary connections, like the smaller tributaries of a delta, include a network of pathways, paseos, and smaller roads that offer more intimate access to neighborhoods, parks, and natural areas. These paths are designed to facilitate pedestrian movement and bicycling, encouraging a slower, more intentional pace that allows residents to interact with each other and the environment. Parks and open spaces are carefully positioned along these routes, serving as communal nodes where nature and community intersect. Much like the waterways of a delta provide habitats and nourishment for wildlife, these spaces provide social sustenance and connections between residents.



# SECONDARY CONNECTIVITY - STREAM CONNECTORS

Secondary connectors branch off from the primary connector paths, providing access to more localized destinations like neighborhood parks, community gardens, and smaller open spaces. These connectors tend to be more localized, narrower and less trafficked than the primary connectors, offering a more intimate and relaxed experience and serve more immediate neighborhood needs.



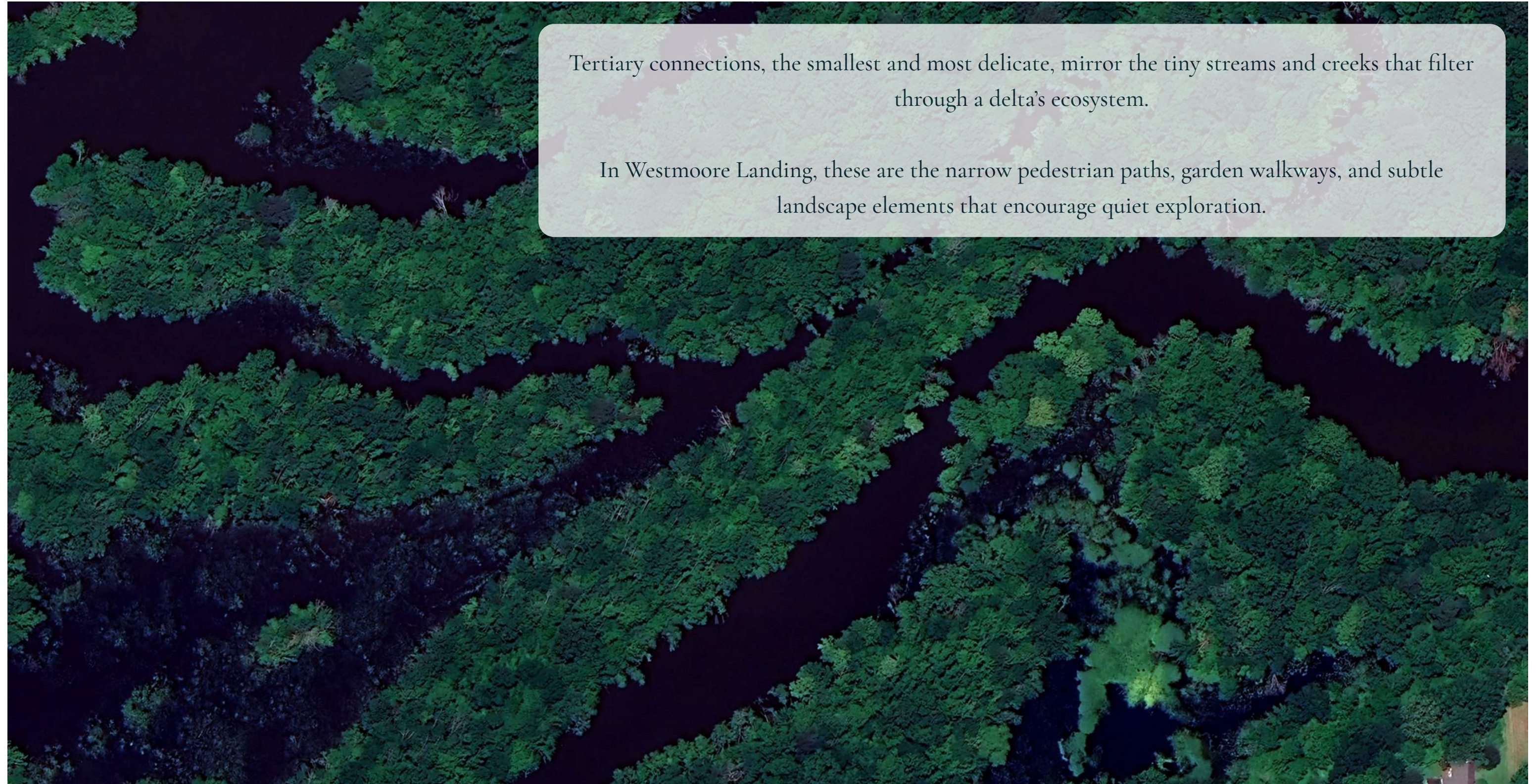


# TERTIARY CONNECTIVITY

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Tertiary connections, the smallest and most delicate, mirror the tiny streams and creeks that filter through a delta's ecosystem.

In Westmoore Landing, these are the narrow pedestrian paths, garden walkways, and subtle landscape elements that encourage quiet exploration.

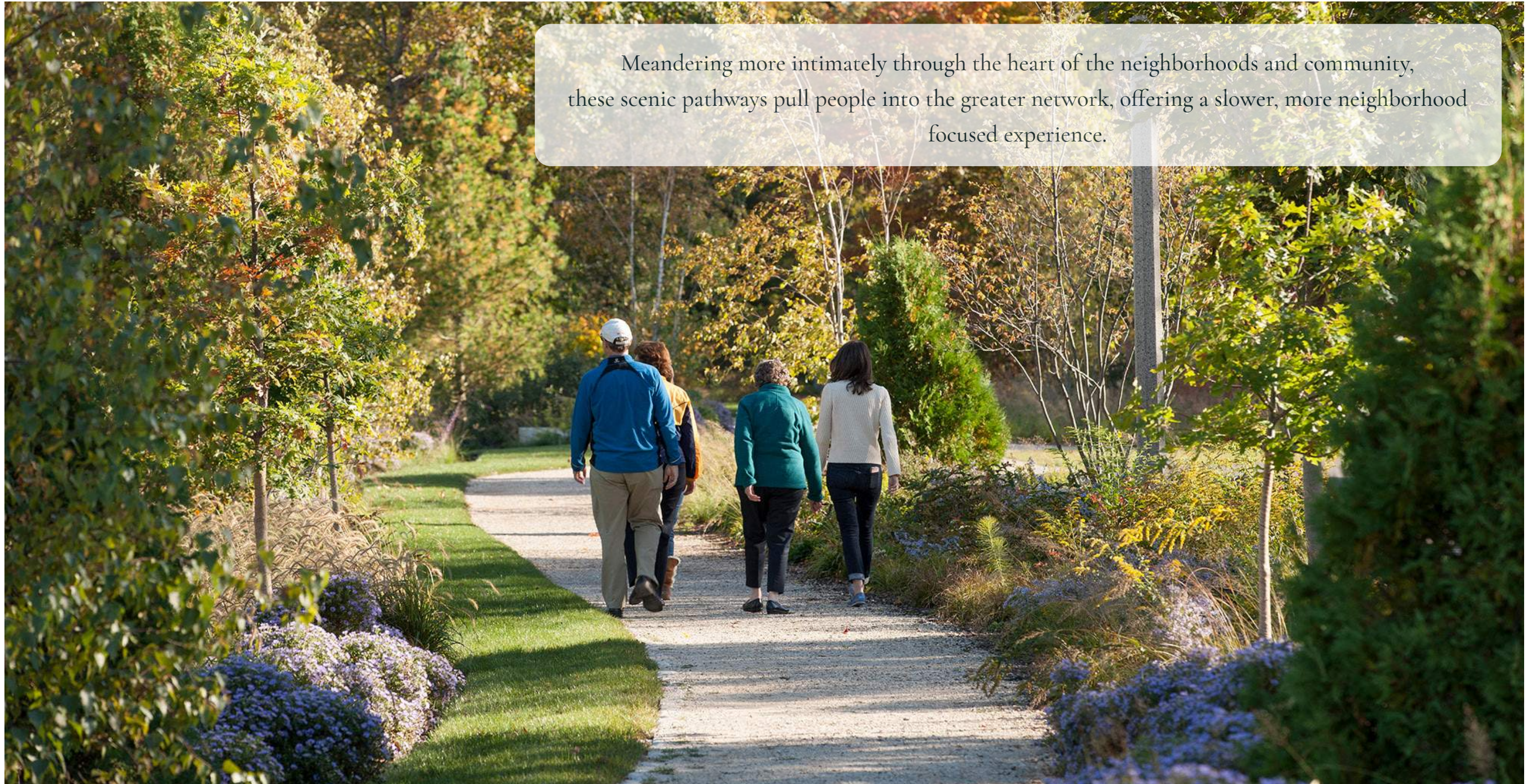




# TERTIARY CONNECTIVITY - CREEK PATHWAYS

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Meandering more intimately through the heart of the neighborhoods and community, these scenic pathways pull people into the greater network, offering a slower, more neighborhood focused experience.



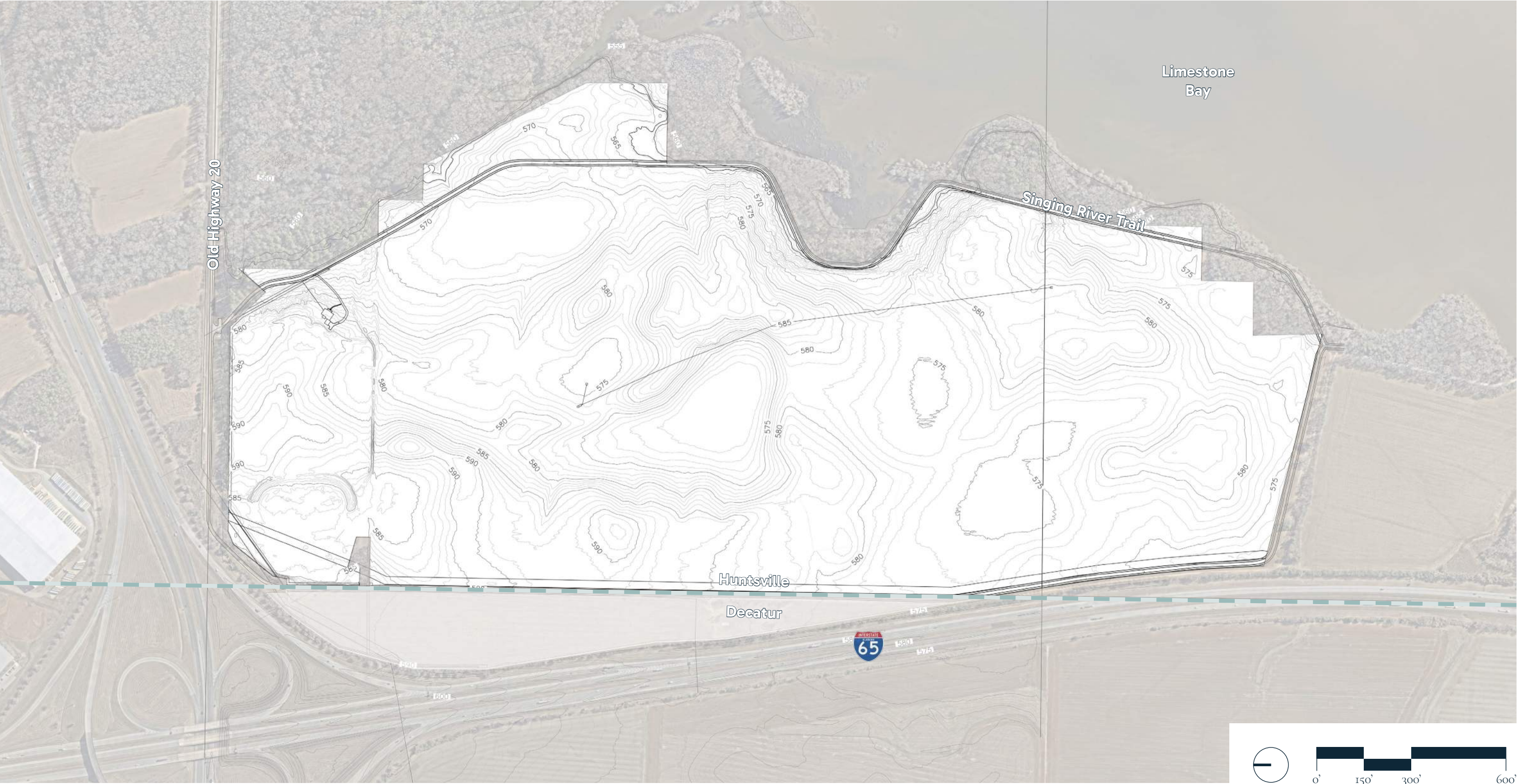


# CONCEPT DIAGRAM





# SURVEY





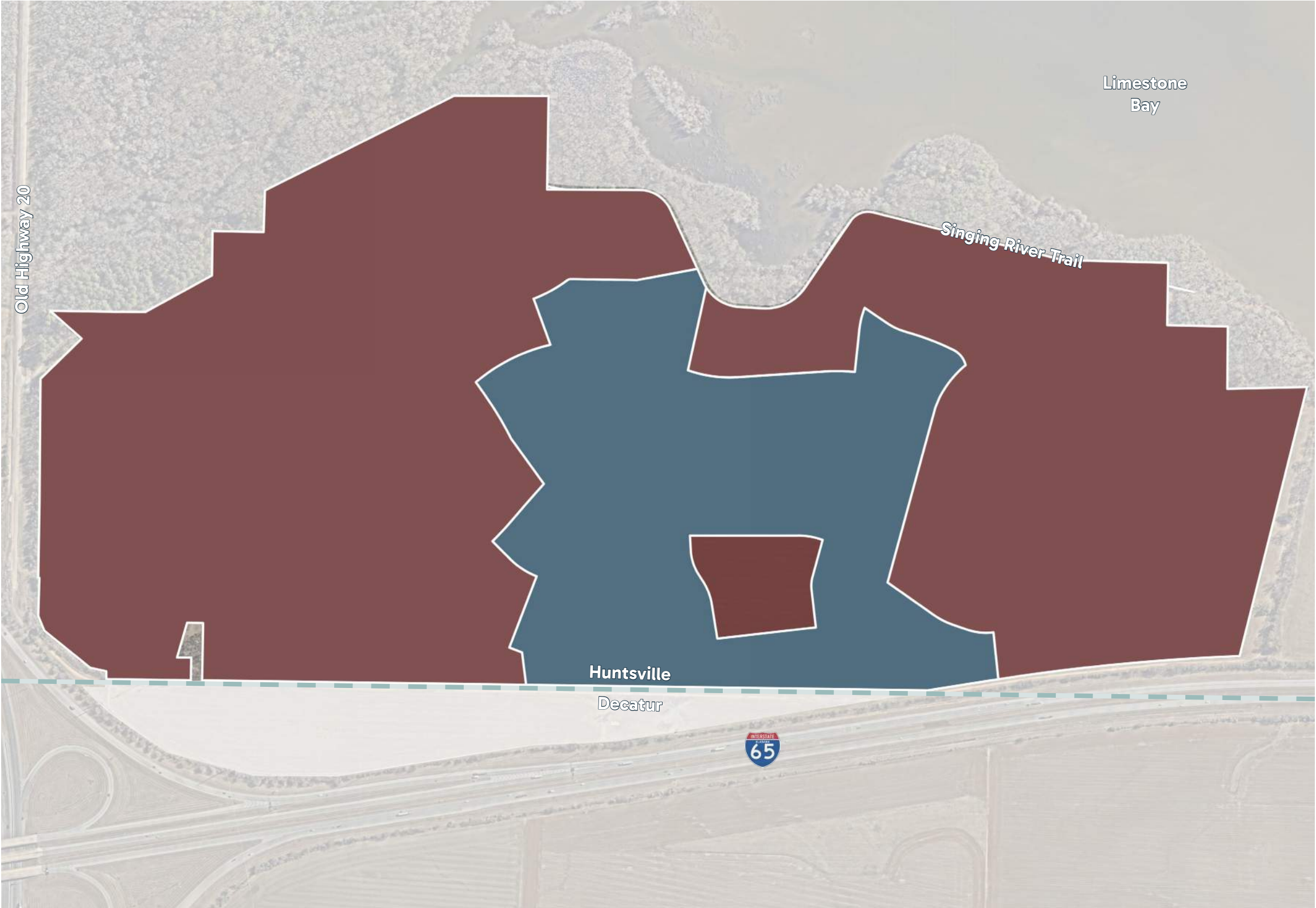
# REGULATORY PLAN

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# BASE ZONING PLAN



- Residence 2-B District  
282.85 acres
- Highway Business C-4 District  
98.08 acres

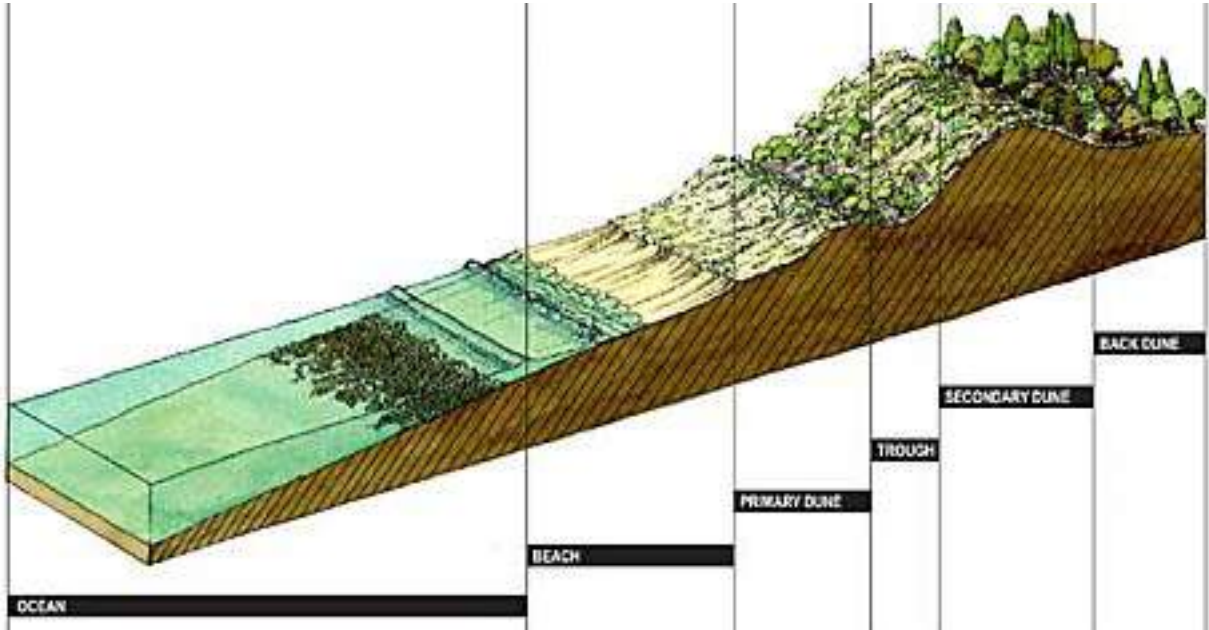


# TRANSECT INTENT

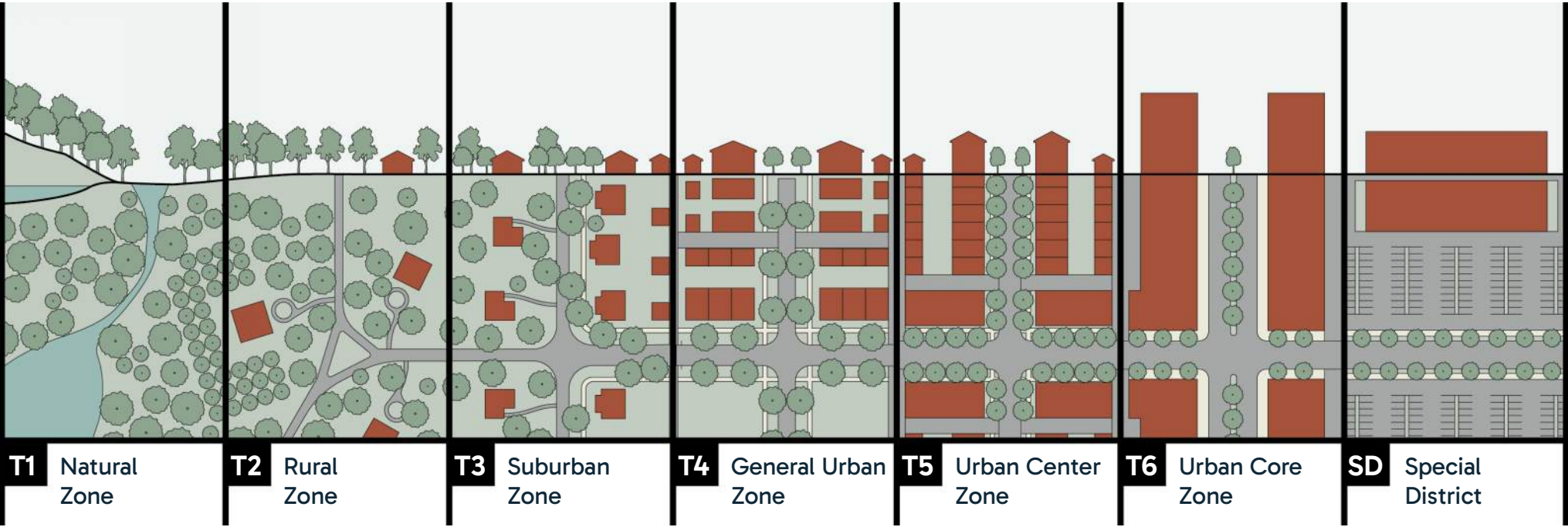
A transect of nature, first conceived by Alexander Von Humbolt at the close of the 18th century, is a geographical cross-section of a region intended to reveal a sequence of environments. Originally, it was used to analyze natural ecologies, showing varying characteristics through different zones such as shores, wetlands, plains, and uplands. It helps study the many elements that contribute to habitats where certain plants and animals thrive in symbiotic relationship to the minerals and micro climate.

The concept of Form-Based Code (also known as SmartCode) is a unified land development ordinance designed to guide urban planning and development based on the concept of the Transect—a continuum of zones ranging from natural and rural areas to dense urban centers. Unlike conventional zoning codes that emphasize land use separation, the SmartCode emphasizes urban form, walkability, mixed uses, and neighborhood design, aiming to create sustainable, vibrant communities with a clear relationship between built environments and their surroundings.

In simpler terms, the Form-Based Code helps cities and towns organize their growth by clearly defining how areas transition from rural to urban, ensuring each neighborhood fits harmoniously within its context.



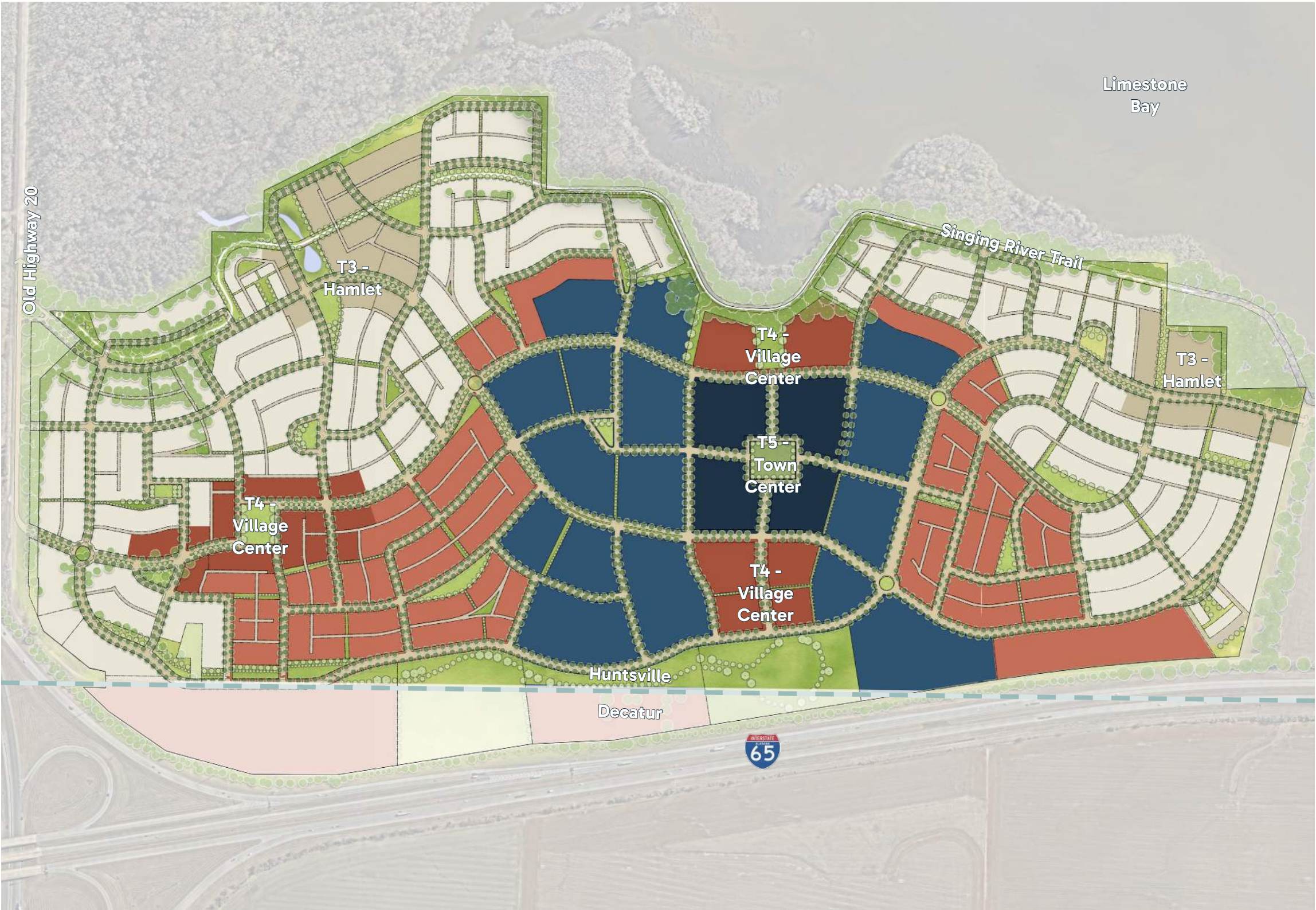
TYPICAL NATURAL TRANSECT



TYPICAL RURAL - URBAN TRANSECT, WITH TRANSECT ZONES

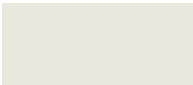







# DEVELOPMENT PLAN



The Development Plan for Westmoore Landing provides for a variety of residential lot types including a mix of single family homes, townhouses, and multi-family building types. The plan also includes a number of neighborhood nodes throughout the development that support mixed use product types.

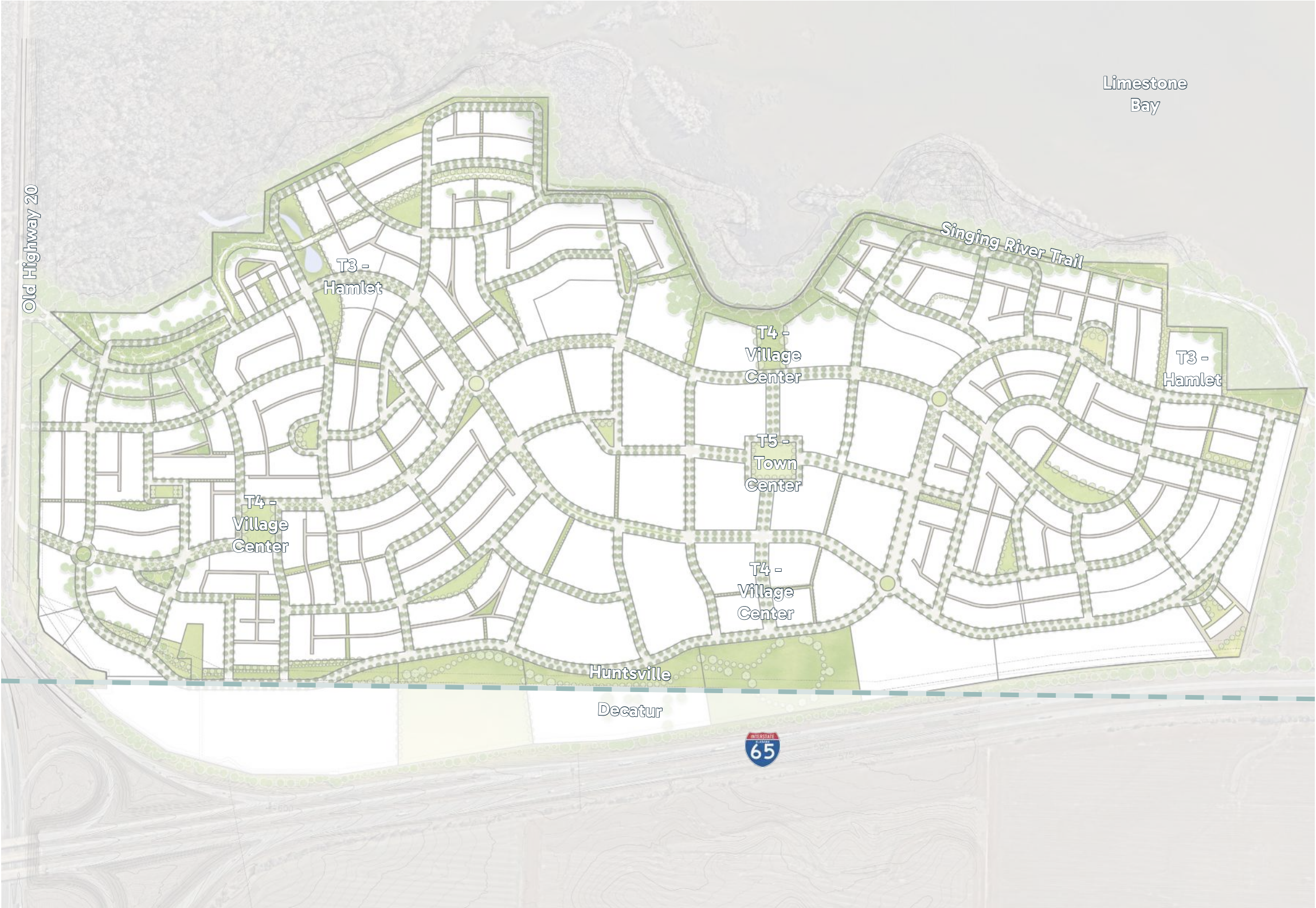
The plan's network of interconnected streets and residential blocks is derived from existing hydrological and topographical patterns of the land. The site provides open spaces of varying sizes for active and passive recreation, while taking into account the natural drainage of the land.

	T3 - Neighborhood 151.75 acres
	T3 - Hamlet 27.13 acres
	T4 - Neighborhood 75.56 acres
	T4 - Village 28.41 acres
	T5 - Neighborhood 80.98 acres
	T5 - Town Center 17.10 acres





# OPEN SPACE PLAN



The Open Space Plan outlines the distribution of defined public open spaces throughout the community, ensuring that residents and visitors alike have access to a variety of both programmed and unprogrammed open spaces of different scales. These open spaces are essential to enhancing the community’s quality of life by offering numerous benefits.

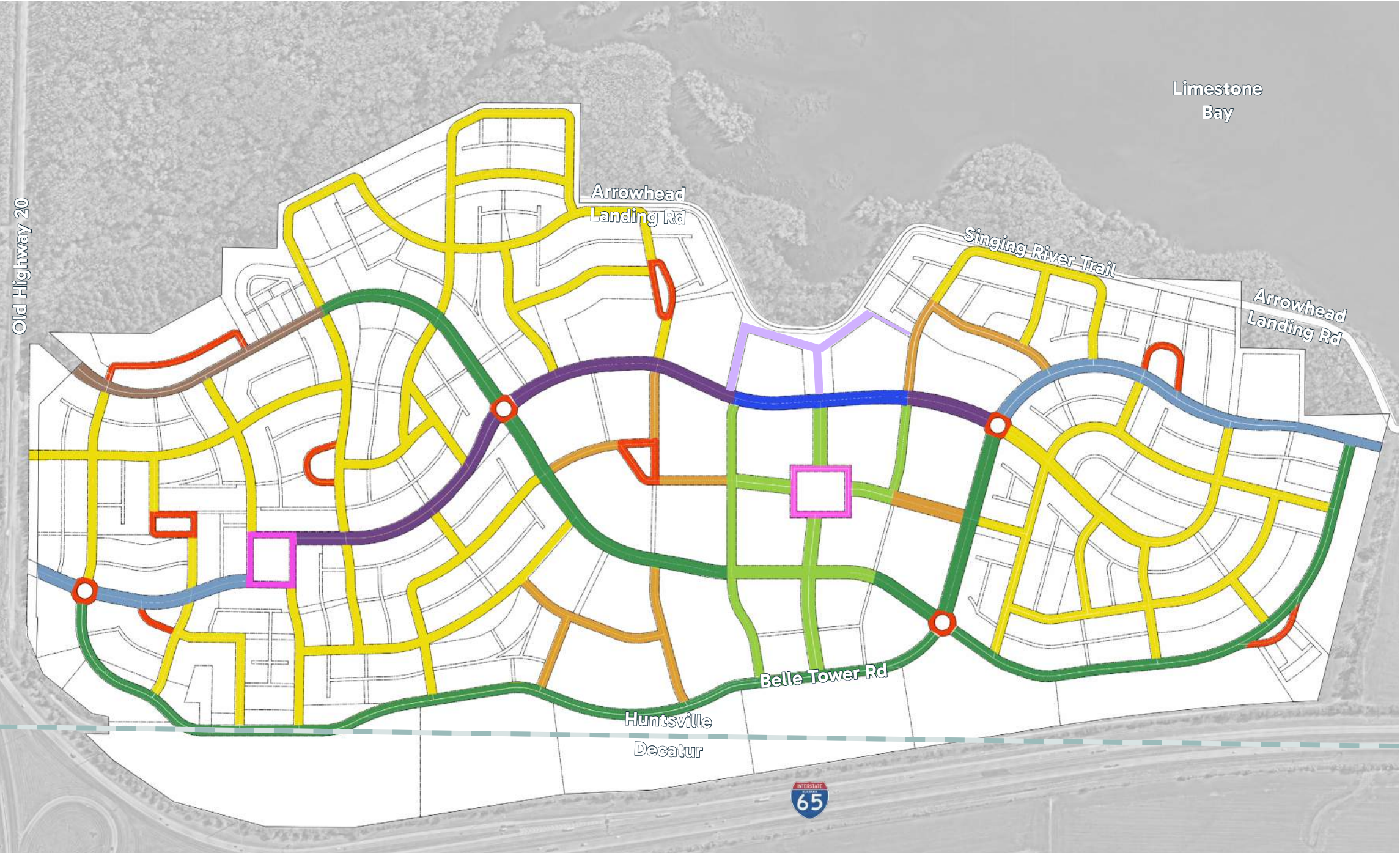
Open spaces provide a natural escape from the built environment and act as community hubs that encourage social interaction and foster a sense of belonging. These spaces will offer opportunities for recreation, exercise, and relaxation, supporting both physical and mental well-being. The open spaces are vital to promoting health, happiness, and community.

The Open Space Standards chapter provides information on the various open spaces and access for residents within their designated pedestrian shed, which is defined as a 1/4 mile radius. Within each pedestrian shed, open space amenities include a diverse range of gathering spaces to accommodate community events, leisurely evening walks, and a variety of outdoor activities.





# STREET NETWORK PLAN



- Mixed-Use with Greenway Trail - Dedicated Parking - 70' R.O.W.
- Mixed-Use - Dedicated Parking - 57' R.O.W.
- Mixed-Use One Way - Dedicated Parking - 30.5' R.O.W
- Mixed-Use Angled Parking - Dedicated Parking - 42' R.O.W.
- Neighborhood Boulevard with Greenway Trail - Unmarked Parking - 73' R.O.W.
- Neighborhood Boulevard - Unmarked Parking - 70' R.O.W.
- Neighborhood Primary Street - Unmarked Parking - 53' R.O.W.
- Neighborhood Secondary Street - Unmarked Parking - 49' R.O.W.
- Neighborhood One Way Street - Unmarked Parking - 32' R.O.W.
- Neighborhood Collector with Greenway Trail - Dedicated Parking - 60' R.O.W.
- Neighborhood Collector - Dedicated Parking - 57' R.O.W.

**NOTE:**  
Street section edge condition may vary where sidewalks are not required for parks and other open spaces.

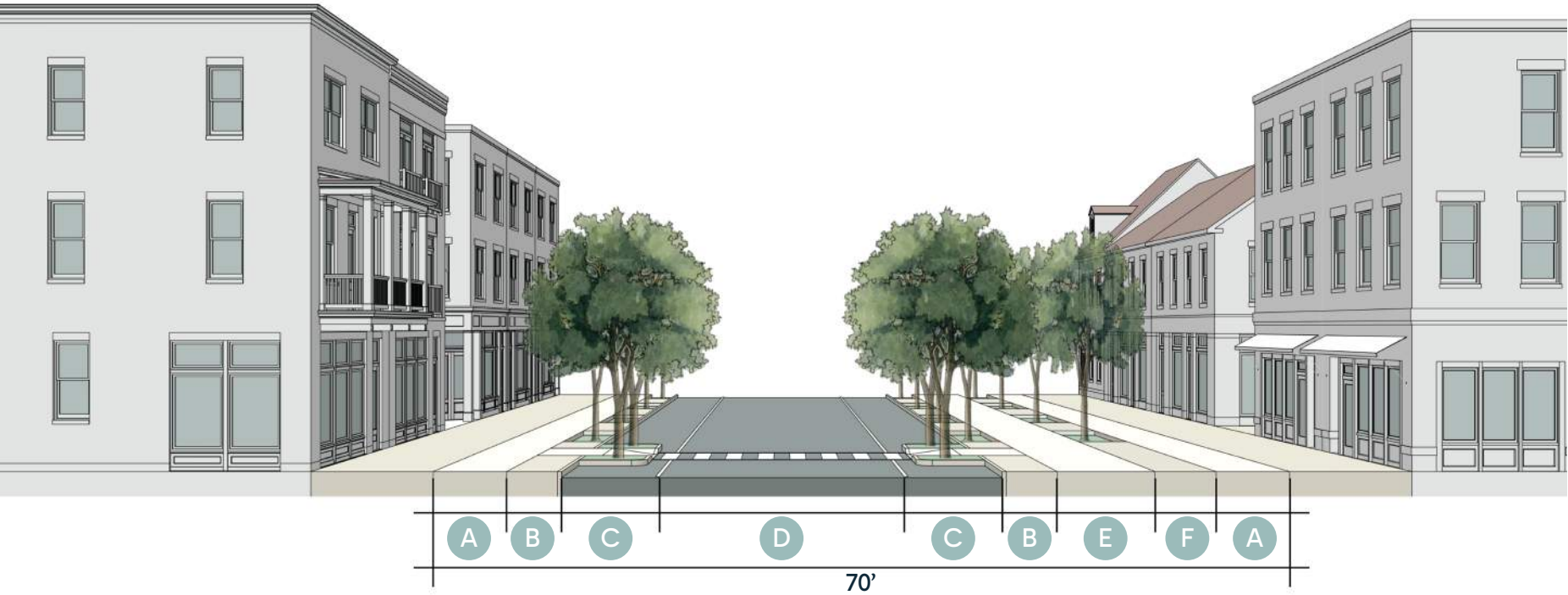
Dedicated access to Arrowhead Landing, TVA land, and the federal interstate bridges to the south of the project will be provided through Belle Tower Road that will be realigned and improved. The location of the Singing River Trail and Arrowhead Landing Road exists mostly outside the boundaries of the project; however, where this does exist within the property boundary , the developer will coordinate with the City of Huntsville and the Singing River Trail organization to determine the best alignment and routing through the project.



**NOTE:**  
Pedestrian bump outs are permitted once per block face. All intersections will meet regulatory and fire codes. Final street sections may vary from those depicted and described herein and, in addition to any other requirements, approval of all street construction documents is required by the Engineering Department and Fire Marshal and will be based on fire code compliance and adherence to City regulations. Where required by the fire code, approved aerial fire apparatus access roads shall be provided in compliance with the fire code.

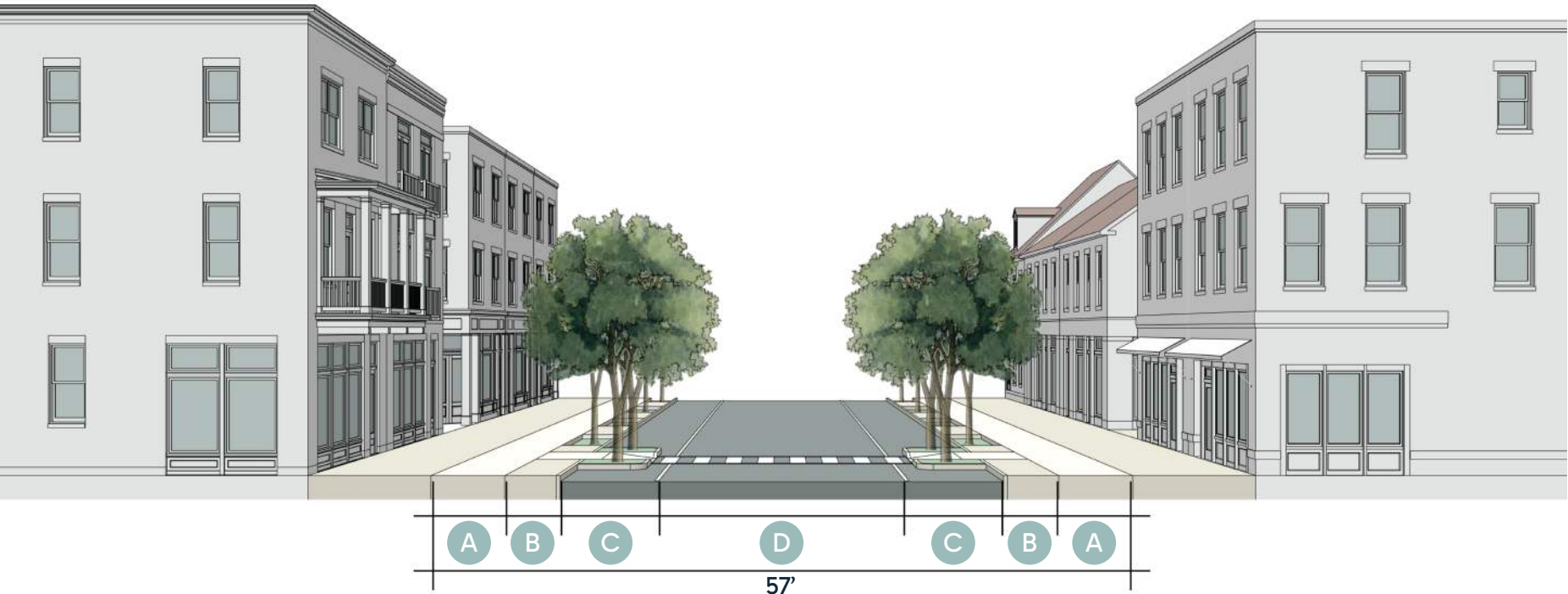


# STREET SECTIONS



**MIXED-USE WITH GREENWAY TRAIL  
DEDICATED PARKING - 70' R.O.W.**

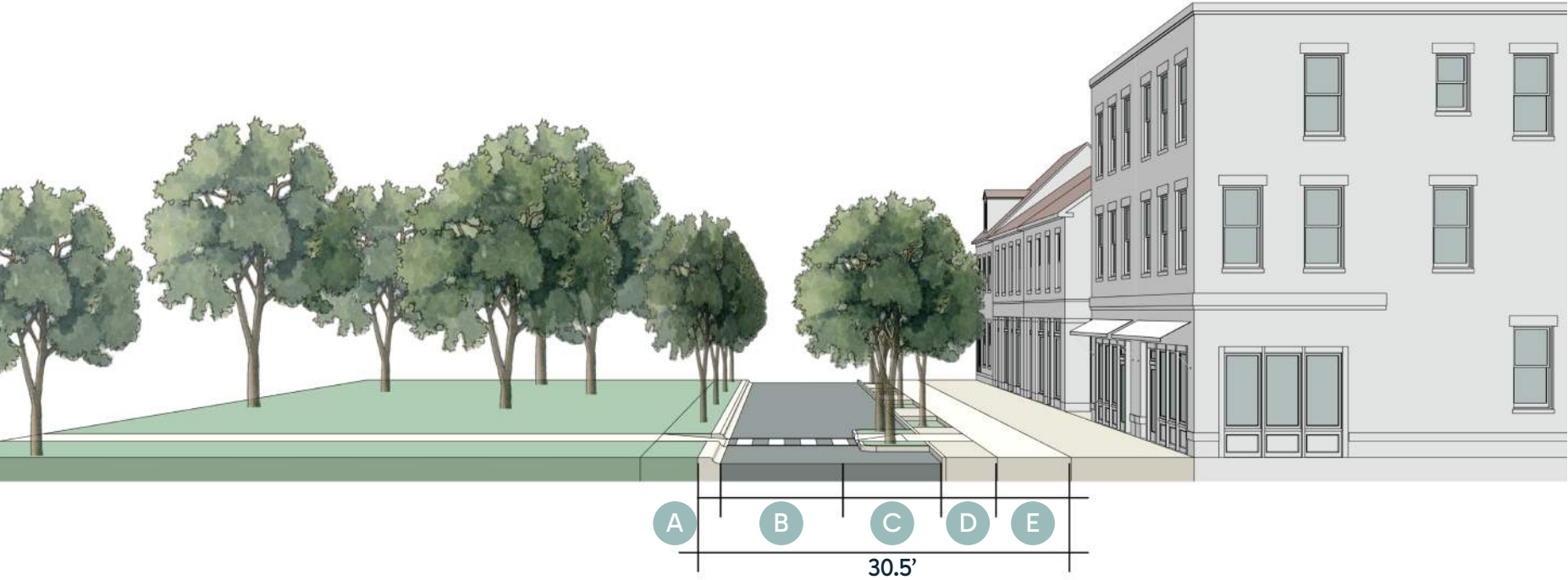
- A 6'-0" Sidewalk
- B 4'-6" Furnishing Zone
- C 8'-0" Parking
- D 20'-0" Pavement
- E 8'-0" Trail
- F 5'-0" Furnishing Zone



**MIXED-USE  
DEDICATED PARKING - 57' R.O.W.**

- A 6'-0" Sidewalk
- B 4'-6" Furnishing Zone
- C 8'-0" Parking
- D 20'-0" Pavement

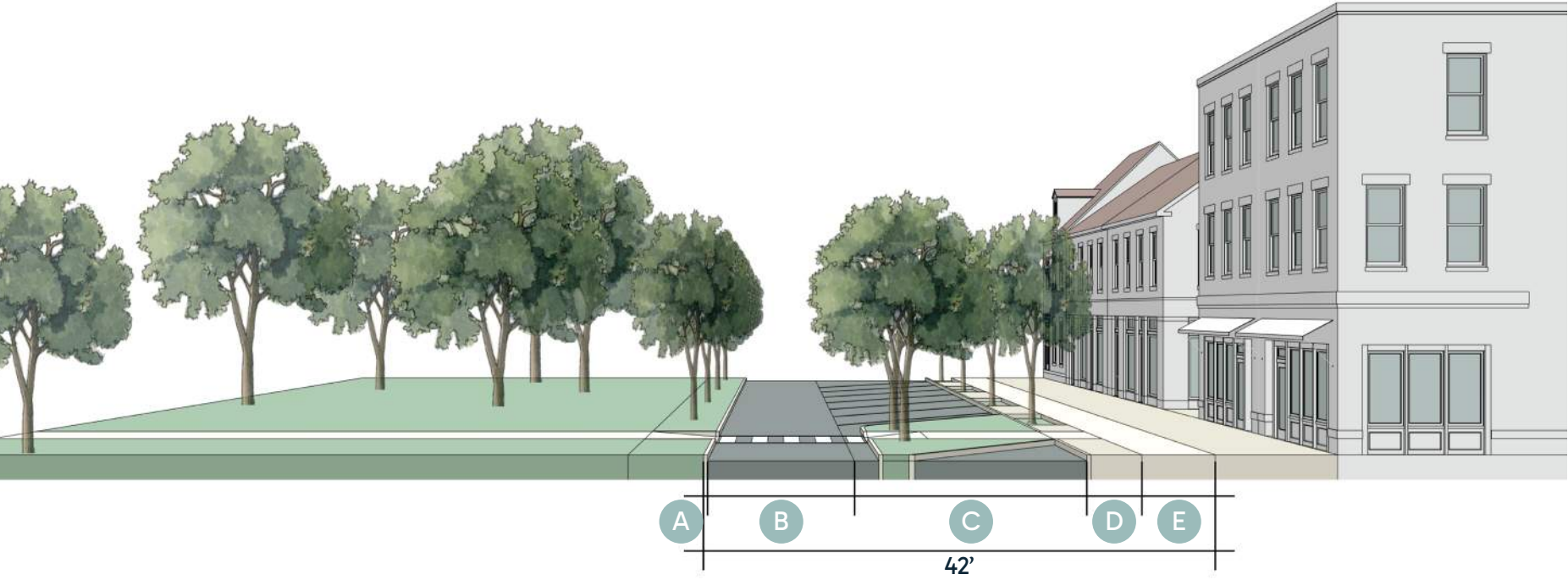
# STREET SECTIONS



## MIXED-USE - ONE WAY DEDICATED PARKING - 30.5' R.O.W.

- A 2'-0" Mountable Curb
- B 10'-0" Pavement
- C 8'-0" Parking
- D 4'-6" Furnishing Zone
- E 6'-0" Sidewalk

**NOTES:**  
Not permissible for fire apparatus access unless item B is widened to meet City of Huntsville Engineering and fire code requirements.  
Minimum 12' clearance is required between travel lane curb & pedestrian bump out.



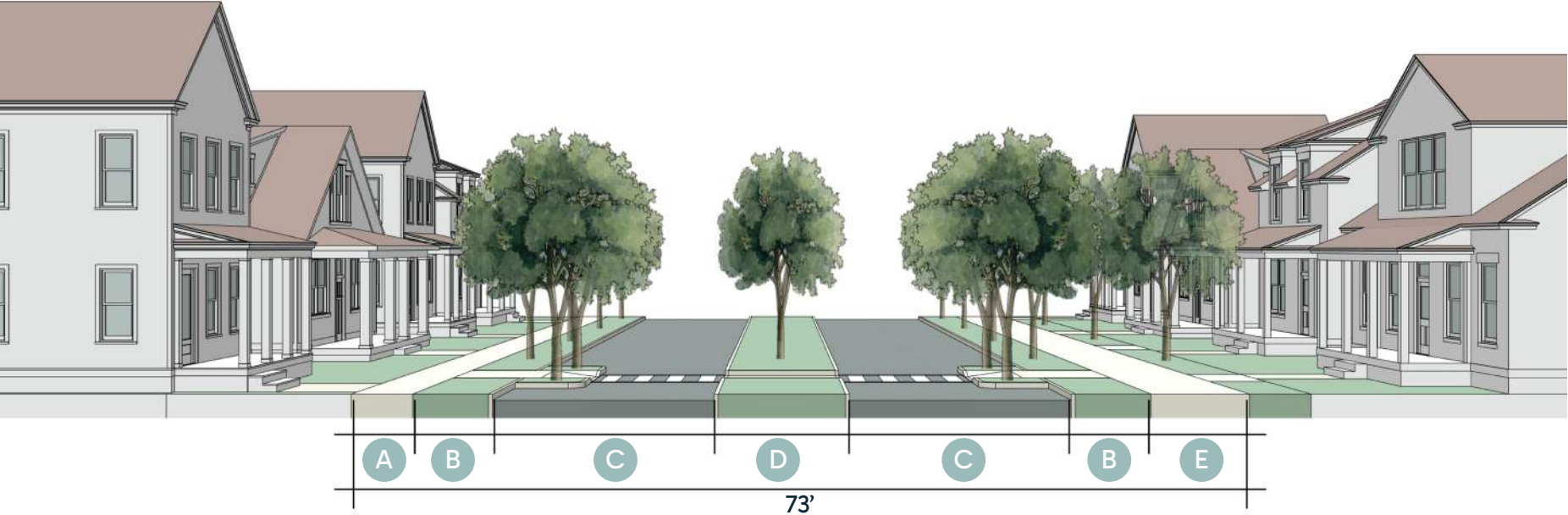
## MIXED-USE - ANGLED PARKING DEDICATED PARKING - 42' R.O.W.

- A 0'-6" Curb
- B 12'-0" Pavement
- C 19'-0" Angled Parking
- D 4'-6" Furnishing Zone
- E 6'-0" Sidewalk

**NOTES:**  
Not permissible for fire apparatus access unless item B is widened to meet City of Huntsville Engineering and fire code requirements.  
Minimum 12' clearance is required between travel lane curb & pedestrian bump out.



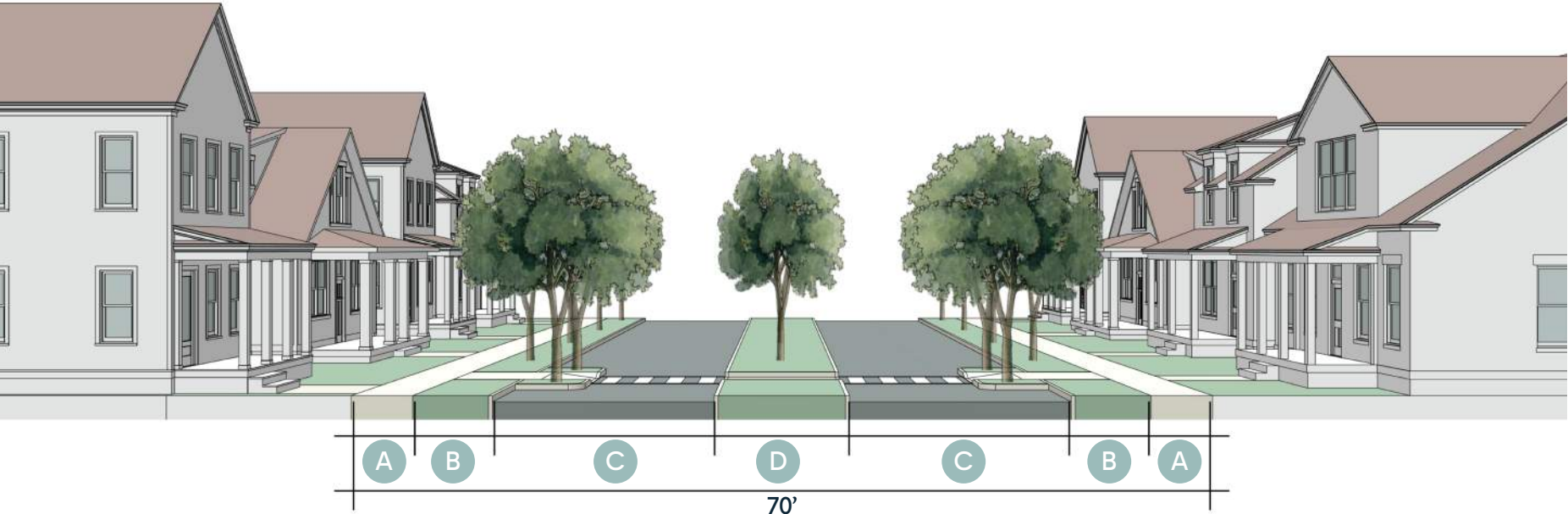
# STREET SECTIONS



**NEIGHBORHOOD BOULEVARD WITH GREENWAY TRAIL**  
**UNMARKED PARKING - 73' R.O.W.**  
(Flush Ribbon Curb as an alternative)

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 18'-0" Pavement
- D 11'-0" Planting Strip
- E 8'-0" Sidewalk

**NOTES:**  
Dimension for item C is permissible if the median is traversable, otherwise the pavement width must be increased to 20'.  
Minimum 12' clearance is required between travel lane curb & pedestrian bump out.



**NEIGHBORHOOD BOULEVARD**  
**UNMARKED PARKING - 70' R.O.W.**  
(Flush Ribbon Curb as an alternative)

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 18'-0" Pavement
- D 11'-0" Planting Strip

**NOTES:**  
Dimension for item C is permissible if the median is traversable, otherwise the pavement width must be increased to 20'.  
Minimum 12' clearance is required between travel lane curb & pedestrian bump out.

# STREET SECTIONS



**NEIGHBORHOOD PRIMARY STREET**  
**UNMARKED PARKING - 53' R.O.W.**  
(Flush Ribbon Curb as an alternative)

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 30'-0" Pavement

**NOTE:**  
Minimum of 18' clearance between pedestrian bump outs.



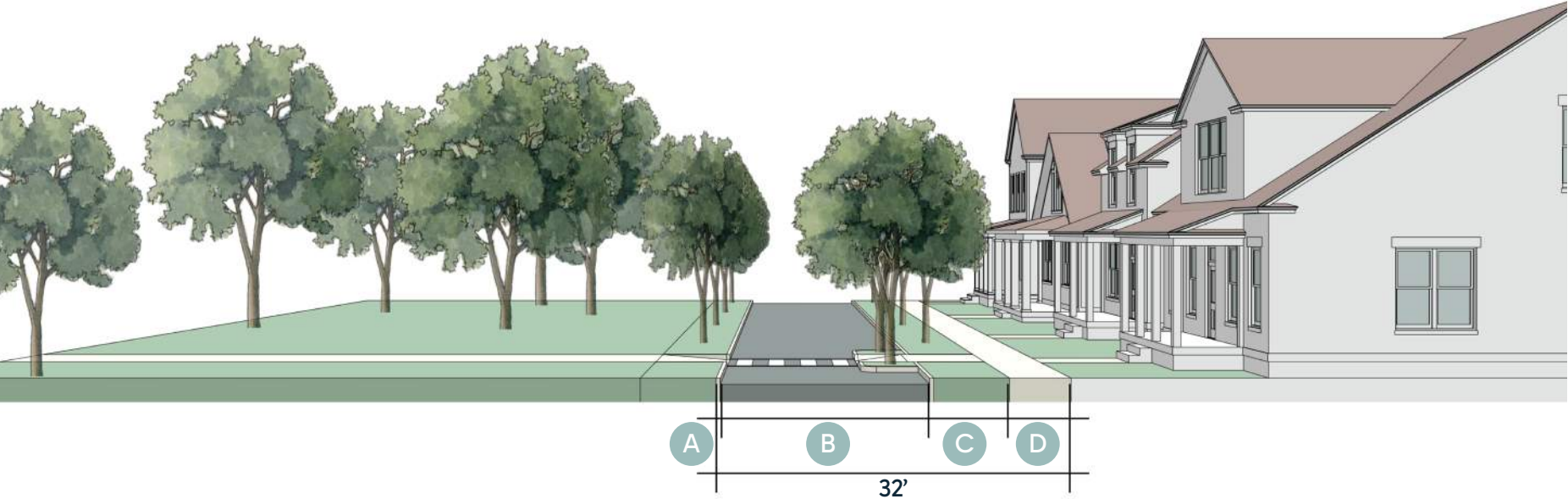
**NEIGHBORHOOD SECONDARY STREET**  
**UNMARKED PARKING - 49' R.O.W.**  
(Flush Ribbon Curb as an alternative)

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 26'-0" Pavement

**NOTE:**  
Minimum of 16' clearance between pedestrian bump outs.



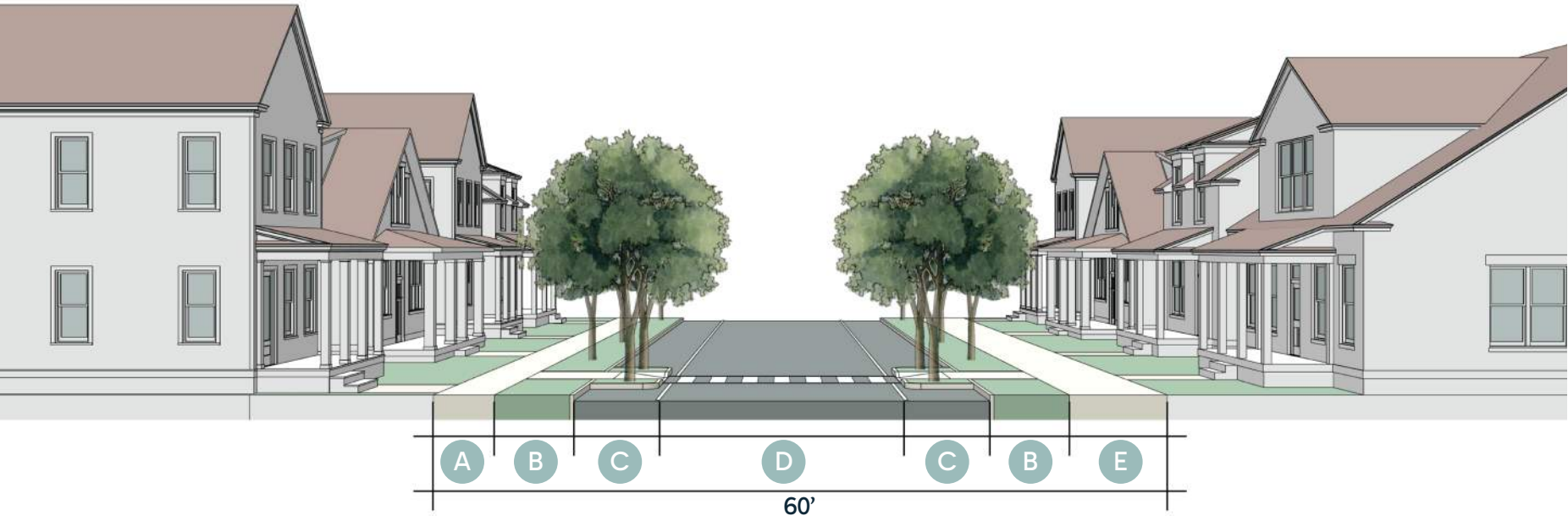
# STREET SECTIONS



**NEIGHBORHOOD - ONE WAY**  
**UNMARKED PARKING - 32' R.O.W.**  
(Flush Ribbon Curb as an alternative)

- A 0'-6" Curb
- B 20'-0" Pavement
- C 6'-6" Planting Strip
- D 5'-0" Sidewalk

**NOTE:**  
Minimum of 12' clearance between pedestrian bump out and post curb.

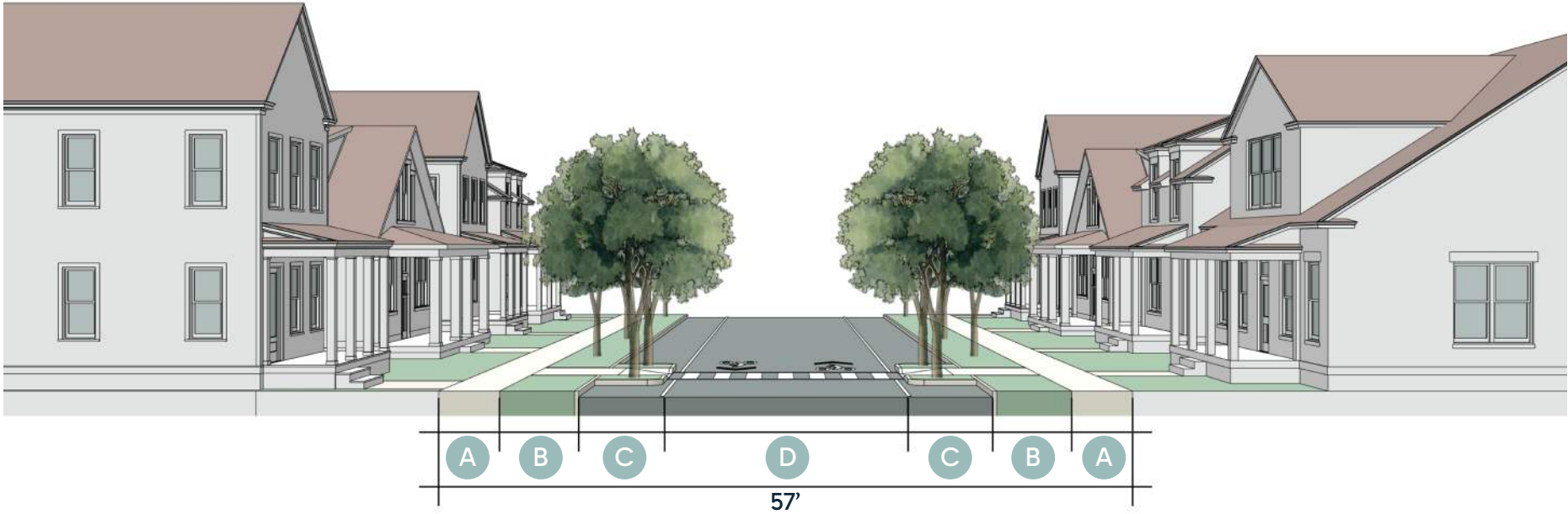


**NEIGHBORHOOD COLLECTOR WITH GREENWAY TRAIL**  
**DEDICATED PARKING - 60' R.O.W.**

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 7'-0" Parking
- D 20'-0" Pavement
- E 8'-0" Sidewalk

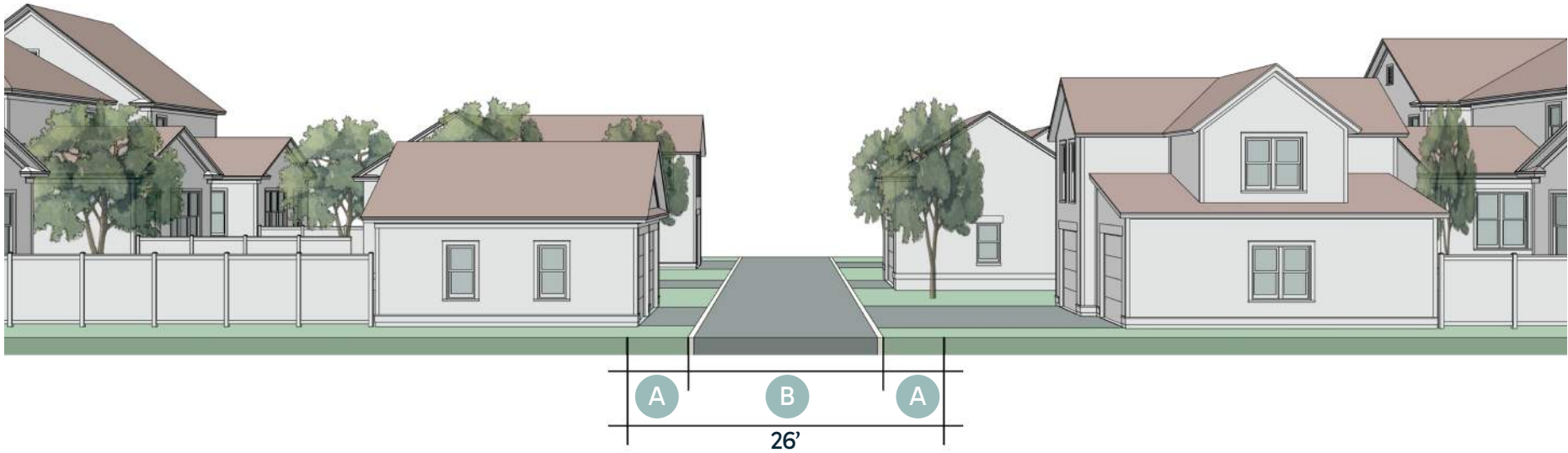


# STREET SECTIONS



NEIGHBORHOOD COLLECTOR  
DEDICATED PARKING - 57' R.O.W.

- A 5'-0" Sidewalk
- B 6'-6" Planting Strip
- C 7'-0" Parking
- D 20'-0" Pavement



RESIDENTIAL ALLEY  
NO PARKING - 26' R.O.W.

- A 5'-0" Furnishing Zone
- B 16'-0" Pavement

**NOTE:**  
Not intended for fire access

# STREET SECTIONS



**FIRE APPARATUS ALLEY ACCESS  
NO PARKING - 30' R.O.W.**

- A** 5'-0" Furnishing Zone
- B** 20'-0" Pavement



# TRANSECT OVERVIEW

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# OVERALL TRANSECT REGULATORY PLAN





# BLOCK DIAGRAM



01. BLOCK DIAGRAM AXON

02. INTENT

The Westmoore Landing block structure is scaled dimensionally to provide optimum flexibility for permitting the appropriate building types as defined in this section of the Regulatory Standards. This flexibility upholds the vision for supporting a variety of housing types, income ranges, and age demographics within Westmoore Landing. The block dimensions may be adjusted to create special focus lots, special addresses, variety of Street Setbacks, and common open spaces within this transect.

While the block diagram on this page does not represent any specific block build-out in Westmoore Landing, it does serve to illustrate the potential for permitting a wide variety of building types within the same block, creating a transition of building densities and scales from single family dwellings to mixed-use multi-family buildings.

The block diagram also illustrates the relationship of differing frontages within the same block, including porches, stoops, and storefronts as defined in these Regulatory Standards.

03. REGULATORY STANDARDS

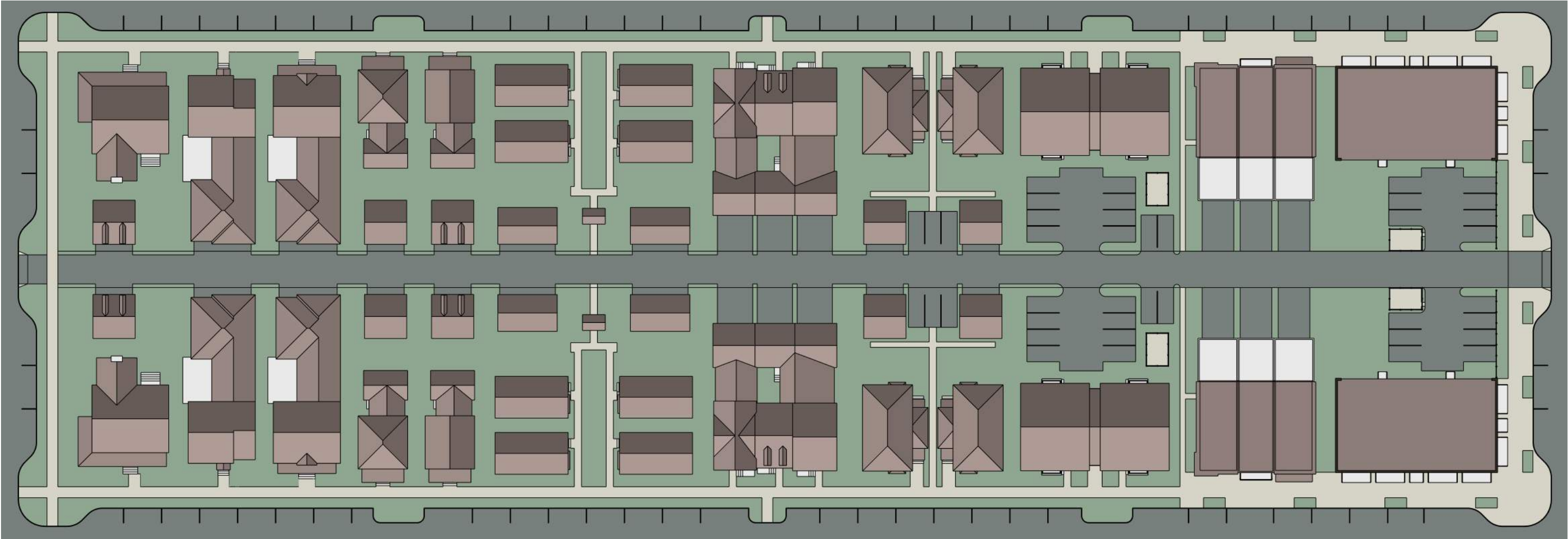
- A. Block perimeters shall not exceed 3,200 linear feet as measured along the street R.O.W.
  - 01. Blocks shall have sufficient width to provide for a minimum of two (2) tiers of lots of appropriate depths at the interior of the block. The inclusion of cottage clusters will permit additional tiers.



# BLOCK DIAGRAM



04. BLOCK DIAGRAM ELEVATION



05. BLOCK DIAGRAM PLAN

# TRANSECT & COMMUNITY TYPES OVERVIEW

## T3 NEIGHBORHOOD (T3-N)



T3 Neighborhood is designed to thoughtfully transition from the least dense natural and rural environment to the denser urban environments. T3 - N areas are moderately developed with a variety of residential uses with open spaces strategically incorporated into the site design. Existing and newly planted vegetation defines curvilinear streets and parks, and the green space associated with civic and institutional uses is part of the neighborhood's design. The balance of landscape and buildings tips toward landscape with more open space and vegetation framing the street than buildings.

## T3 HAMLET (T3-H)



T3 Hamlet is designed to complement the residential development pattern of T3 - N with commercial / live-work / mixed-use Hamlets that are appropriately scaled to the surrounding residential neighborhood. T3 - H areas are pedestrian oriented with a variety of attached residential uses and open spaces that are focused on neighborhood gathering. Buildings and hardscapes are expected to have slightly more presence and influence on the streetscape to identify nodes of activity and to define more formal open spaces.

## T4 NEIGHBORHOOD (T4 - N)



T4 Neighborhood includes residential neighborhoods intended to be developed in a more intense, urban fashion. Building may be taller and set closer to the street to create a denser streetscape. The T4 - Neighborhood features an integrated mixture of single-family, attached, and multi-family housing within walking distance of commercial and neighborhood-scaled open spaces. They feature highly connected street systems with sidewalks and bikeways.

## T4 VILLAGE (T4 - V)



T4 Village is designed to complement the denser residential development pattern of T4 - N with commercial / mixed-use Village Centers that are appropriately scaled to the surrounding residential neighborhood. T4 - V areas are pedestrian oriented with a variety of attached and multi-family residential uses, commercial uses, and open spaces that are focused on community-scaled gatherings and events. Buildings and hardscapes are expected to have significant presence and influence on the streetscape with mid-rise building heights and zero setback build-to zones to create vibrant commercial activity.

## T5 NEIGHBORHOOD (T5 - N)



T5 Neighborhood is the densest residential transect at Westmoore Landing. It is intended to be a vibrant neighborhood of diverse buildings that directly address active streetscapes and provide the necessary population density to support the commercial and office uses of the adjacent T5 - TC. T5 - N buildings are anticipated to include multi-story, mid-rise with parapet roofs and may include structured parking solutions. Buildings in T5 - N generally directly front streetscapes and open spaces with shallow setbacks and residential stoops and / or commercial ground floors that enliven the street with pedestrian activity.

## T5 TOWN CENTER (T5 - TC)



T5 Town Center is a gathering place for residents and visitors at a regional scale, where people can live, work, shop, and connect. Residents and visitors meet at centers to engage in commerce, civic, and recreational activities on a grander scale than can be found in their individual neighborhoods or communities. Building forms are typically mid-rise with parapet wall roof forms and storefronts at the street level. Buildings are built to the sidewalk with active pedestrian zones along the streets. Parking can be head-in angle parking, parallel, structured, and surface lots that are concealed from street views.



# PERMITTED LAND USES OVERVIEW

Land use development standards are necessary because certain land uses are more unique or impacting than other uses in the same zone district and therefore conditions are established to ensure compatibility with surrounding land uses. Other uses require large land areas, have unique operating characteristics, or tend to dominate the area more intensely than other uses permitted in the same zone district.

## 01. INTENT

- A. Some uses are socially and economically necessary in a particular zone district even though the potential may exist for the use to be incompatible with existing and future development. The development standards are intended to lessen and minimize incompatible land use relationships. Because the potential impacts cannot be satisfactorily predetermined for every location, some land uses are permitted in one transect only with a condition.

Land uses within Westmoore Landing are determined by Transects. To create a sustainable and mixed-use development, the zoning of each transect regulates the shape, scale, and placement of the buildings, and allows a variety of uses. To determine the allowed land uses, locate the property on the development plan and the designated transect it is in, and refer to the transect column on the Land Use Chart for the allowed uses.

Uses Permitted with Conditions shall follow the standards written in.

## 02. TABLE OF PRINCIPAL USES

- A. Permitted (●)
  - 01. These uses are permitted by right in the districts in which they are listed provided that they comply with the development standards of that transect.
- B. Permitted with Conditions (ⓘ)
  - 01. These uses are permitted by right in the districts in which they are listed, provided that they comply with the development standards of that transect and with any standards that are listed specific to that use on page 36 of this document.
- C. Prohibited
  - 01. Uses without a symbol are prohibited in the district because they are considered incompatible with the intent of the transect.
- D. Definitions
  - 01. Uses set out in this document are interpreted consistent with their meaning in the Huntsville Zoning Ordinance unless specifically modified on page 134 of this document.



# PERMITTED LAND USE BY TRANSECT

	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
Alcoholic Beverage Establishment						
Alcoholic Beverage Manufacturers that conduct tastings or samplings on the licensed premises (with or without entertainment)				◐		◐
Restaurant Liquor Retailers, Class I Club Liquor Retailers without package sales and on/off-premises beer/table wine retailers with food service (all with or without entertainment)						◐
Class I Liquor Retailers without package sales, and on/off-premises beer/table wine retailers (all without entertainment).						◐
Class I Lounge Liquor Retailers without package sales, and on/off-premises beer/table wine retailers (all with entertainment)						◐
Off-Premises Beer/Table Wine Retailers						◐
Restaurant Liquor Retailers, excluding hotels and civic center authorities licensed as a Restaurant Liquor Retailer, and On/Off Premises Beer/Table Wine Retailers with food service (all without entertainment)		◐		◐		◐
Off-Premises Beer/Table Wine Retailers operating as a specialty store		◐		◐		◐
Commercial						
Animal Hospital Or Veterinary Clinic, Pet Shops		◐		◐		◐
Apparel Stores		●		●		●
Auction Gallery, Including Auto Auction		◐		◐		◐
Automobile Repair Garage, Mechanical and Body				◐		◐
Bakery		◐		◐		◐
Bank, Savings and Loan Associations		●		●		●
Barber Shop, Beauty Parlor, Reducing Salons		●		●		●
Book Stores		●		●		●

● Permitted      ◐ Permitted with Conditions

	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
Cafes, Delicatessens, Coffee Shops, and Restaurants without Alcohol		●		●		●
Commercial Heliport & / Or Helipads						◐
Computer & Telephone Sales & Service Shop		●		●		●
Confectionery Stores		●		●		●
Dancing or Music Academies		●		●		●
Drug Stores & Apothecaries		●		●		●
Dry Cleaning Establishments		◐		◐		◐
Florist Shops & Greenhouses		●		●		●
Grocery Stores		●		●		●
Hardware Store, Gift Shop, Variety Store		●		●		●
Hotels and Motels		●		●		●
Jewelry Stores , Watch Repair Shop		●		●		●
Laundry Operations, Launderette, Coin Operated Dry Cleaning Establishments		●		●		●
Music or Record Shop		●		●		●
Newsstand, Hobby Shop		●		●		●
Notions & Dry Goods Stores		●		●		●
Photographic Studios & Photo Retail Sales		●		●		●
Print Shop, Newspaper Publishing House		●		●		●
Radio, Televisions, & Appliance Stores & Repair Shops				●		●
Radio and Television Studios, not Including Broadcast Towers				●		●
Shoe Store, Shoe Repair Shop, Tailor Shop		●		●		●
Upholstering Shops (<5 Employees)		●		●		●
Uses, Accessory and Temporary		●		●		●

# PERMITTED LAND USE BY TRANSECT

	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
Industrial						
Light Manufacturing & Assembly		⦿		⦿		⦿
Institutional						
Church and Similar Places of Worship	●	●	●	●	●	●
Municipal, County, State, or Federal Use	●	●	●	●	●	●
Nursery, Kindergartens Or Day Care For Children		●		●		●
Private School		⦿		●		●
Medical Use						
Assisted Living Facilities				●		●
Private Hosptials, Sanitariums, or Nursing Homes				●		●
Office						
Office, Professional Building		●		●		●
Office - Warehouse						⦿
Recreational & Entertainment Use						
Gymnasiums, Fitness & Exercise Centers, Health Clubs & Spas		●		●		●
Places of Amusement including Skating, Bowling, Trampolines, Golf Driving Ranges, Miniature Golf, Baseball Batting, and Similar Non-Offensive Establishments, but not Including Theatres or Drive-In Theatres.		●		●		●
Skateboard Parks	●	●	●	●	●	●
Skating Rinks		●		●		●
Theatres, not Including Drive-In Theatres				●		●
Residential						
Accessory Structures	●	●	●	●	●	●

● Permitted      ⦿ Permitted with Conditions

	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
Accessory Uses	●	●	●	●	●	●
Multiple Family Dwellings	●	●	●	●	●	●
Residential Over Commercial or Office Uses		●		●		●
Semi-Detached Dwellings	●	●	●	●	●	●
Single Family Attached Dwellings	●	●	●	●	●	●
Single Family Detached Dwellings	●	●	●	●	●	●
Two Family Dwellings	●	●	●	●	●	●
Zero Lot Line Dwellings	●	●	●	●	●	●
Elderly Housing		⦿		⦿		⦿
Home Occupation	●	●	●	●	●	●
Essential Community Home	⦿	⦿	⦿	⦿	⦿	⦿
Group Community Home	⦿	⦿	⦿	⦿	⦿	⦿
Transitional Community Home		⦿		⦿		⦿
Transportation						
Commuter Rail		●		●		●
Helistop		●		●		●
Railroad Station		●		●		●
Water Taxi Station		●		●		●
Other						
Agricultural Uses	●	●	●	●	●	●
Farming	●	●	●	●	●	●
Pond / Lake	●	●	●	●	●	●
Parking Lots and Parking Garages	●	●	●	●	●	●

# PERMITTED LAND USE BY TRANSECT

01. CONDITIONS ON USES

- A. Animal Hospital or Veterinary Clinic
  - 01. Indoor facility
  - 02. Restricted to domesticated animals
  - 03. No large animals
- B. Auction Gallery, Including Auto Auction
  - 01. Must be indoors, no vehicles stored outside
- C. Automobile Repair Garage, Mechanical and Body
  - 01. No exterior Storage
- D. Bakery
  - 01. No more than five (5) persons are employed on the premises and where the products made are sold exclusively for retail on the premises
- E. Commercial Heliport, Helipad, Helistop
  - 01. Must meet the following performace standards:
    - a. Air Pollution - Every use shall be so operated as to comply with the emission limitations specified in Chapter 12, Article II, Section 12-31, Code of Ordinances, City of Huntsville, Alabama, April 2003, as the same may be amended or revised, except that in the case of visible emission restrictions no visible emissions will be allowable.
    - b. Odor - No odors shall be emitted that are detectable without instruments at or beyond any property line.
    - c. Vibration - Maximum permitted vibration from any cause shall not exceed the following, when measured at the lot line:

Frequency (Cycles per Second)	Displacement (Inches)
10 and below	.0008
10 - 20	.0005
20 - 30	.0003
30 - 40	.0002
40 - 50	.0001
50 and above	.0001
    - d. Noise - Every use shall be so operated as to comply with the noise limitations specified in Chapter 12, Article V, Noise, Code of Ordinances, City of Huntsville, Alabama, April 2003, as the same may be amended or revised.
    - e. Fire and Explosion Hazard - All activities and all storage of flammable and explosive materials shall be in accordance with the National Board of Fire Underwriters' publications and other local ordinances.

- f. Glare - No direct or reflected glare shall be produced so as to be visible at or beyond any lot line.
  - g. Radiation - Research operations shall cause no dangerous radiation at any property line as specified by the regulations of the Alabama Department of Public Health, Bureau of Radiological Health.
  - h. Radioactive Materials - The handling of radioactive materials, the discharge of such materials into air or water, and the disposal of radioactive wastes, shall be in conformance with the regulations of the Alabama Department of Public Health, Bureau of Radiological Health.
  - i. Electromagnetic Interference - Industries shall emit only that amount of unshielded spurious electromagnetic radiation as is necessary for the conduct of their operations. Federal Communications Commission requirements shall govern maximum radiation which tends to interfere with meaningful signals.
  - j. Water Pollution - No effluent shall be discharged in any stream. Discharge into the Huntsville sewerage system shall be as approved by the City of Huntsville Engineer of Public Works.
  - k. Waste - All industrial wastes shall be treated and disposed of in such a manner as to comply with all federal, local, and state standards.
- F. Dry Cleaning Establishments
    - 01. Only utilizing non-flammable dry cleaning fluids
  - G. Light Manufacturing & Assembly
    - 01. Provided the goods are to be sold on the premises only, and provided not more than ten (10) persons are employed in the process provided such uses are in accordance with the following performance standards:
      - a. Air Pollution - Every use shall be so operated as to comply with the emission limitations specified in Chapter 12, Article II, Section 12-31, Code of Ordinances, City of Huntsville, Alabama, April 2003, as the same may be amended or revised, except that in the case of visible emission restrictions no visible emissions will be allowable.
      - b. Odor - No odors shall be emitted that are detectable without instruments at or beyond any property line.
      - c. Vibration - Maximum permitted vibration from any cause shall not exceed the following, when measured at the lot line:

- | Frequency<br>(Cycles per Second) | Displacement<br>(Inches) |
|----------------------------------|--------------------------|
| 10 and below                     | .0008                    |
| 10 - 20                          | .0005                    |
| 20 - 30                          | .0003                    |
| 30 - 40                          | .0002                    |
| 40 - 50                          | .0001                    |
| 50 and above                     | .0001                    |
- d. Noise - Every use shall be so operated as to comply with the noise limitations specified in Chapter 12, Article V, Noise, Code of Ordinances, City of Huntsville, Alabama, April 2003, as the same may be amended or revised.
  - e. Fire and Explosion Hazard - All activities and all storage of flammable and explosive materials shall be in accordance with the National Board of Fire Underwriters' publications and other local ordinances.
  - f. Glare - No direct or reflected glare shall be produced so as to be visible at or beyond any lot line.
  - g. Radiation - Research operations shall cause no dangerous radiation at any property line as specified by the regulations of the Alabama Department of Public Health, Bureau of Radiological Health.
  - h. Radioactive Materials - The handling of radioactive materials, the discharge of such materials into air or water, and the disposal of radioactive wastes, shall be in conformance with the regulations of the Alabama Department of Public Health, Bureau of Radiological Health.
  - i. Electromagnetic Interference - Industries shall emit only that amount of unshielded spurious electromagnetic radiation as is necessary for the conduct of their operations. Federal Communications Commission requirements shall govern maximum radiation which tends to interfere with meaningful signals.
  - j. Water Pollution - No effluent shall be discharged in any stream. Discharge into the Huntsville sewerage system shall be as approved by the City of Huntsville Engineer of Public Works.
  - k. Waste - All industrial wastes shall be treated and disposed of in such a manner as to comply with all federal, local, and state standards.



# PERMITTED LAND USE BY TRANSECT

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- H. Private Schools
  - 01. T3 limited to 15,000 sf of floor area
- I. Office - Warehouse
  - 01. Only where the structure is used for offices and the storage of goods, wares, or merchandise in conjunction with a business located on the site.
  - 02. No outside storage of goods, wares, or merchandise is permitted on premises.
- J. Elderly Housing
  - 01. Will follow the building type standards for stacked flats and/or mixed use
- K. Restaurant Liquor Retailers, Class I Club Liquor Retailers without package sales and on/off-premises beer/table wine retailers with food service (all with or without entertainment).
  - 01. Provided that no alcoholic beverages shall be sold by any restaurant or food vendor located in or adjacent to a food court.
- L. Class I Liquor Retailers without package sales, and on/off-premises beer/table wine retailers (all without entertainment).
  - 01. Such retailer's premises shall not be located within one thousand five hundred (1500) feet of the retailer's premises of a Package Sales Establishment.
- M. Class I Lounge Liquor Retailers without package sales, and on/off-premises beer/table wine retailers (all with entertainment).
  - 01. Such retailer's premises shall not be located within two hundred (200) feet of a T3-N, T4-N, or T5-N district except where the retailer's premises is separated from said residential district by an expressway or an interstate highway; and
  - 02. Such retailer's premises shall not be located within five hundred (500) feet of a protected use, except such retailer's premises may be located within five hundred (500) feet of accessory child care centers located in office buildings; and
  - 03. The aforesaid distance restriction relative to protected uses shall not apply where said retailer's premises is located in an enclosed mall shopping center with a minimum of one hundred thousand (100,000) square feet of floor area; and
  - 04. Such retailer's premises shall not be located within one thousand (1000) feet of other such retailer's premises; and
  - 05. Such retailer's premises shall not be located within one thousand five hundred (1500) feet of the retailer's premises of a Package Sales Establishment. (13-247)
- N. Off-Premises Beer/Table Wine Retailers
  - 01. Such retailer's premises shall not be located within five hundred (500) feet of a T3-N, T4-N, or T5-N district; provided, however, the aforesaid distance restriction shall not apply in the case of specialty stores; and
  - 02. Such retailer's premises shall not be located within five hundred (500) feet of a protected use unless a special exception has been granted in the case of churches in accordance with Section 92.5 - Powers and Duties, except such retailer's premises may be located within two hundred (200) feet of accessory child care centers located in office buildings; and
  - 03. The aforesaid distance restriction relative to protected uses shall not apply where said retailer's premises is located in an enclosed mall shopping center with a minimum of one hundred thousand (100,000) square feet of floor area."
- O. Restaurant Liquor Retailers, excluding hotels and civic center authorities licensed as a Restaurant Liquor Retailer, and On/Off Premises Beer/Table Wine Retailers with food service (all without entertainment).
  - 01. No alcoholic beverages shall be served, offered, dispensed, sold, or consumed between the hours of midnight and 2 a.m. unless a special exception has been granted in accordance with Section 92.5 – Powers and Duties, and further provided, the provisions of Section 74.4 – Non-Conforming Uses of Structures shall not operate to allow existing establishments to function contrary to this provision; and
  - 02. There shall be no private sidewalk or patio seating unless a special exception has been granted in accordance with Section 92.5 – Powers and Duties; and
  - 03. No alcoholic beverages shall be sold by any restaurant or food vendor located in or adjacent to a food court.
- P. Off-Premises Beer/Table Wine Retailers operating as a specialty store.
  - 01. Such retailers are not located within five hundred (500) feet of any protected use, unless a special exception has been granted in the case of churches in accordance with Section 92.5 - Powers and Duties.
- Q. Essential Community Home, Group Community Home, and Transitional Community Home uses shall be subject to all provisions of Article 78 of the Zoning Ordinance.
- R. Alcoholic Beverage Manufacturers that conduct tastings or samplings on the licensed premises (with or without entertainment) shall be subject to all provisions of Section 92.5.2(35) of the Huntsville Zoning Ordinance as a special exception within the T4-V and T5-TC.

# BUILDING TYPES OVERVIEW

This section regulates different physical forms of buildings within each Transect, creating variation in the built environment within an appropriate scale.

### 01. APPLICABILITY

- A. Any building construction or alteration shall comply with the standards for each building type and other applicable standards as forth herein.
- B. The applicant shall select which allowed building frontage type will be used with the selected building.
- C. Each building type may accommodate any use allowed in the transect in which it is located, per the Permitted Land Use Tables within this PD-H document. The names of the building types are not intended to limit or expand uses within a building type.

### 02. NUMBER OF BUILDINGS

- A. Each lot shall only have one primary building type from the building types table, with the following exceptions:
  - 01. More than one primary building is allowed on a lot if the submitted building permit application includes a site plan ensuring all associated buildings meet all required building type standards and specifications of this document.
  - 02. Multiple primary buildings and/or multiple individual units may be permitted on a lot within an approved condominium development.
- B. Accessory structures and ADUs are permitted in addition to a primary building, see pages 50, 67, 84 for more standards and regulations.

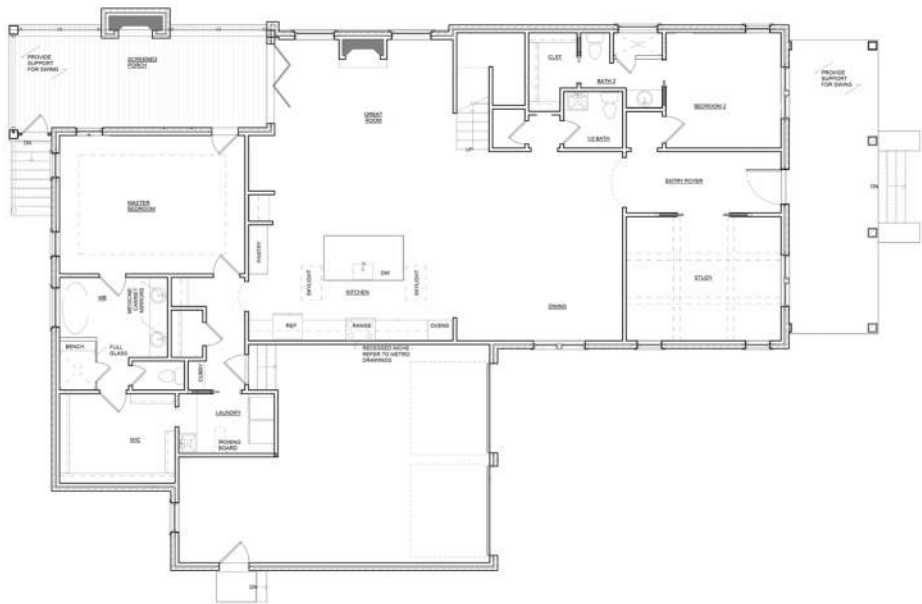
### 03. BUILDING ORIENTATION

- A. Primary facade shall be oriented to street or open space.
- B. See Frontage Standards on pages 97 - 105 for additional regulations.

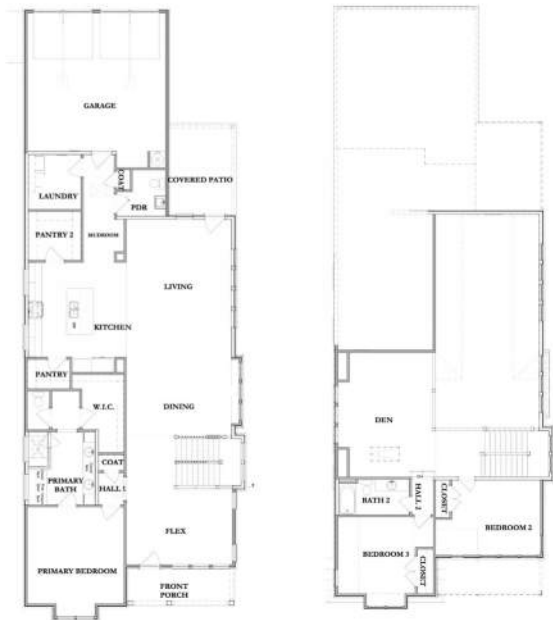
	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
Single Family Detached						
Cluster Housing	■	■	■	■	■	■
Single Family Rear Entry	■	■	■	■	■	
Single Family Front Entry	■	■	■			
Accessory Dwelling Unit	■	■	■	■	■	■
Single Family Attached						
Duplex	■	■	■	■	■	■
Townhouse	■	■	■	■	■	■
Multifamily						
Carriage House Flat	■	■	■	■	■	■
Manor House	■	■	■	■	■	■
Stacked Flats			■	■	■	■
Mixed-Use		■		■	■	■
Live-Work		■		■	■	■
Commercial		■		■		■
Civic	■	■	■	■	■	■
Amenities	■	■	■	■	■	■

■ Permitted

# TYPICAL FLOOR PLANS



SINGLE FAMILY FRONT ENTRY



SINGLE FAMILY REAR ENTRY



CLUSTER HOUSING



ACCESSORY DWELLING UNIT

**NOTE:**  
The floor plans shown are illustrative only and represent typical configurations that are subject to change



# TYPICAL FLOOR PLANS



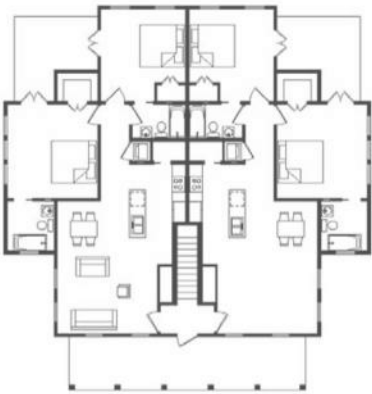
DUPLEX



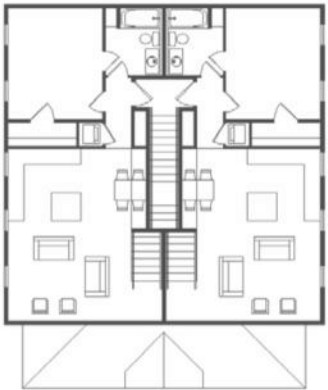
TOWNHOUSE



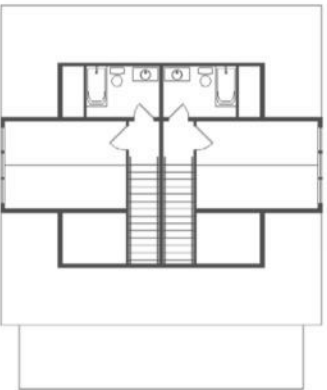
CARRAIGE HOUSE FLAT



First Floor Plan



Second Floor Plan

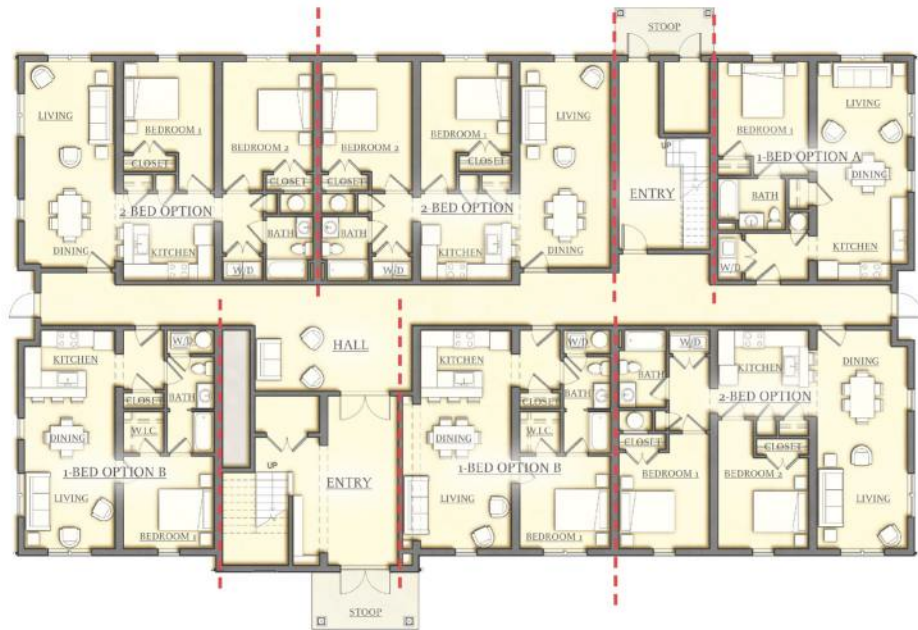


Third Floor Plan

MANOR HOUSE

**NOTE:**  
The floor plans shown are illustrative only and represent typical configurations that are subject to change

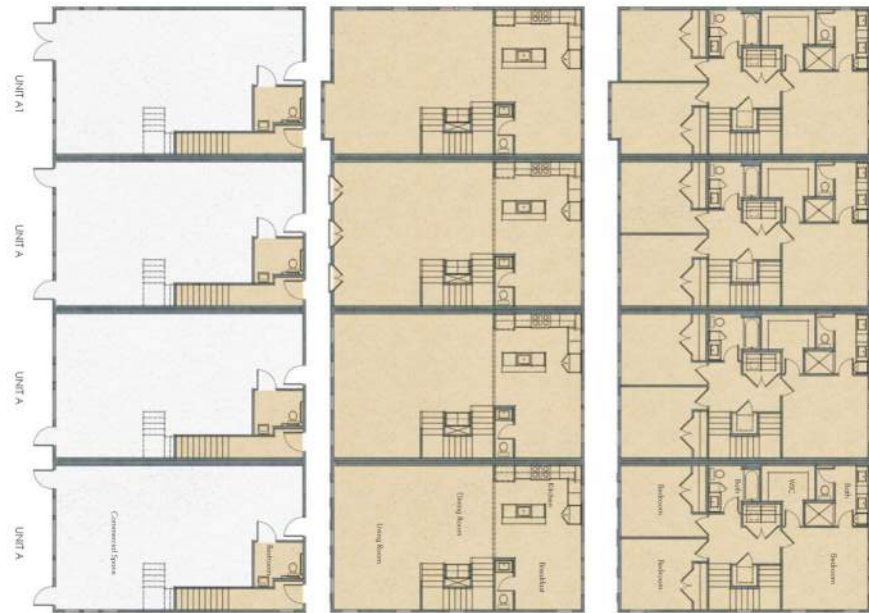
# TYPICAL FLOOR PLANS



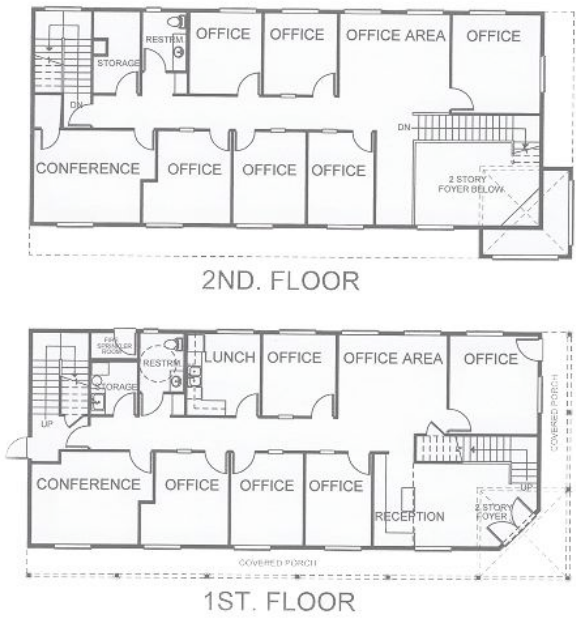
STACKED FLATS



MIXED-USE



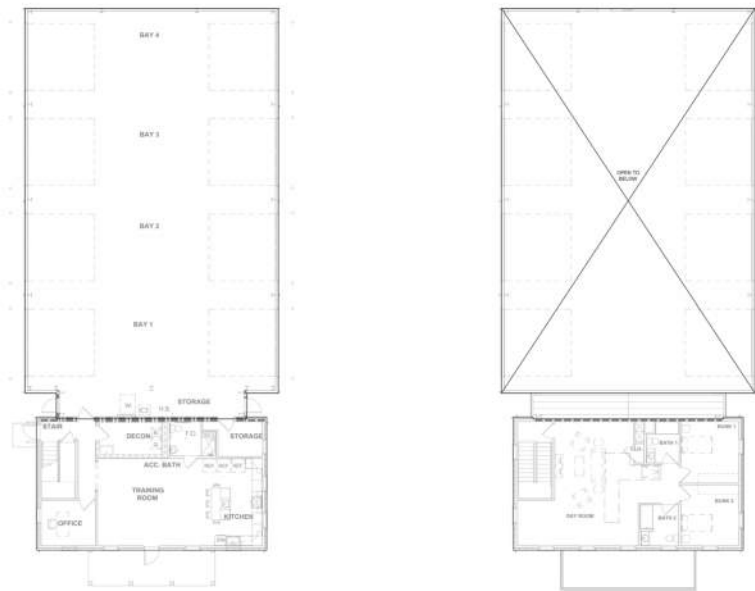
LIVE-WORK



COMMERCIAL

**NOTE:**  
The floor plans shown are illustrative only and represent typical configurations that are subject to change

# TYPICAL FLOOR PLANS



CIVIC - EXAMPLE FIRE STATION

**NOTE:**  
The floor plans shown are illustrative only and represent typical configurations that are subject to change





# T<sub>3</sub> REGULATORY STANDARDS

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T3 TRANSECT

44

T3 BUILDING TYPES OVERVIEW

45



# T3 TRANSECT

## T3 NEIGHBORHOOD (T3 - N)



### 01. INTENT

T3 - N is designed to thoughtfully transition from the least dense natural and rural environment to the denser urban environments. T3 - N areas are moderately developed with a variety of residential uses with open spaces strategically incorporated into the site design. Existing and newly planted vegetation defines curvilinear streets and parks, and the green space associated with civic and institutional uses is part of the neighborhood's design. The balance of landscape and buildings tips toward landscape with more open space and vegetation framing the street than buildings.



## T3 HAMLET (T3 - H)



### 02. INTENT

T3 - H is designed to complement the residential development pattern of T3 - N with commercial / live-work / mixed-use Hamlets that are appropriately scaled to the surrounding residential neighborhood. T3 - H areas are pedestrian oriented with a variety of attached residential uses and open spaces that are focused on neighborhood gathering. Buildings and hardscapes are expected to have slightly more presence and influence on the streetscape to identify nodes of activity and to define more formal open spaces.





# T3 BUILDING TYPES OVERVIEW



**CLUSTER HOUSING**

Series of small, detached or attached dwellings arranged to define a shared court

T3 - N

T3 - H



**DUPLEX**

2 side-by-side or stacked dwelling units within a single building mass

T3 - N

T3 - H



**MIXED-USE**

Pedestrian oriented retail and services with dwellings and/or office above

T3 - N

T3 - H



**AMENITIES**

Structures used for recreation, relaxation, or social interactions by residents and their guests

T3 - N

T3 - H



**SINGLE FAMILY REAR ENTRY**

Medium-sized, detached dwelling on a single lot

T3 - N

T3 - H



**TOWNHOUSE**

3 - 8 side-by-side dwelling units within a single building mass

T3 - N

T3 - H



**LIVE-WORK**

Dwelling unit with commercial use on the first floor within unit

T3 - N

T3 - H



**SINGLE FAMILY FRONT ENTRY**

Medium to large-sized, detached dwelling on a single lot

T3 - N

T3 - H



**CARRIAGE HOUSE FLAT**

1 - 4 dwellings above several garages, often part of multifamily development

T3 - N

T3 - H



**COMMERCIAL**

Pedestrian oriented retail and services

T3 - N

T3 - H



**ACCESSORY DWELLING UNIT**

Accessory dwelling to a primary residence

T3 - N

T3 - H



**MANOR HOUSE**

2 - 14 dwelling units, typically with a shared entry

T3 - N

T3 - H



**CIVIC**

Used for community, government, cultural, educational, and / or religious uses

T3 - N

T3 - H



# BUILDING TYPE: CLUSTER HOUSING



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	18 ft. min.	18 ft. min.
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	10 ft.	10 ft.
D Minimum Rear Setback	0 or 10 ft.	0 or 10 ft.
E Maximum Height	2 stories	3 stories
F Separation between Buildings	0 or 10 ft. min.	0 or 10 ft. min.
G Paseo (Public Access Easement) Width	20 ft. min.	20 ft. min.
H Minimum Open Space Setback	5 ft.	5 ft.

## 04. BUILDING ORIENTATION

- A. Fronts of units may not face the rear of another unit.

## 05. PARKING ACCESS & LOCATION

- A. Rear Loaded with No Street Access allowed if Alley Access is available, Side and / or Rear Surface Parking Allowed.
- B. All units shall be served by a pedestrian circulation system that connects to an existing or planned sidewalk or trail system.

## 06. OPEN SPACE / PASEO (PUBLIC ACCESS EASEMENT)

- A. Common Open Space shall be improved for active or passive recreational use. Amenities & LID stormwater may be included in this space.
- B. 150 square feet of Common Open Space shall be provided per unit.
- C. Cluster Housing Open Space, Small Paseo, and / or Large Paseo Landscape Standards shall apply and meet required PD-H ratios / percentages.
- D. Private Easements for Ingress, Egress, and Public Access shall be referenced on the final plat and must, at a minimum, meet all the requirements and standards outlined in the Westmoore Landing PD-H.



# BUILDING TYPE: CLUSTER HOUSING

07. CLUSTER HOUSING EXAMPLE: 3 UNITS



08. CLUSTER HOUSING EXAMPLE: 12 UNITS



09. CLUSTER HOUSING EXAMPLE: 13 UNITS



10. CLUSTER HOUSING EXAMPLE: 36 UNITS

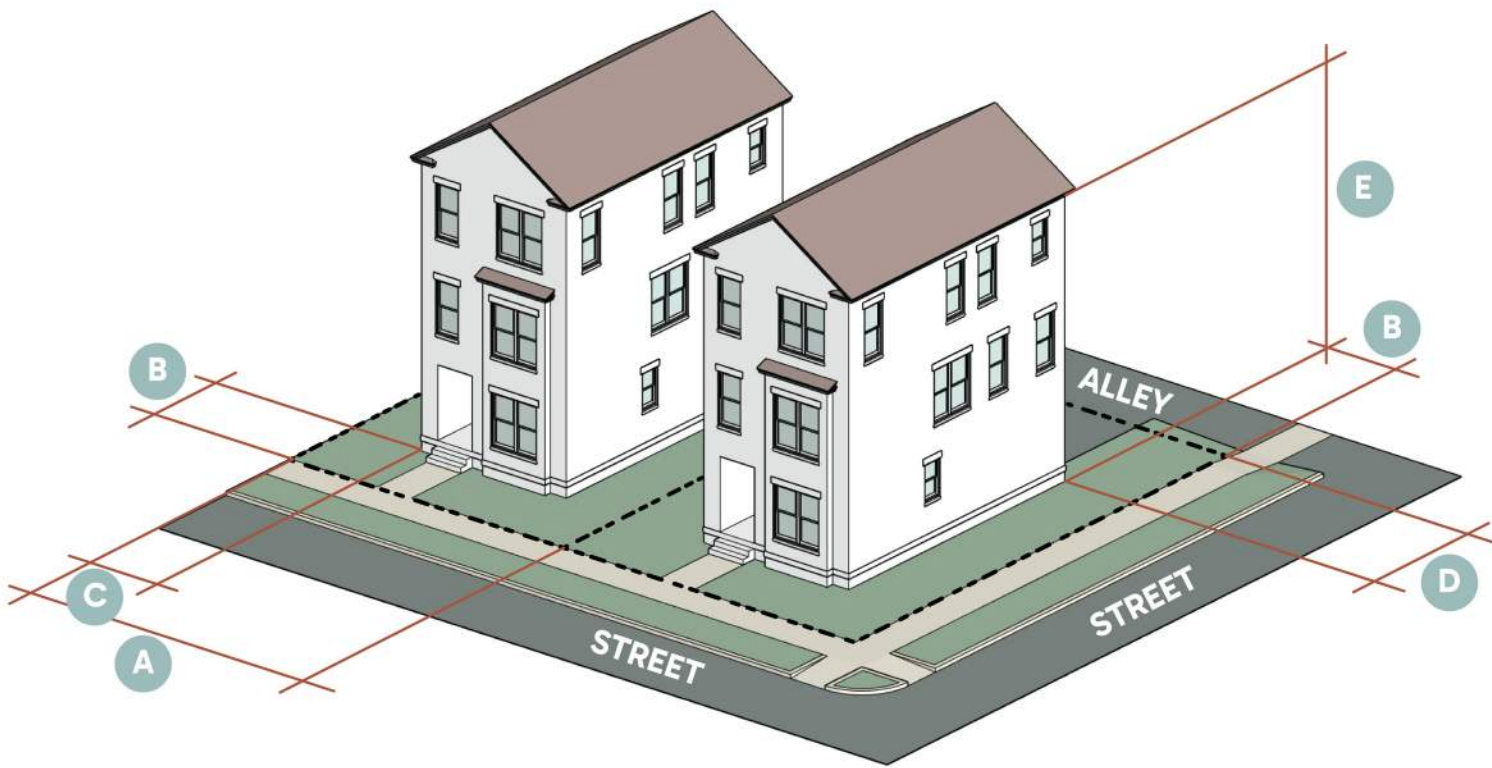


## 11. NOTES

- A. Clusters may be divided up into fee simple condominium lots provided they meet necessary building and fire code requirements.
- B. Cluster Housing is 3 - 36 units per cluster and may account for a maximum of 20% of the overall development dwelling unit count.
- C. Housing Clusters shall have a 400 ft. minimum distance buffer between any two clusters.
- D. Cluster Housing shall include a minimum of 1 understory tree and at least 2 levels of planting along the front facade area.
- E. Trash collection facilities shall be located behind building setback line and shall be screened from the R.O.W. and adjacent properties by either architectural treatments or vegetative screening.
- F. Area between buildings will be appropriately drained.
- G. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- H. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.



# BUILDING TYPE: SINGLE FAMILY REAR ENTRY



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	25 ft. min.	25 ft. min.
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories

## 04. PARKING ACCESS & LOCATION

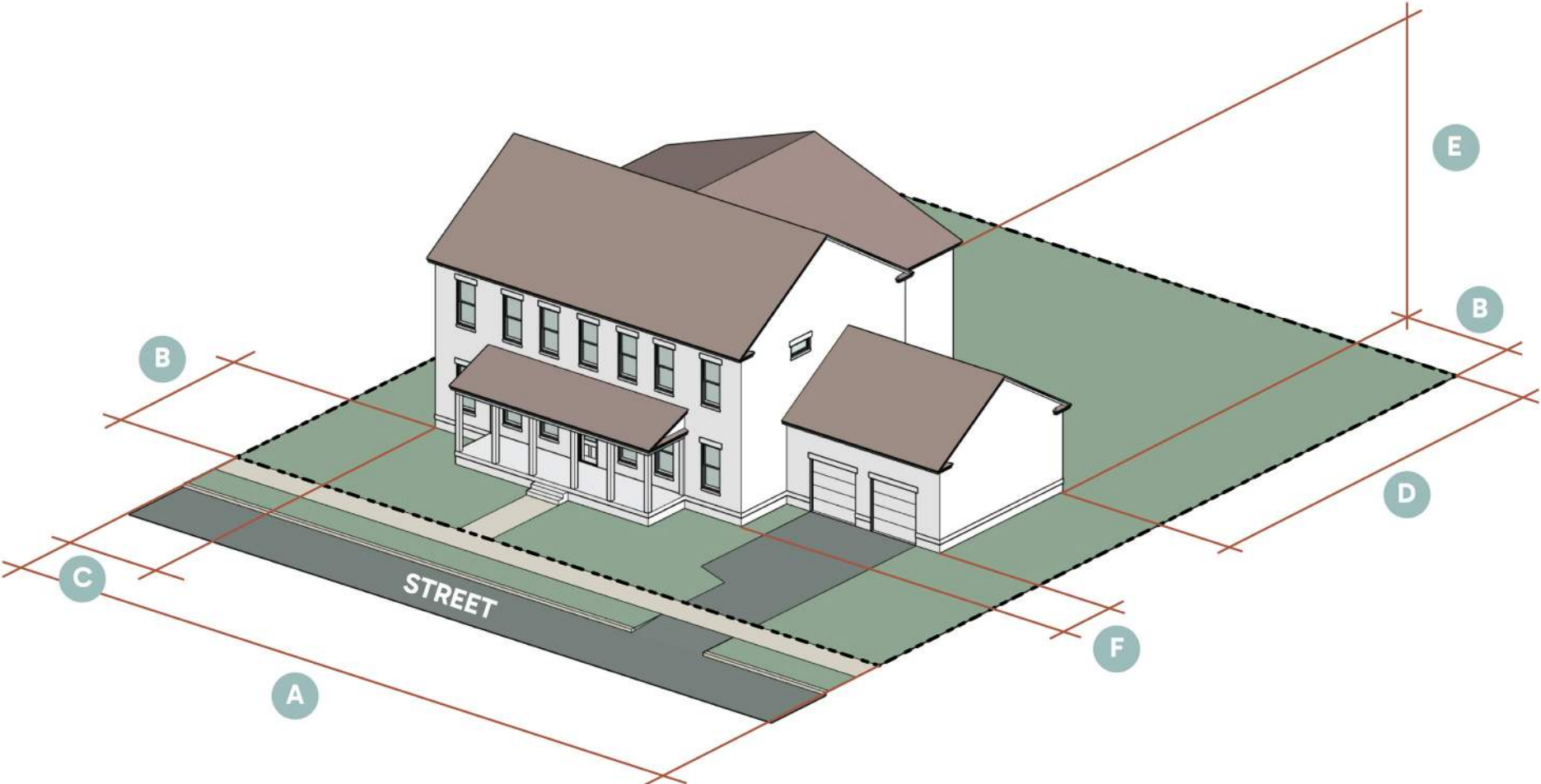
- A. Rear Loaded with Alley Access, when available. Detached/Attached Garage.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: SINGLE FAMILY FRONT ENTRY



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	55 ft. min.	55 ft. min.
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories
F Garage Setback	10 ft. min.	10 ft. min.

04. PARKING ACCESS & LOCATION

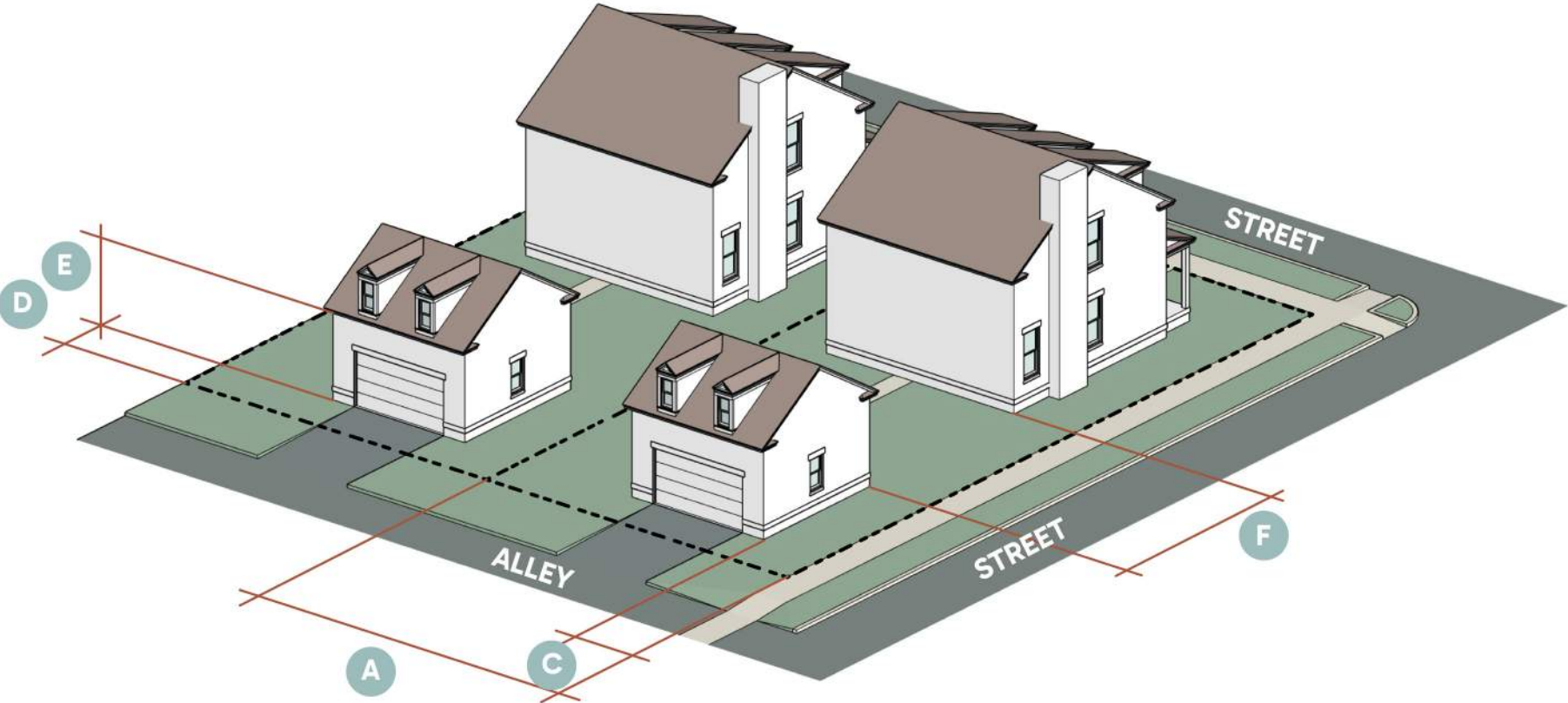
- A. Front or side loaded with street access. Detached or attached garage permitted.
- B. Motorcourts are permitted.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: ACCESSORY DWELLING UNIT



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

### 03. BUILDING TYPE STANDARDS

	T3 - N	T3 - H
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories
F Separation between Buildings	10 ft. min.	10 ft. min.

### 04. PERMITTED PRIMARY BUILDING TYPES

- A. Single Family Rear Entry

B. Single Family Front Entry

C. Duplex

G. ADUs are not permitted on fee simple cluster lots.
- D. Townhouse

E. Manor House

F. Live-Work

### 05. PARKING ACCESS & LOCATION

- A. Shall be located underneath, behind, or beside building only on interior lot lines.

### 06. NOTES

- A. Size of ADU not to exceed 60% of primary building habitable floor area.

B. ADU's may be attached, semi detached, or detached structures. Separation between buildings does not apply when ADU is attached to primary building. Where applicable, open air breezeways may encroach on separation between buildings. ADU's shall be subordinate in mass, size, and height to the primary building. The architectural style shall be consistent or complementary in design, character, and style to the primary building.

C. Frontage types may encroach into a required street setback as specified in G.96.01.A.

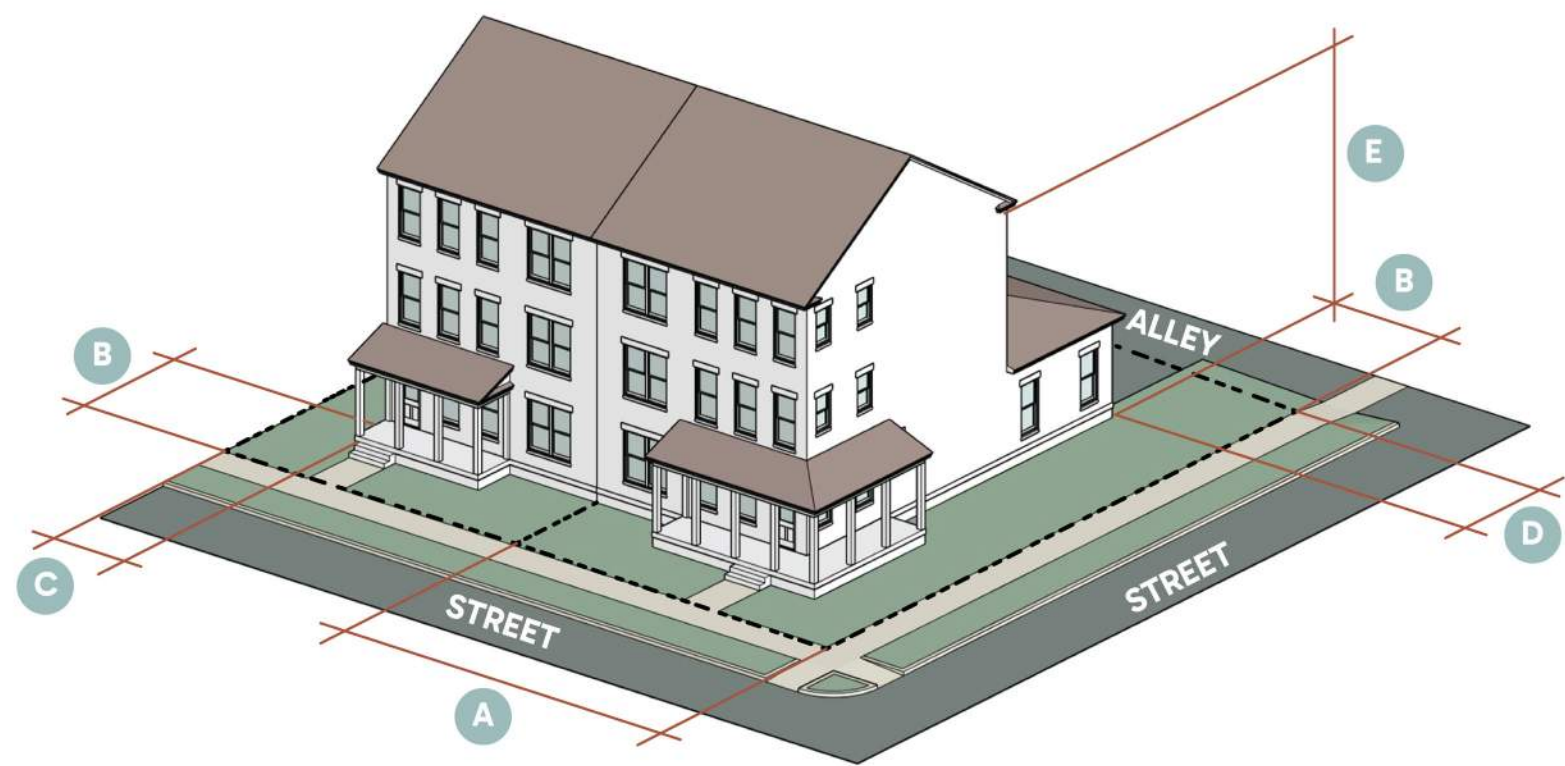
D. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.

E. ADU's shall be labeled on individual site plans.

F. ADU's shall provide one off-street parking space.



# BUILDING TYPE: DUPLEX



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	16 ft. min. per unit	16 ft. min. per unit
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

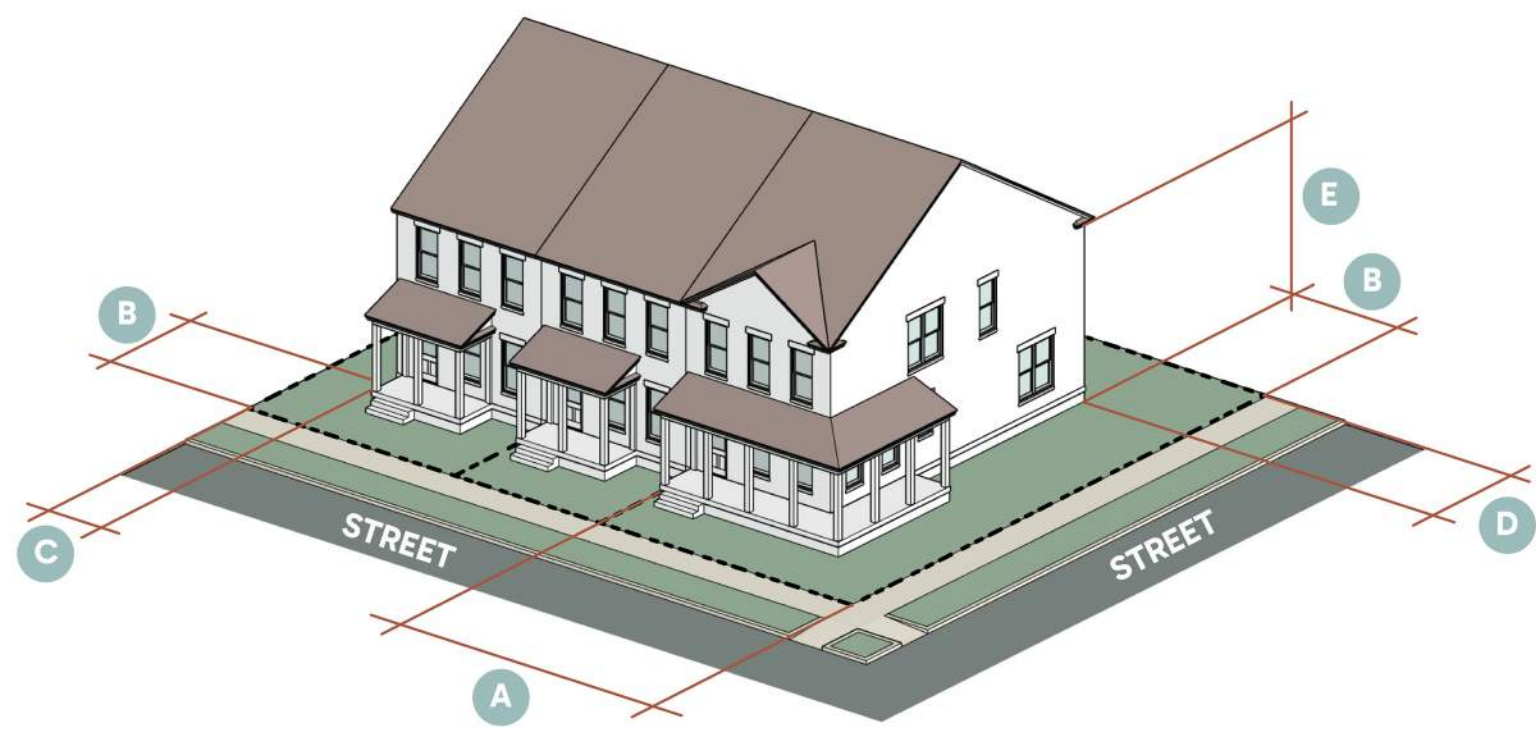
- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: TOWNHOUSE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	16 ft. min. per unit	16 ft. min. per unit
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

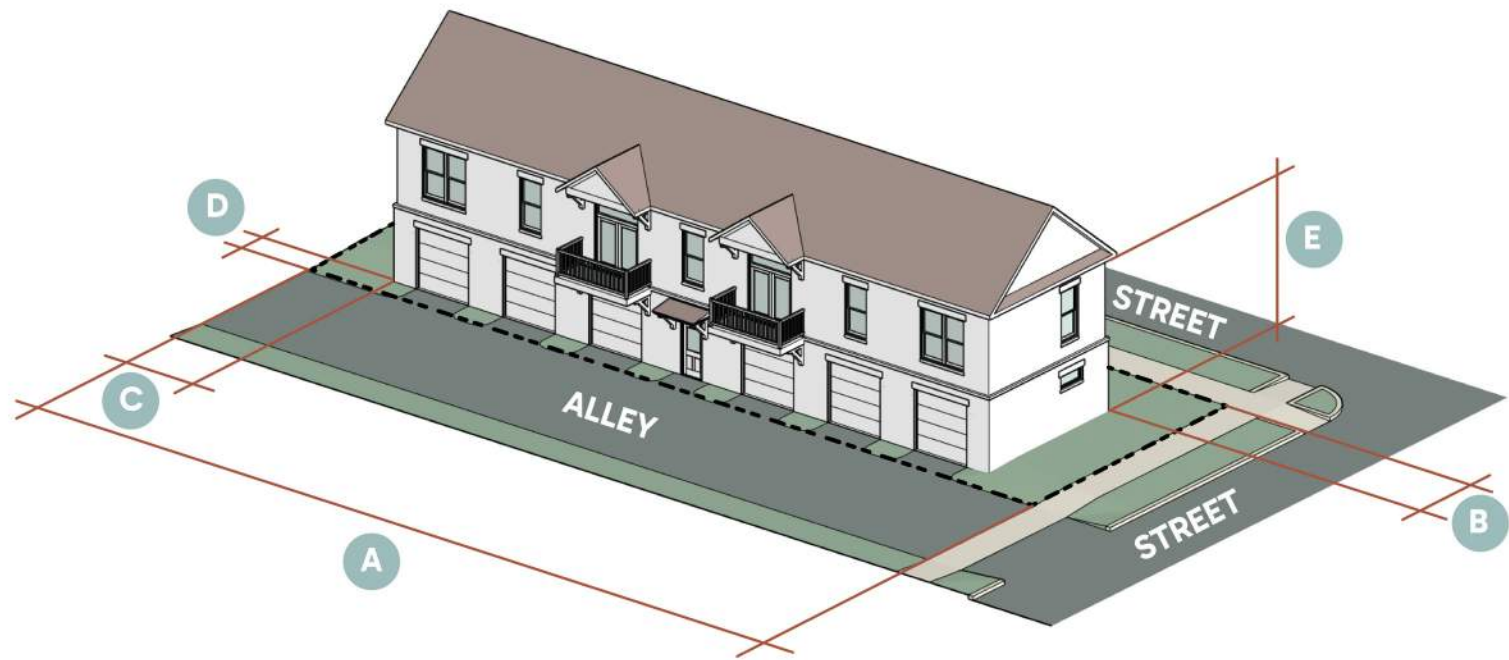
- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Townhouses can be 3 - 8 units in a building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.



# BUILDING TYPE: CARRIAGE HOUSE FLAT



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	24 ft. min.	24 ft. min.
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

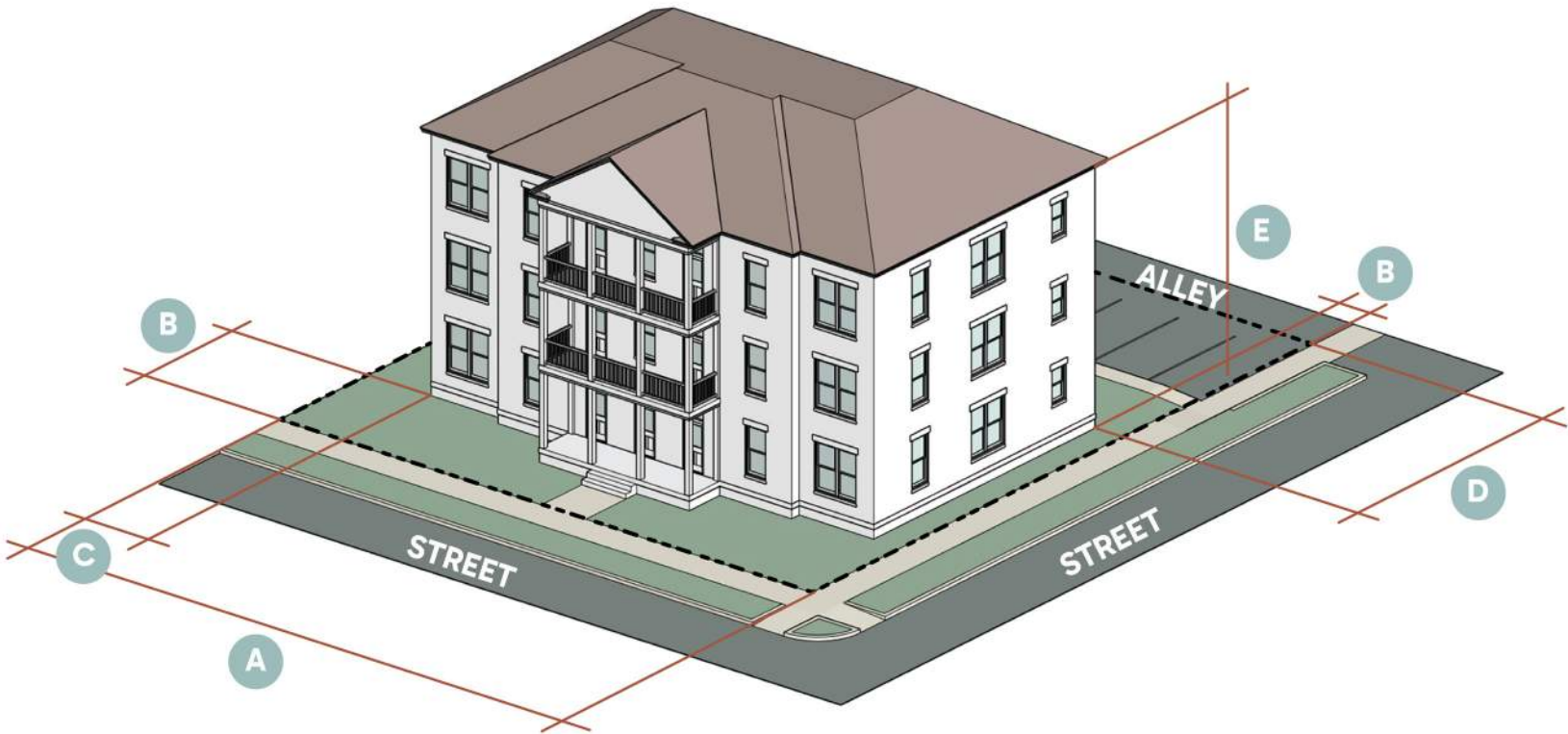
- A. Rear loaded with alley access. Garage parking is located below the unit on the ground floor.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MANOR HOUSE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	40 ft. min.	40 ft. min.
B Minimum Street Setback	15 ft.	10 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, Side and / or rear surface parking allowed.

05. NOTES

- A. Manor Houses can be 2 - 14 units per building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MIXED-USE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	Not Permitted	N/A
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 or 5 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		4 stories

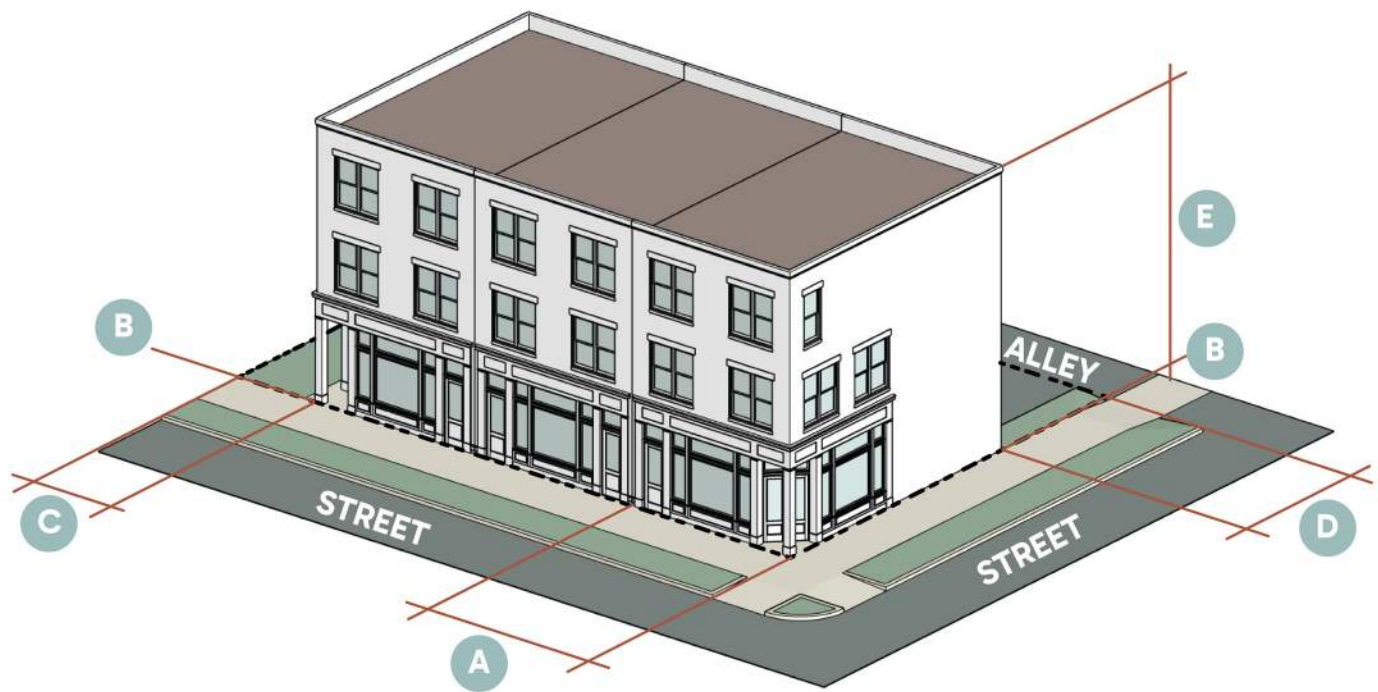
04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. There are no min. square footage requirements for residential units over commercial, however no more than 50% of units will be less than 650 sf.

# BUILDING TYPE: LIVE-WORK



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

### 03. BUILDING TYPE STANDARDS

	T3 - N	T3 - H
A Lot Width	Not Permitted	18 ft. min. per unit
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 or 5 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		3 stories

### 04. PARKING ACCESS & LOCATION

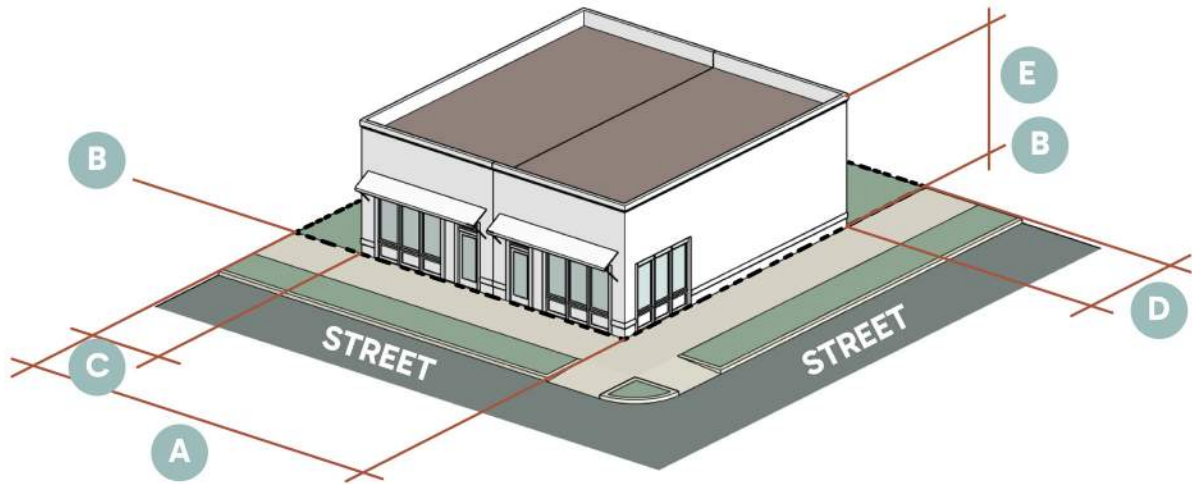
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

### 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.



# BUILDING TYPE: COMMERCIAL



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	Not Permitted	N/A
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		3 stories

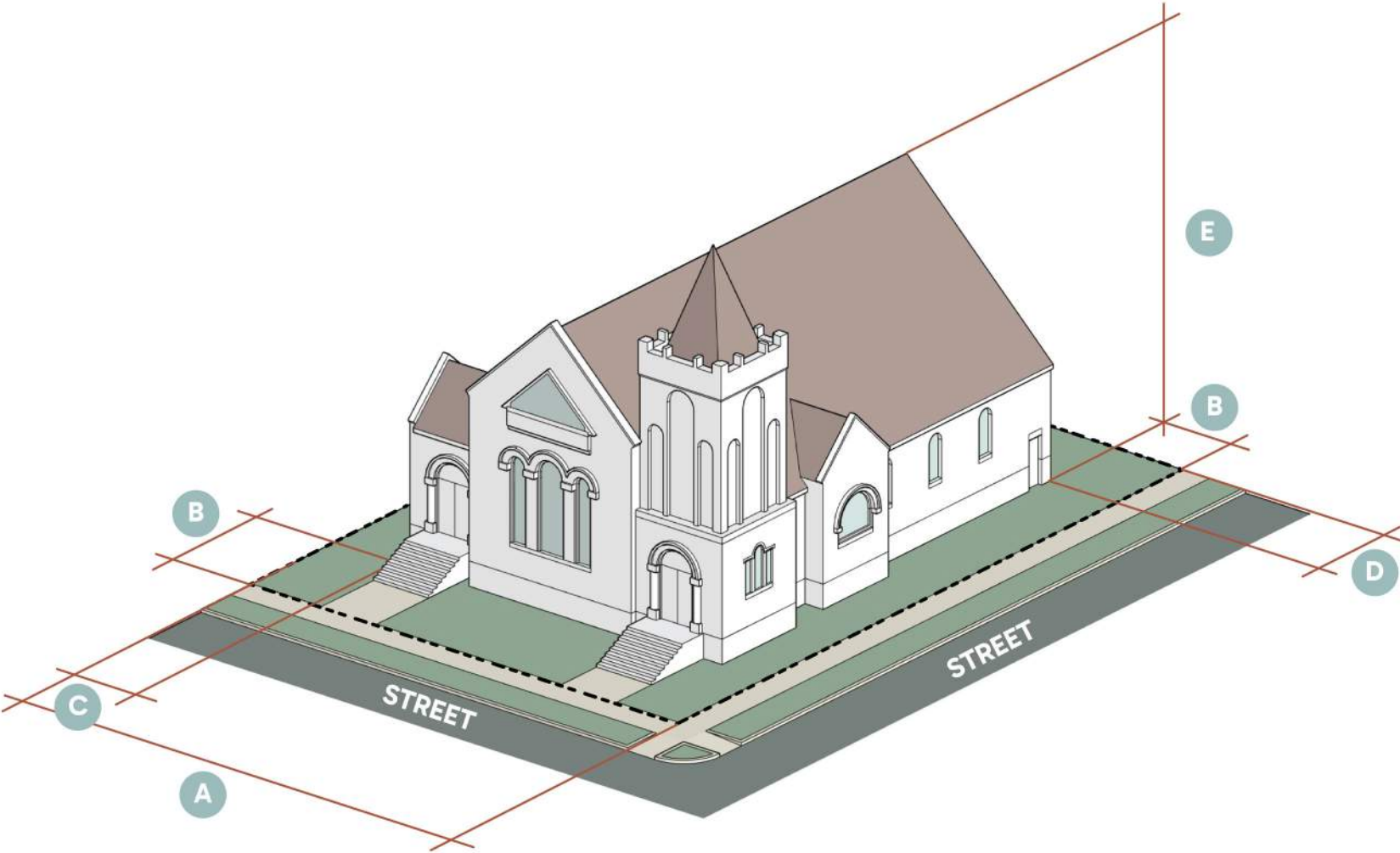
## 04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.

# BUILDING TYPE: CIVIC



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	2 stories	2 stories

04. PARKING ACCESS & LOCATION

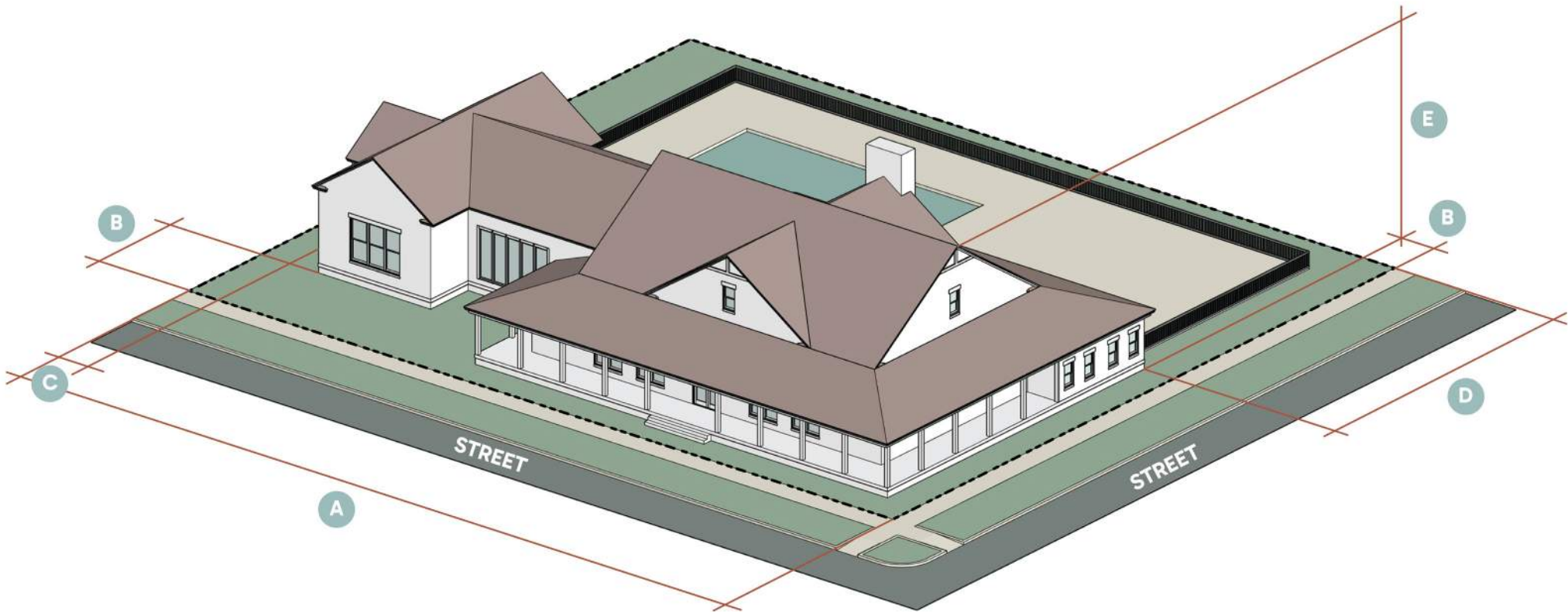
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: AMENITIES



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T3 - N	T3 - H
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	2 stories	2 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# T<sub>4</sub> REGULATORY STANDARDS

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T4 TRANSECT	61
T4 BUILDING TYPES OVERVIEW	62



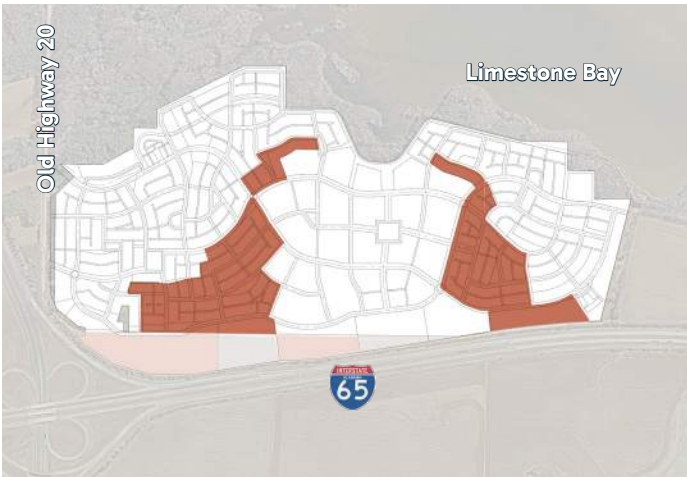
# T4 TRANSECT

## T4 NEIGHBORHOOD (T4 - N)



### 01. INTENT

T4 Neighborhood includes residential neighborhoods intended to be developed in a more intense, urban fashion. Buildings may be taller and set closer to the street to create a denser streetscape. The T4 - Neighborhood features an integrated mixture of single-family, attached, and multi-family housing within walking distance of commercial and neighborhood-scaled open spaces. They feature highly connected street systems with sidewalks and bikeways.

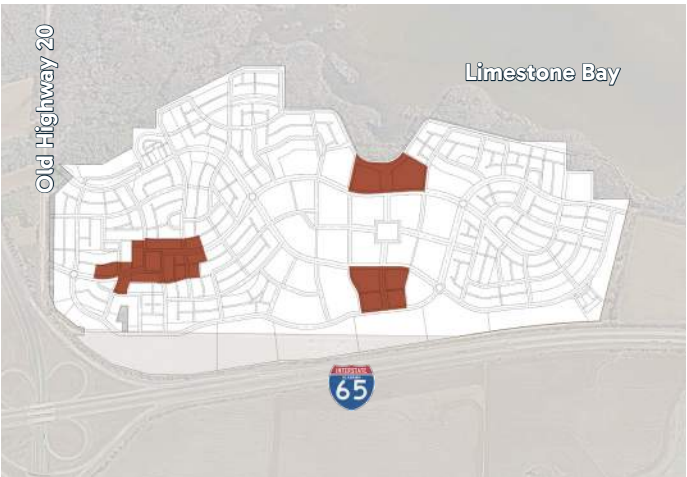


## T4 VILLAGE (T4 - V)



### 02. INTENT

T4 Village is designed to complement the denser residential development pattern of T4 - N with commercial / mixed-use Village Centers that are appropriately scaled to the surrounding residential neighborhood. T4 - V areas are pedestrian oriented with a variety of attached and multi-family residential uses, commercial uses, and open spaces that are focused on community-scaled gatherings and events. Buildings and hardscapes are expected to have significant presence and influence on the streetscape with mid-rise building heights and zero setback build-to zones to create vibrant commercial activity.





# T4 BUILDING TYPES OVERVIEW



**CLUSTER HOUSING**

Series of small, detached or attached dwellings arranged to define a shared court

T4 - N

T4 - V



**DUPLEX**

2 side-by-side or stacked dwelling units within a single building mass

T4 - N

T4 - V



**STACKED FLATS**

12 or more dwelling units, typically with one or more shared entries

T4 - N

T4 - V



**CIVIC**

Used for community, government, cultural, educational, and / or religious uses

T4 - N

T4 - V



**SINGLE FAMILY REAR ENTRY**

Medium-sized, detached dwelling on a single lot

T4 - N

T4 - V



**TOWNHOUSE**

3 - 8 side-by-side dwelling units within a single building mass

T4 - N

T4 - V



**MIXED-USE**

Pedestrian oriented retail and services with dwellings and/or office above

T4 - N

T4 - V



**AMENITIES**

Structures used for recreation, relaxation, or social interactions by residents and their guests

T4 - N

T4 - V



**SINGLE FAMILY FRONT ENTRY**

Medium to large-sized, detached dwelling on a single lot

T4 - N

T4 - V



**CARRIAGE HOUSE FLAT**

1 - 4 dwellings above several garages, often part of multifamily development

T4 - N

T4 - V



**LIVE-WORK**

Dwelling unit with commercial use on the first floor within unit

T4 - N

T4 - V



**ACCESSORY DWELLING UNIT**

Accessory dwelling to a primary residence

T4 - N

T4 - V



**MANOR HOUSE**

2 - 14 dwelling units, typically with a shared entry

T4 - N

T4 - V



**COMMERCIAL**

Pedestrian oriented retail and services

T4 - N

T4 - V



# BUILDING TYPE: CLUSTER HOUSING



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

### 03. BUILDING TYPE STANDARDS

	T4 - N	T4 - V
A Lot Width	18 ft. min.	18 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	10 ft.	10 ft.
D Minimum Rear Setback	0 or 10 ft.	0 or 10 ft.
E Maximum Height	3 stories	3 stories
F Separation between Buildings	0 or 6 ft. min.	0 or 6 ft. min.
G Paseo (Public Access Easement) Width	15 ft. min.	15 ft. min.
H Minimum Open Space Setback	5 ft.	5 ft.

### 04. BUILDING ORIENTATION

- A. Fronts of units may not face the rear of another unit.

### 05. PARKING ACCESS & LOCATION

- A. Rear Loaded with No Street Access allowed if Alley Access is available, Side and / or Rear Surface Parking Allowed.
- B. All units shall be served by a pedestrian circulation system that connects to an existing or planned sidewalk or trail system.

### 06. OPEN SPACE / PASEO (PUBLIC ACCESS EASEMENT)

- A. Common Open Space shall be improved for active or passive recreational use. Amenities & LID stormwater may be included in this space.
- B. 150 square feet of Common Open Space shall be provided per unit.
- C. Cluster Housing Open Space, Small Paseo, and / or Large Paseo Landscape Standards shall apply and meet required PD-H ratios / percentages.
- D. Private Easements for Ingress, Egress, and Public Access shall be referenced on the final plat and must, at a minimum, meet all the requirements and standards outlined in the Westmoore Landing PD-H.



# BUILDING TYPE: CLUSTER HOUSING

07. CLUSTER HOUSING EXAMPLE: 3 UNITS



08. CLUSTER HOUSING EXAMPLE: 12 UNITS



09. CLUSTER HOUSING EXAMPLE: 13 UNITS



10. CLUSTER HOUSING EXAMPLE: 36 UNITS



## 11. NOTES

- A. Clusters may be divided up into fee simple condominium lots provided they meet necessary building and fire code requirements.
- B. Cluster Housing is 3 - 36 units per cluster and may account for a maximum of 20% of the overall development dwelling unit count.
- C. Housing Clusters shall have a 400 ft. minimum distance buffer between any two clusters.
- D. Cluster Housing shall include a minimum of 1 understory tree and at least 2 levels of planting along the front facade area.
- E. Trash collection facilities shall be located behind building setback line and shall be screened from the R.O.W. and adjacent properties by either architectural treatments or vegetative screening.
- F. Area between buildings will be appropriately drained.
- G. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- H. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.



# BUILDING TYPE: SINGLE FAMILY REAR ENTRY



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	25 ft. min.	25 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

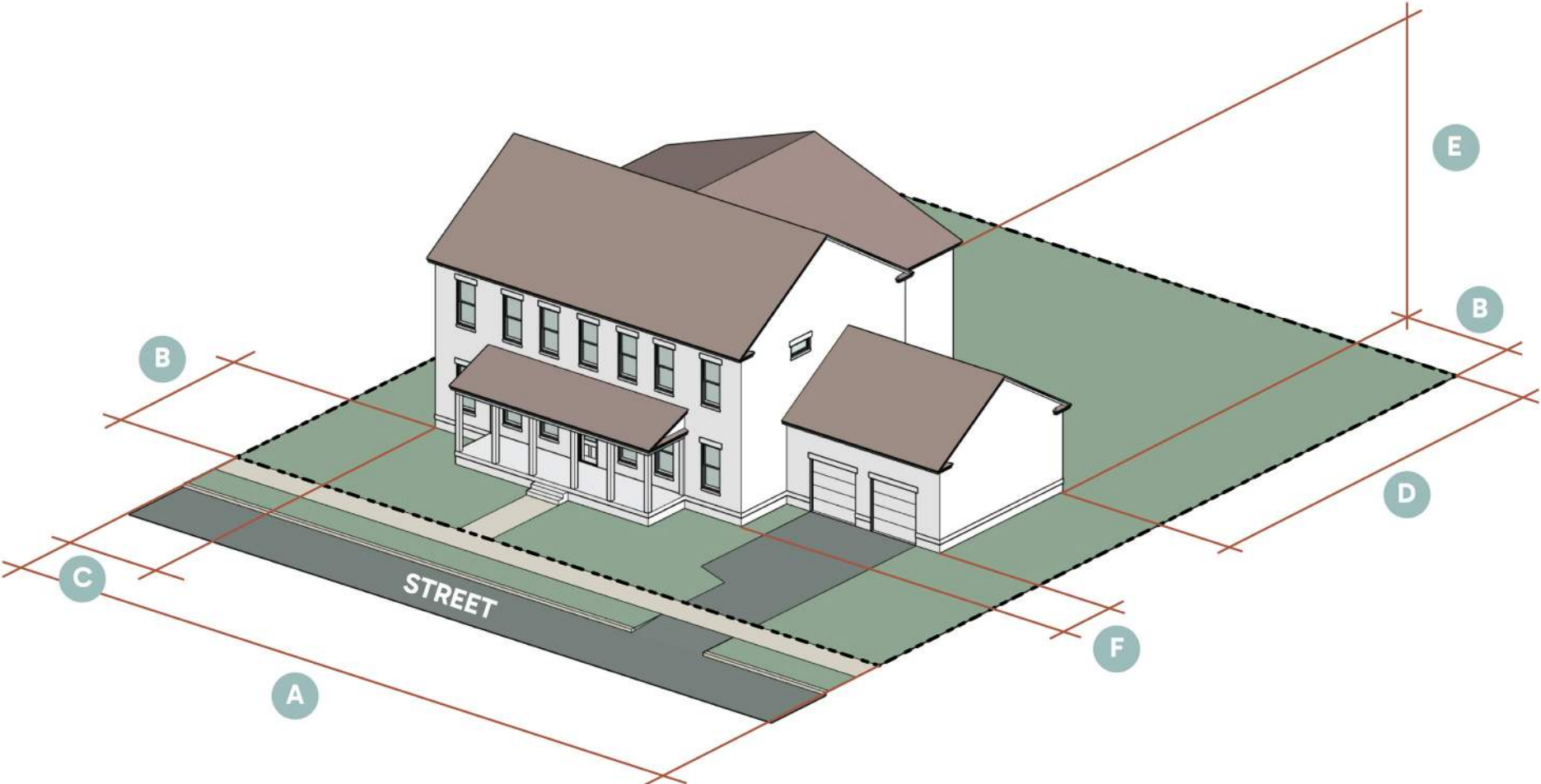
- A. Rear Loaded with Alley Access, when available. Detached/ Attached Garage.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: SINGLE FAMILY FRONT ENTRY



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	55 ft. min.	Not Permitted
B Minimum Street Setback	10 ft.	
C Minimum Side Setback	5 ft.	
D Minimum Rear Setback	0 or 15 ft. min.	
E Maximum Height	4 stories	
F Garage Setback	10 ft. min.	

04. PARKING ACCESS & LOCATION

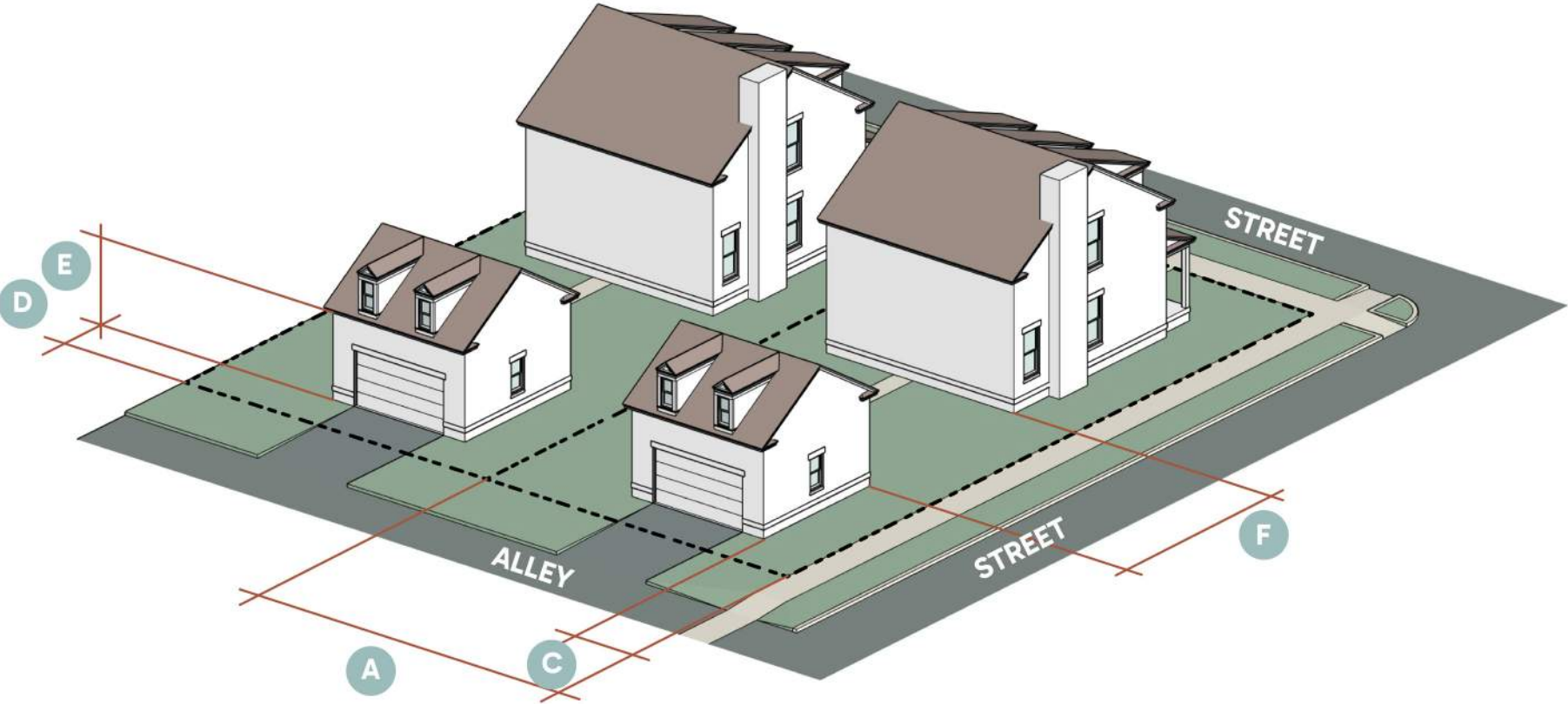
- A. Front or side loaded with street access. Detached or attached garage permitted.
- B. Motorcourts are permitted.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: ACCESSORY DWELLING UNIT



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	0 or 5 ft.	0 or 3 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories
F Separation between Buildings	10 ft. min.	5 ft. min.

## 04. PERMITTED PRIMARY BUILDING TYPES

- A. Single Family Rear Entry

B. Single Family Front Entry

C. Duplex

G. ADUs are not permitted on fee simple cluster lots.
- D. Townhouse

E. Manor House

F. Live-Work

## 05. PARKING ACCESS & LOCATION

- A. Shall be located underneath, behind, or beside building only on interior lot lines.

## 06. NOTES

- A. Size of ADU not to exceed 60% of primary building habitable floor area.

B. ADU's may be attached, semi detached, or detached structures. Separation between buildings does not apply when ADU is attached to primary building. Where applicable, open air breezeways may encroach on separation between buildings. ADU's shall be subordinate in mass, size, and height to the primary building. The architectural style shall be consistent or complementary in design, character, and style to the primary building.

C. Frontage types may encroach into a required street setback as specified in G.96.01.A.

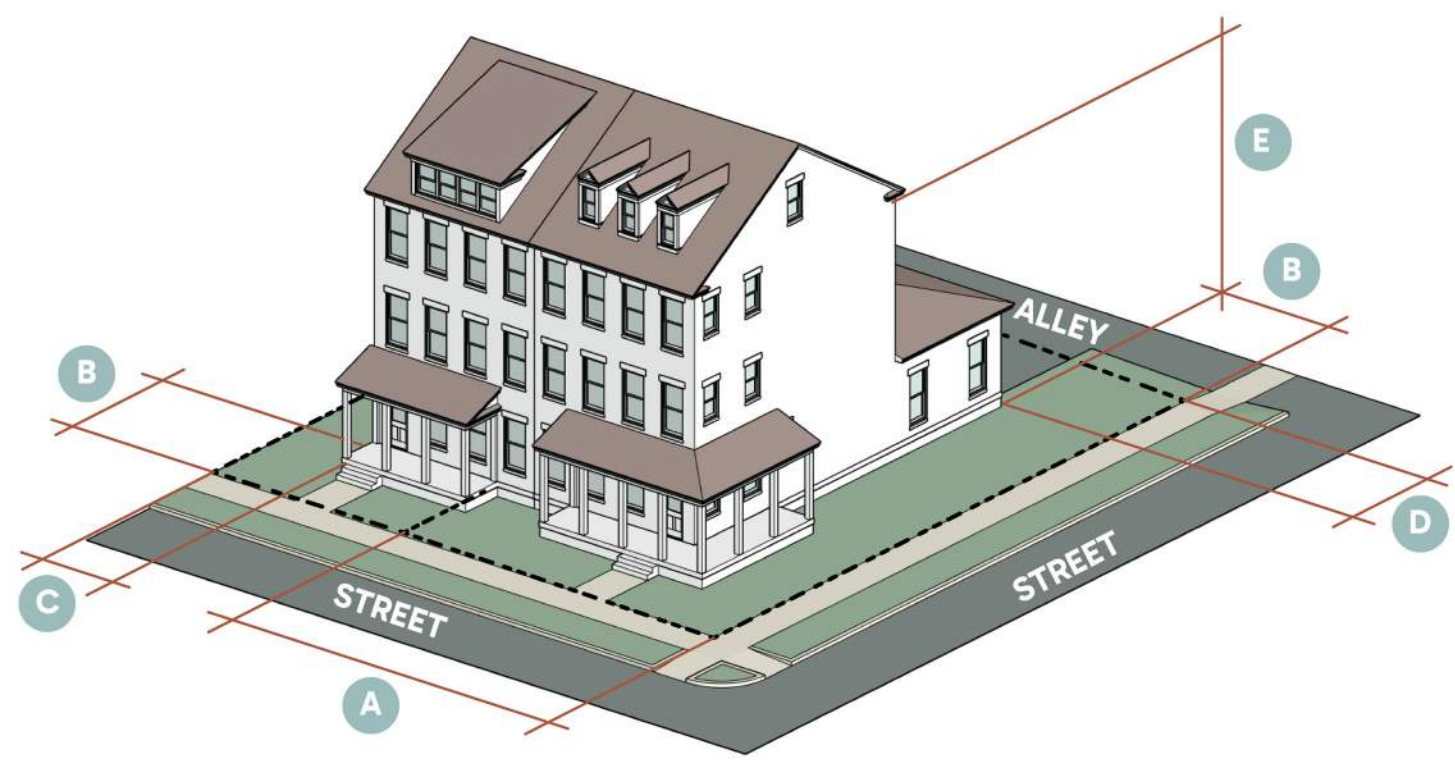
D. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.

E. ADU's shall be labeled on individual site plans.

F. ADU's shall provide one off-street parking space.



# BUILDING TYPE: DUPLEX



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	16 ft. min. per unit	16 ft. min. per unit
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: TOWNHOUSE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	14 ft. min. per unit	14 ft. min. per unit
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

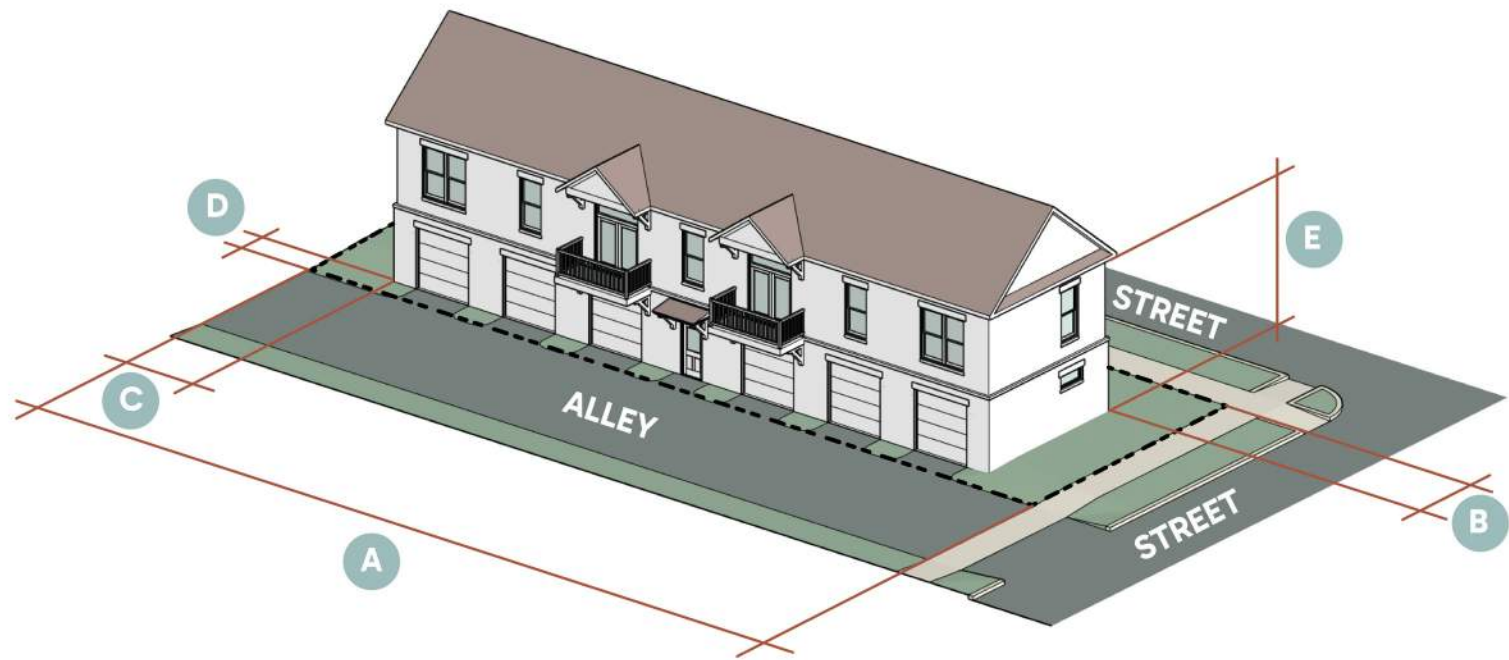
- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Townhouses can be 3 - 8 units in a building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.



# BUILDING TYPE: CARRIAGE HOUSE FLAT



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	24 ft. min.	24 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

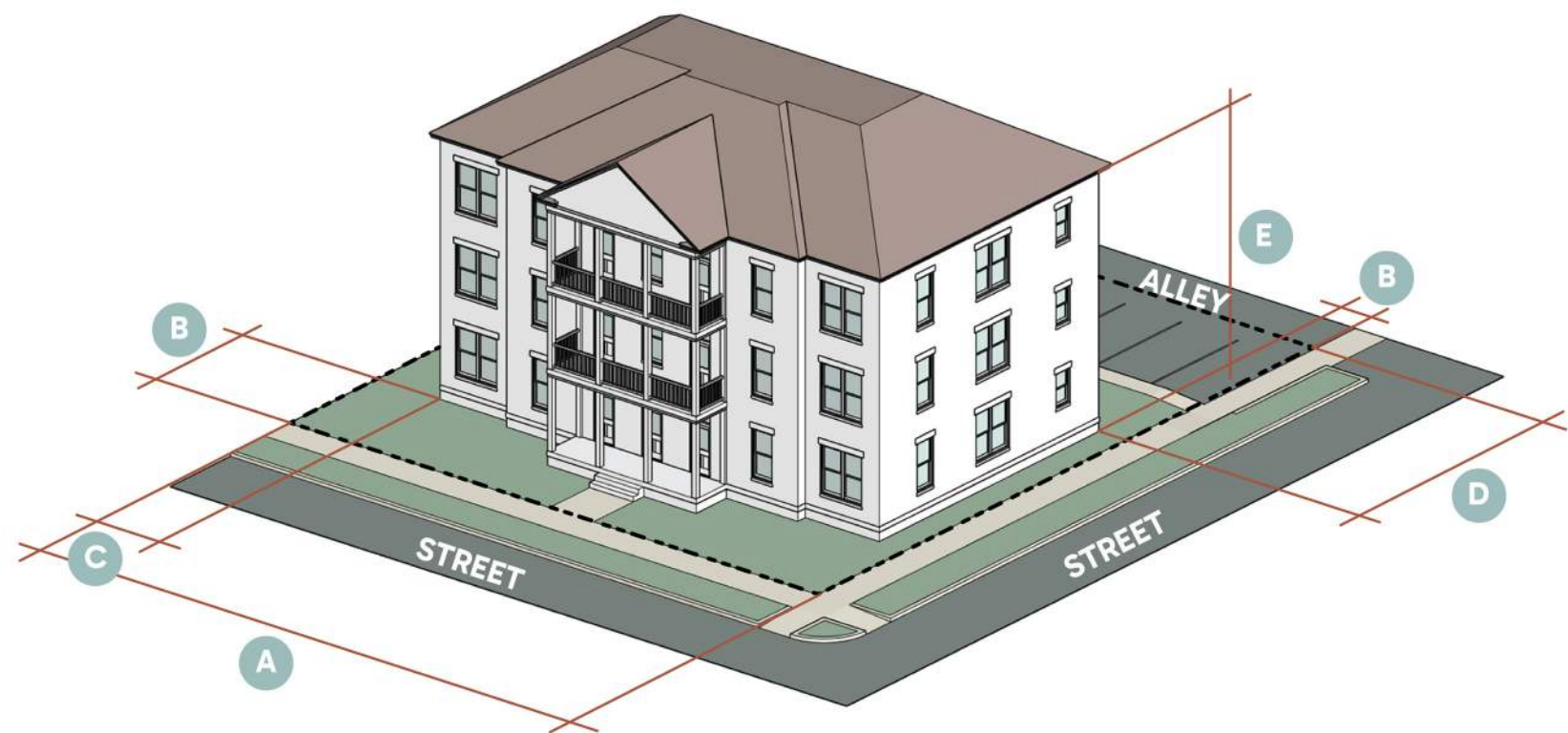
- A. Rear loaded with alley access. Garage parking is located below the unit on the ground floor.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MANOR HOUSE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	40 ft. min.	40 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, Side and / or rear surface parking allowed.

05. NOTES

- A. Manor Houses can be 2 - 14 units per building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: STACKED FLATS



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	70 ft. min.	70 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

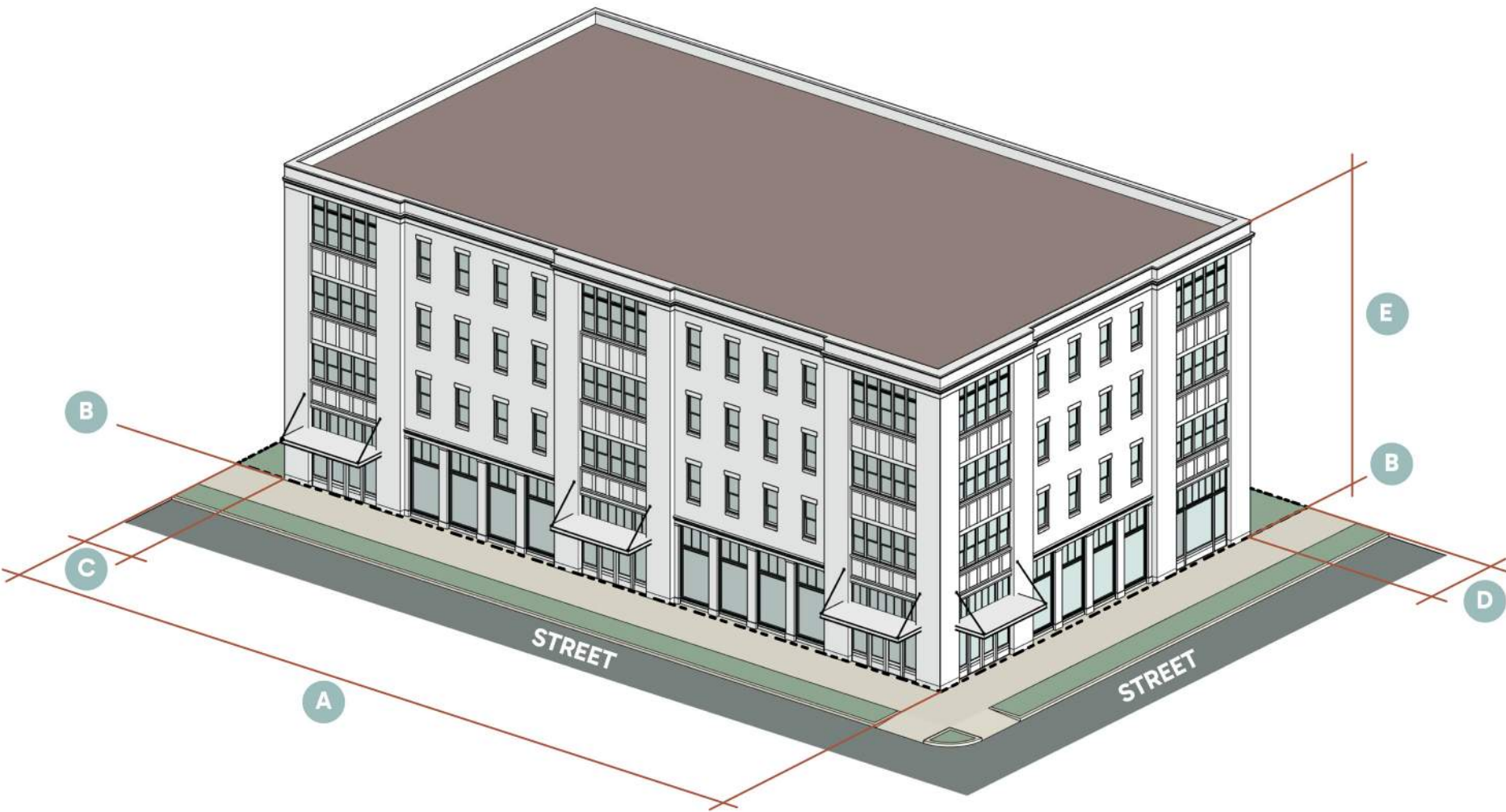
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Stacked Flats can be 12+ units per building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MIXED-USE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	Not Permitted	N/A
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 or 5 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		4 stories

04. PARKING ACCESS & LOCATION

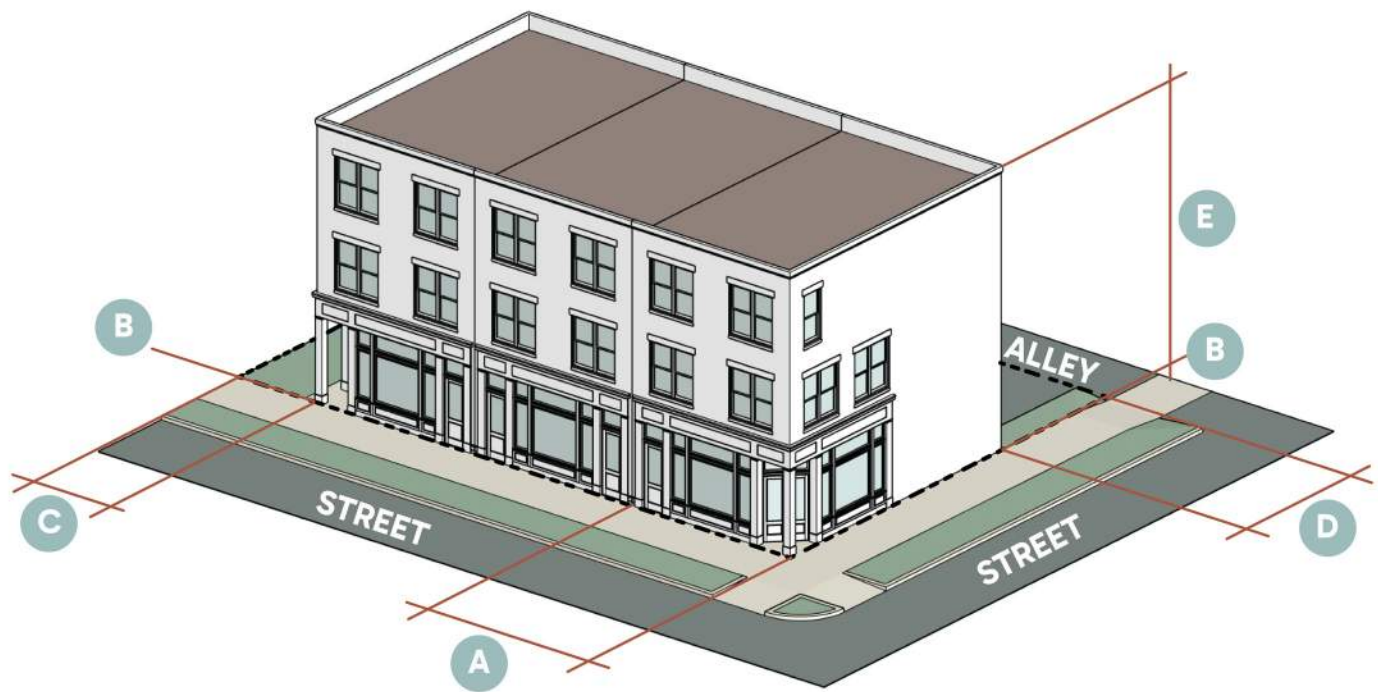
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. There are no min. square footage requirements for residential units over commercial, however no more than 50% of units will be less than 650 sf.



# BUILDING TYPE: LIVE-WORK



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	Not Permitted	18 ft. min. per unit
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 or 5 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		4 stories

## 04. PARKING ACCESS & LOCATION

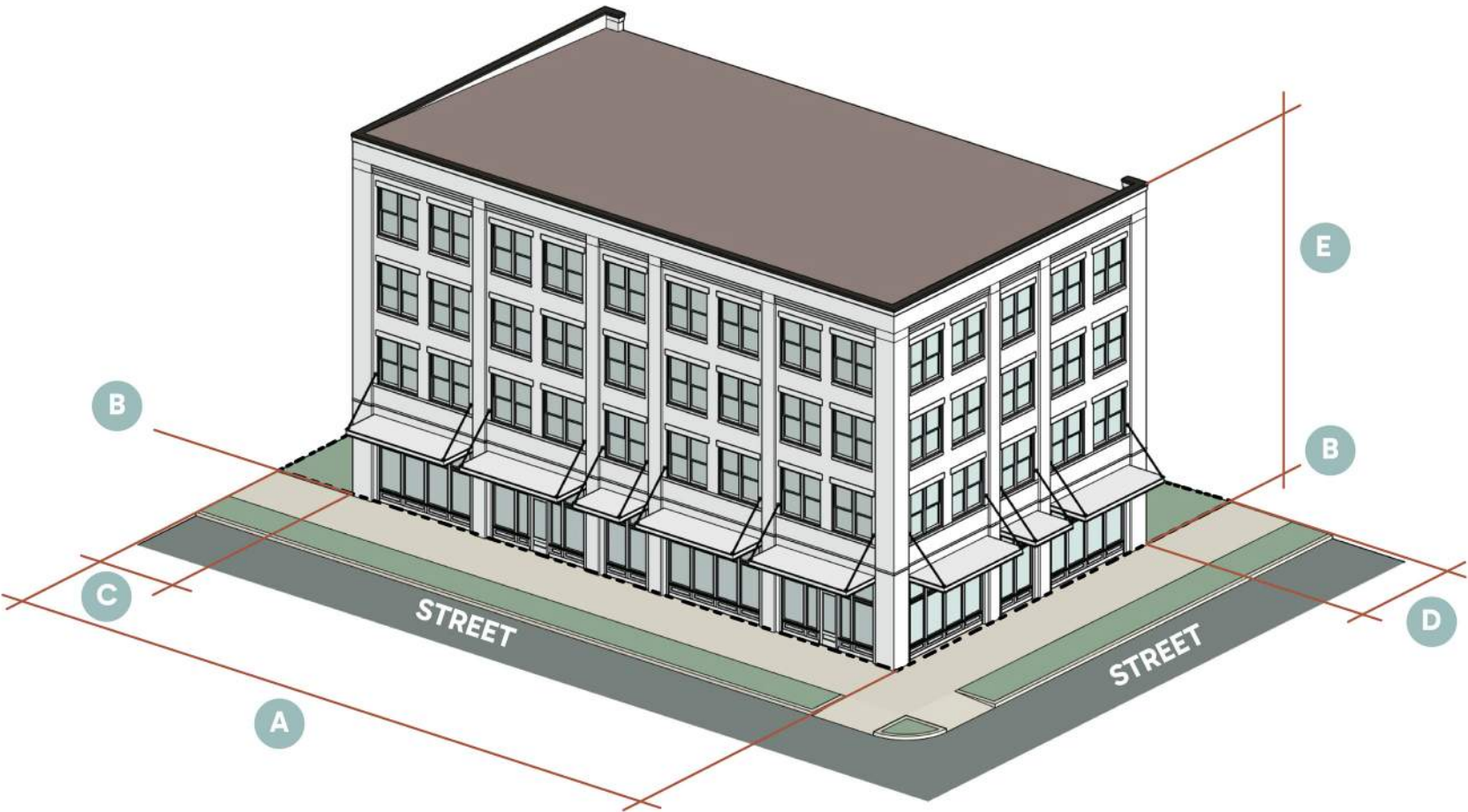
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.



# BUILDING TYPE: COMMERCIAL



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	Not Permitted	N/A
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		4 stories

## 04. PARKING ACCESS & LOCATION

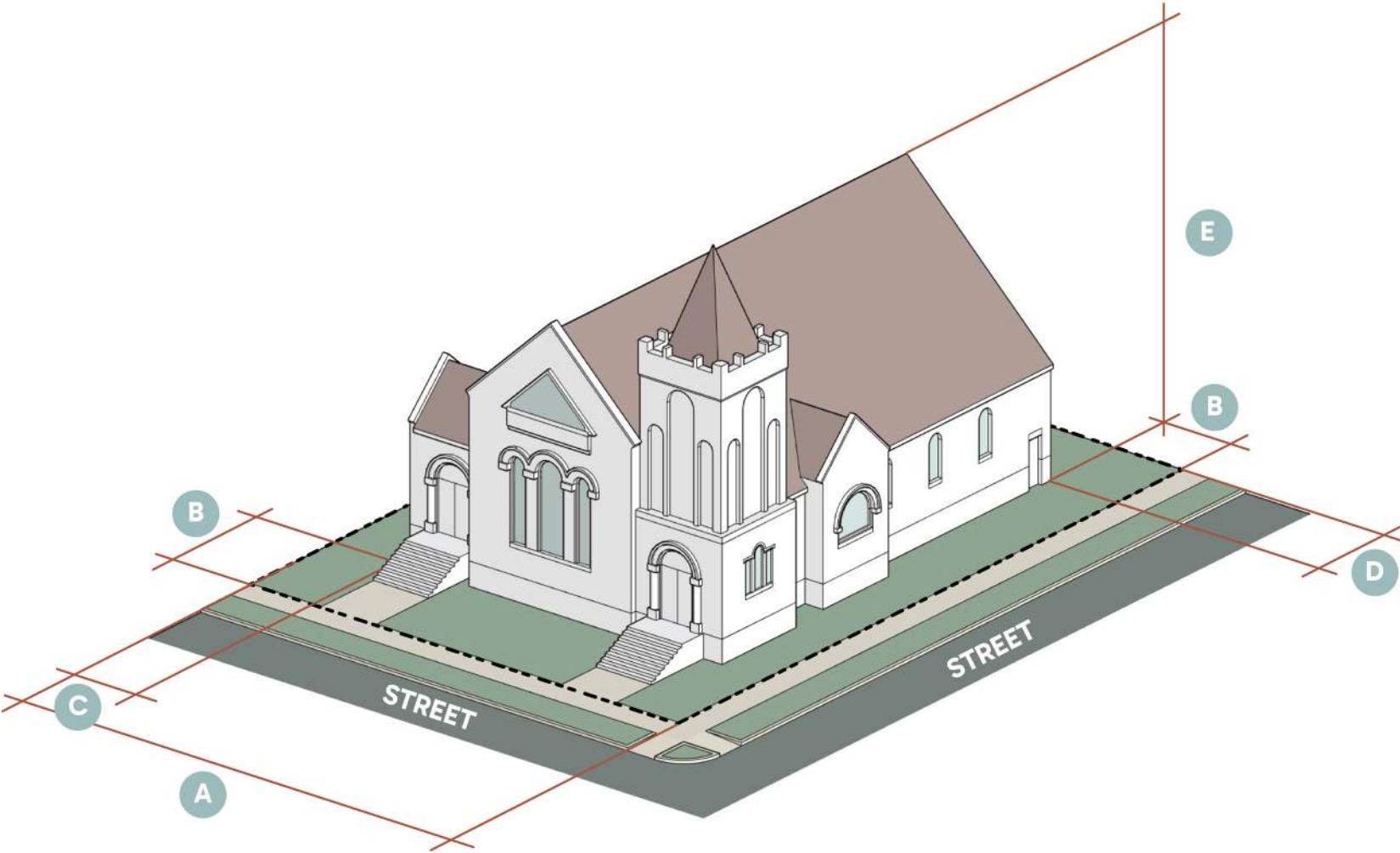
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.



# BUILDING TYPE: CIVIC



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

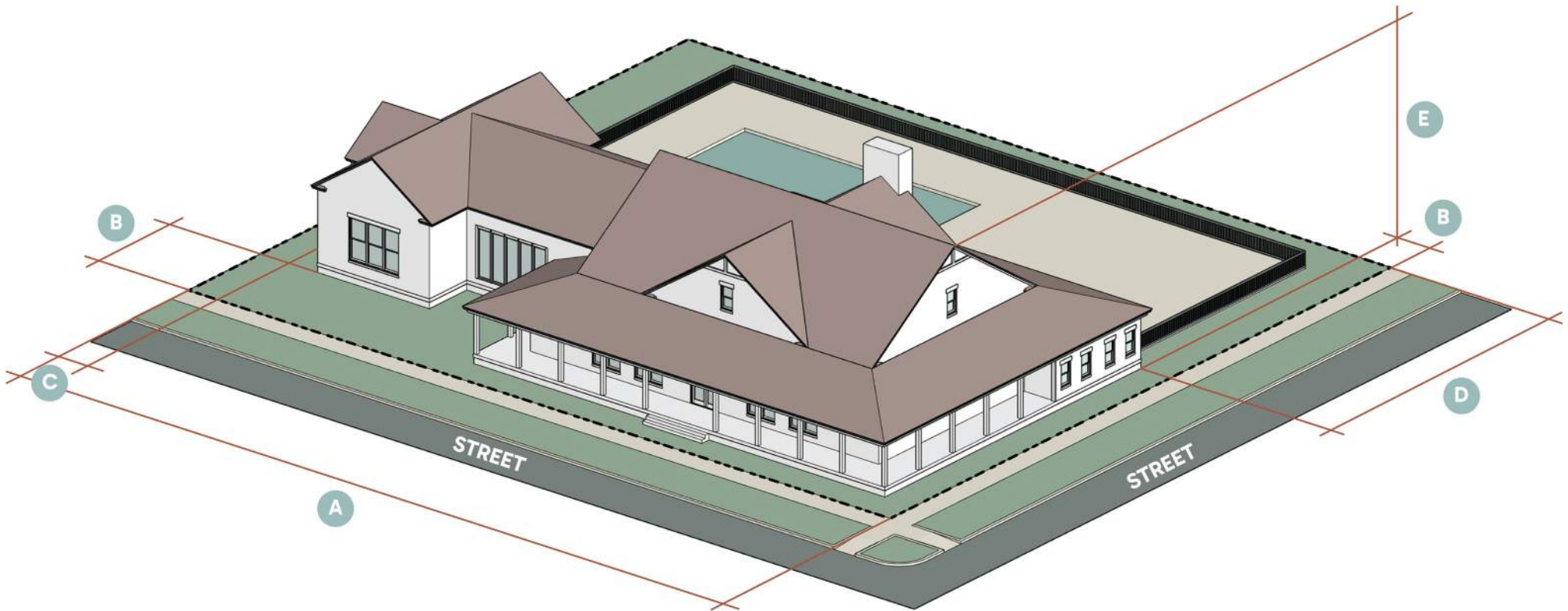
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: AMENITIES



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T4 - N	T4 - V
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# T<sub>5</sub> REGULATORY STANDARDS

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T5 TRANSECT	79
T5 BUILDING TYPES OVERVIEW	80



# T5 TRANSECT

## T5 NEIGHBORHOOD (T5 - N)



### 01. INTENT

T5 Neighborhood is the densest residential transect at Westmoore Landing. It is intended to be a vibrant neighborhood of diverse buildings that directly address active streetscapes and provide the necessary population density to support the commercial and office uses of the adjacent T5 - TC. T5 - N buildings are anticipated to include multi-story, mid-rise with parapet roofs and may include structured parking solutions. Buildings in T5 - N generally directly front streetscapes and open spaces with shallow setbacks and residential stoops and / or commercial ground floors that enliven the street with pedestrian activity.



## T5 TOWN CENTER (T5 - TC)



### 02. INTENT

T5 Town Center is a gathering place for residents and visitors at a regional scale, where people can live, work, shop, and connect. Residents and visitors meet at centers to engage in commerce, civic, and recreational activities on a grander scale than can be found in their individual neighborhoods or communities. Building forms are typically mid-rise with parapet wall roof forms and storefronts at the street level. Buildings are built to the sidewalk with active pedestrian zones along the streets. Parking can be head-in angle parking, parallel, structured, and surface lots that are concealed from street views.





# T5 BUILDING TYPES OVERVIEW



**CLUSTER HOUSING**

Series of small, detached or attached dwellings arranged to define a shared court

T5 - N T5 - TC



**TOWNHOUSE**

3 - 8 side-by-side dwelling units within a single building mass

T5 - N T5 - TC



**MIXED-USE**

Pedestrian oriented retail and services with dwellings and/or office above

T5 - N T5 - TC



**AMENITIES**

Structures used for recreation, relaxation, or social interactions by residents and their guests

T5 - N T5 - TC



**SINGLE FAMILY REAR ENTRY**

Medium-sized, detached dwelling on a single lot

T5 - N T5 - TC



**CARRIAGE HOUSE FLAT**

1 - 4 dwellings above several garages, often part of multifamily development

T5 - N T5 - TC



**LIVE-WORK**

Dwelling unit with commercial use on the first floor within unit

T5 - N T5 - TC



**ACCESSORY DWELLING UNIT**

Accessory dwelling to a primary residence

T5 - N T5 - TC



**MANOR HOUSE**

2 - 14 dwelling units, typically with a shared entry

T5 - N T5 - TC



**COMMERCIAL**

Pedestrian oriented retail and services

T5 - N T5 - TC



**DUPLEX**

2 side-by-side or stacked dwelling units within a single building mass

T5 - N T5 - TC



**STACKED FLATS**

12 or more dwelling units, typically with one or more shared entries

T5 - N T5 - TC



**CIVIC**

Used for community, government, cultural, educational, and / or religious uses

T5 - N T5 - TC



# BUILDING TYPE: CLUSTER HOUSING



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	16 ft. min.	16 ft. min.
B Minimum Street Setback	5 ft.	5 ft.
C Minimum Side Setback	0 or 3 ft.	0 or 3 ft.
D Minimum Rear Setback	0 or 10 ft.	0 or 10 ft.
E Maximum Height	4 stories	4 stories
F Separation between Buildings	0 or 6 ft. min.	0 or 6 ft. min.
G Paseo (Public Access Easement) Width	15 ft. min.	15 ft. min.
H Minimum Open Space Setback	5 ft.	5 ft.

## 04. BUILDING ORIENTATION

- A. Fronts of units may not face the rear of another unit.

## 05. PARKING ACCESS & LOCATION

- A. Rear Loaded with No Street Access allowed if Alley Access is available, Side and / or Rear Surface Parking Allowed.
- B. All units shall be served by a pedestrian circulation system that connects to an existing or planned sidewalk or trail system.

## 06. OPEN SPACE / PASEO (PUBLIC ACCESS EASEMENT)

- A. Common Open Space shall be improved for active or passive recreational use. Amenities & LID stormwater may be included in this space.
- B. 150 square feet of Common Open Space shall be provided per unit.
- C. Cluster Housing Open Space, Small Paseo, and / or Large Paseo Landscape Standards shall apply and meet required PD-H ratios / percentages.
- D. Private Easements for Ingress, Egress, and Public Access shall be referenced on the final plat and must, at a minimum, meet all the requirements and standards outlined in the Westmoore Landing PD-H.



# BUILDING TYPE: CLUSTER HOUSING

07. CLUSTER HOUSING EXAMPLE: 3 UNITS



08. CLUSTER HOUSING EXAMPLE: 12 UNITS



09. CLUSTER HOUSING EXAMPLE: 13 UNITS



10. CLUSTER HOUSING EXAMPLE: 36 UNITS NOTES

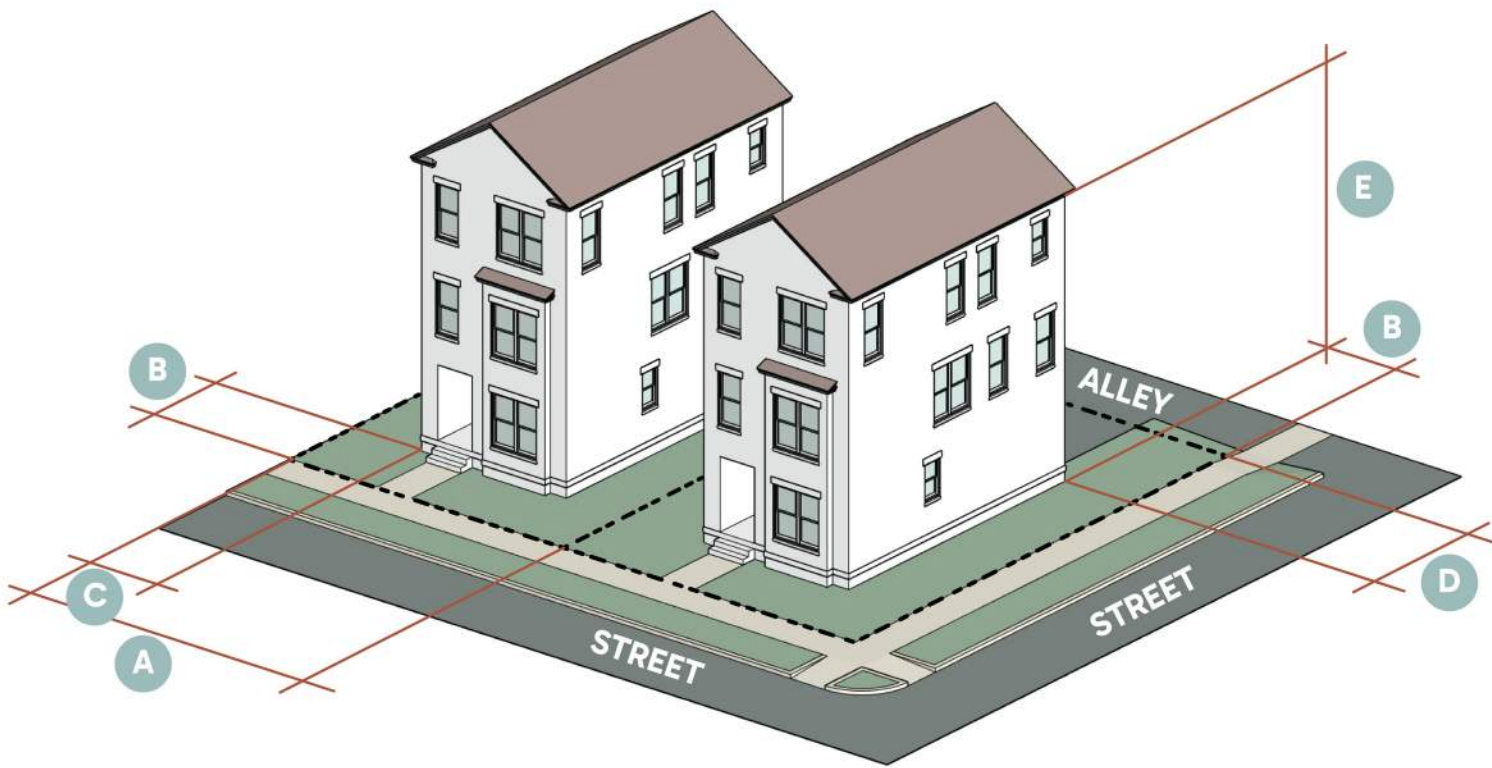


## 11. NOTES

- A. Clusters may be divided up into fee simple condominium lots provided they meet necessary building and fire code requirements.
- B. Cluster Housing is 3 - 36 units per cluster and may account for a maximum of 20% of the overall development dwelling unit count.
- C. Housing Clusters shall have a 400 ft. minimum distance buffer between any two clusters.
- D. Cluster Housing shall include a minimum of 1 understory tree and at least 2 levels of planting along the front facade area.
- E. Trash collection facilities shall be located behind building setback line and shall be screened from the R.O.W. and adjacent properties by either architectural treatments or vegetative screening.
- F. Area between buildings will be appropriately drained.
- G. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- H. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.



# BUILDING TYPE: SINGLE FAMILY REAR ENTRY



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	25 ft. min.	Not Permitted
B Minimum Street Setback	5 ft.	
C Minimum Side Setback	5 ft.	
D Minimum Rear Setback	0 or 15 ft. min.	
E Maximum Height	4 stories	

04. PARKING ACCESS & LOCATION

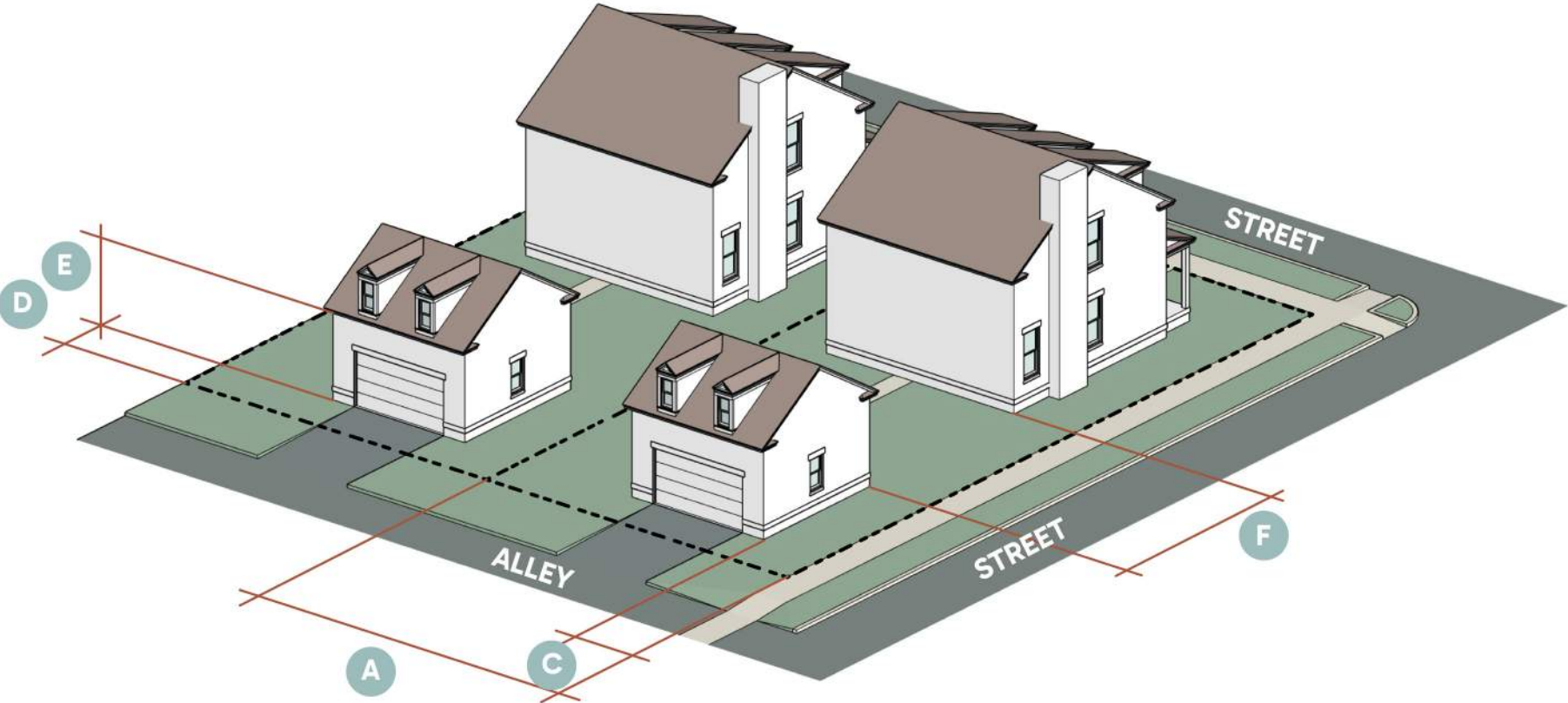
- A. Rear Loaded with Alley Access, when available. Detached/ Attached Garage.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: ACCESSORY DWELLING UNIT



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	0 or 3 ft.	0 or 3 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	3 stories	3 stories
F Separation between Buildings	10 ft. min.	5 ft. min.

## 04. PERMITTED PRIMARY BUILDING TYPES

- A. Single Family Rear Entry
- B. Single Family Front Entry
- C. Duplex
- D. Townhouse
- E. Manor House
- F. Live-Work
- G. ADUs are not permitted on fee simple cluster lots.

## 05. PARKING ACCESS & LOCATION

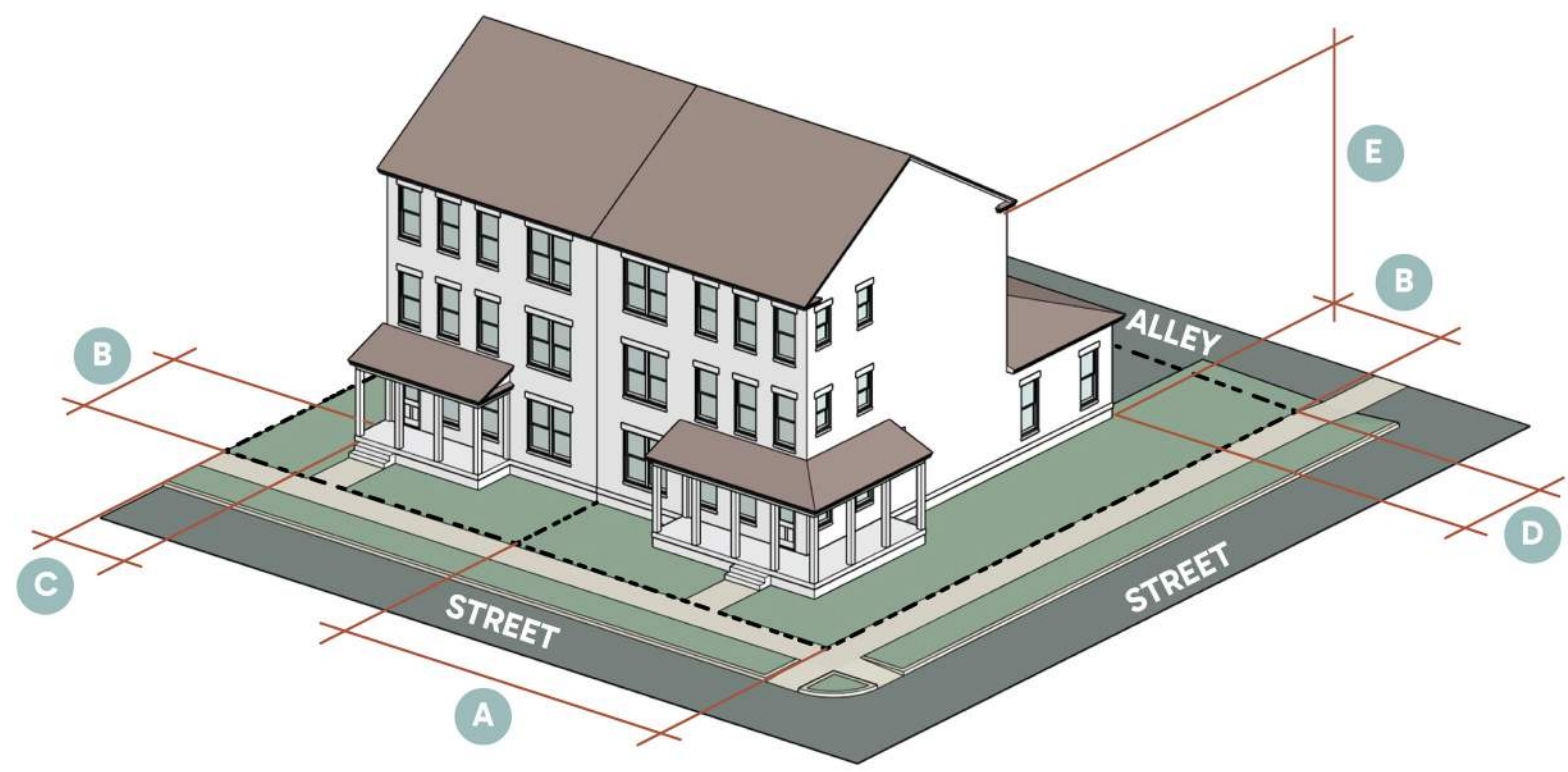
- A. Shall be located underneath, behind, or beside building only on interior lot lines.

## 06. NOTES

- A. Size of ADU not to exceed 60% of primary building habitable floor area.
- B. ADU's may be attached, semi detached, or detached structures. Separation between buildings does not apply when ADU is attached to primary building. Where applicable, open air breezeways may encroach on separation between buildings. ADU's shall be subordinate in mass, size, and height to the primary building. The architectural style shall be consistent or complementary in design, character, and style to the primary building.
- C. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- D. Raised foundations shall be a minimum of 18" with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.
- E. ADU's shall be labeled on individual site plans.
- F. ADU's shall provide one off-street parking space.



# BUILDING TYPE: DUPLEX



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	16 ft. min. per unit	16 ft. min. per unit
B Minimum Street Setback	5 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft. min.	0 or 15 ft. min.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.

# BUILDING TYPE: TOWNHOUSE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	11 ft. min. per unit	11 ft. min. per unit
B Minimum Street Setback	5 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	4 stories	4 stories

04. PARKING ACCESS & LOCATION

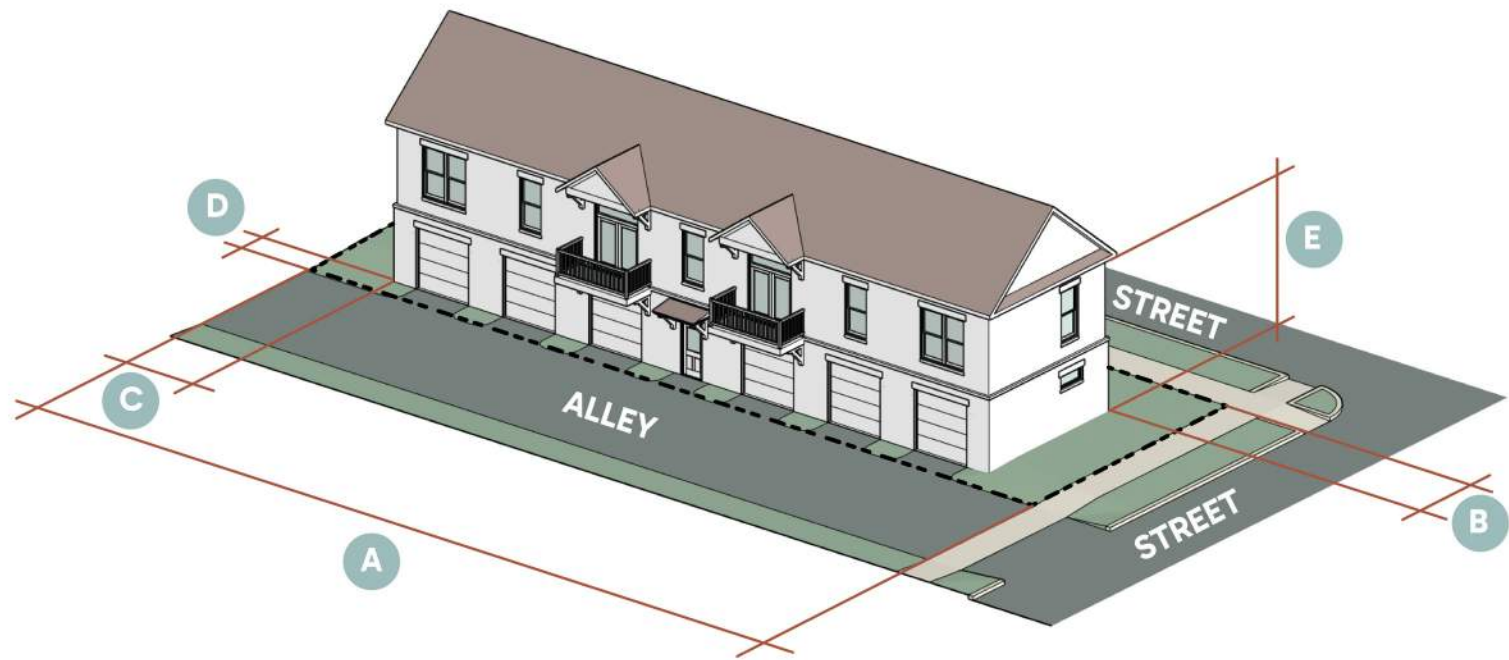
- A. Rear loaded with alley access. Surface lot or detached or attached garage permitted.

05. NOTES

- A. Townhouses can be 3 - 8 units in a building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: CARRIAGE HOUSE FLAT



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	24 ft. min.	24 ft. min.
B Minimum Street Setback	5 ft.	5 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	3 stories	3 stories

04. PARKING ACCESS & LOCATION

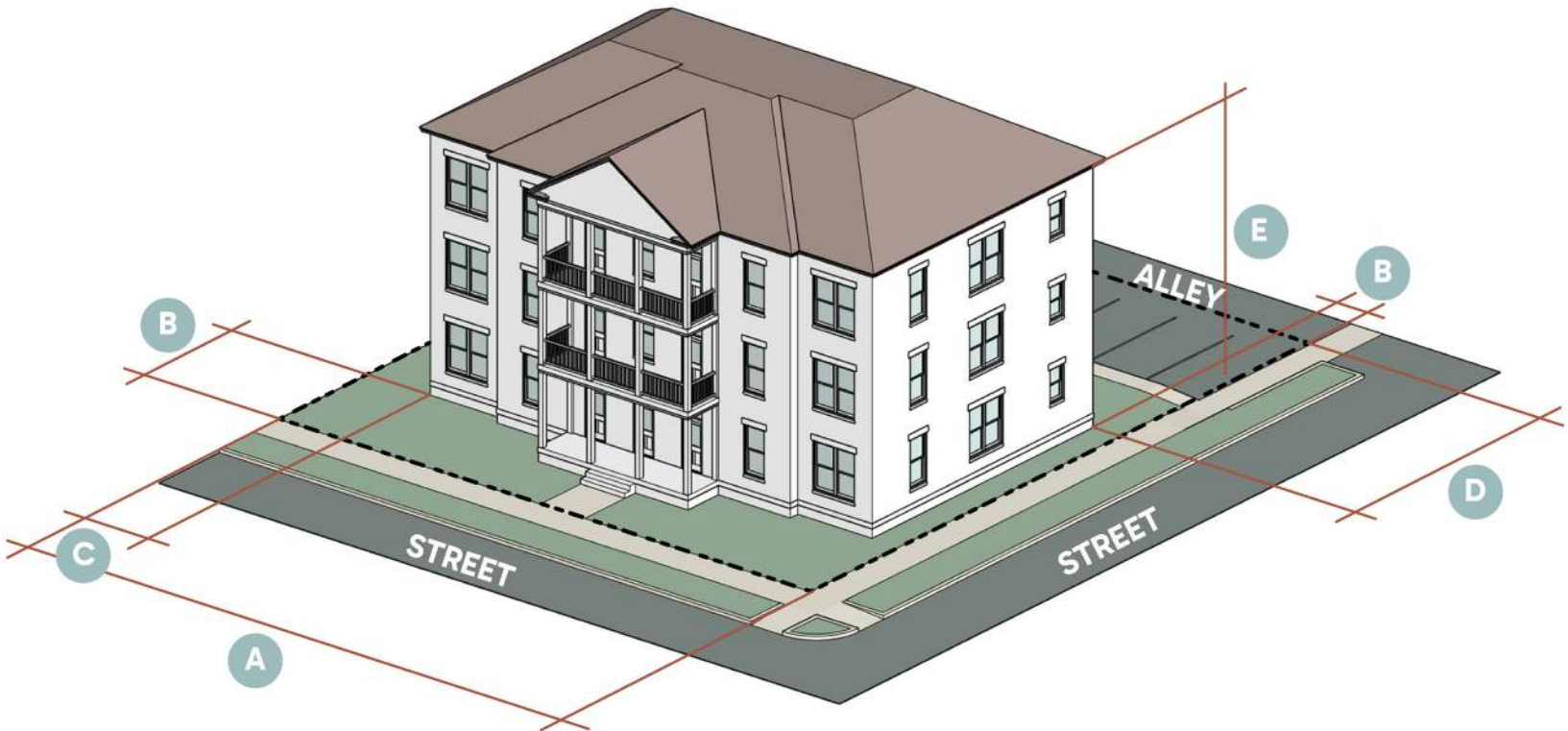
- A. Rear loaded with alley access. Garage parking is located below the unit on the ground floor.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MANOR HOUSE



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES



03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	40 ft. min.	40 ft. min.
B Minimum Street Setback	5 ft.	5 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 or 15 ft.	0 or 15 ft.
E Maximum Height	4 stories	4 stories

## 04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, Side and / or rear surface parking allowed.

## 05. NOTES

- A. Manor Houses can be 2 - 14 units per building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: STACKED FLATS



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	70 ft. min.	70 ft. min.
B Minimum Street Setback	10 ft.	5 ft.
C Minimum Side Setback	5 ft.	5 ft.
D Minimum Rear Setback	0 ft.	0 ft.
E Maximum Height	8 stories	8 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Stacked Flats can be 12+ units per building.
- B. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- C. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: MIXED-USE



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES



03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	N/A	N/A
B Minimum Street Setback	0 ft.	0 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 ft.	0 ft.
E Maximum Height	8 stories	8 stories

04. PARKING ACCESS & LOCATION

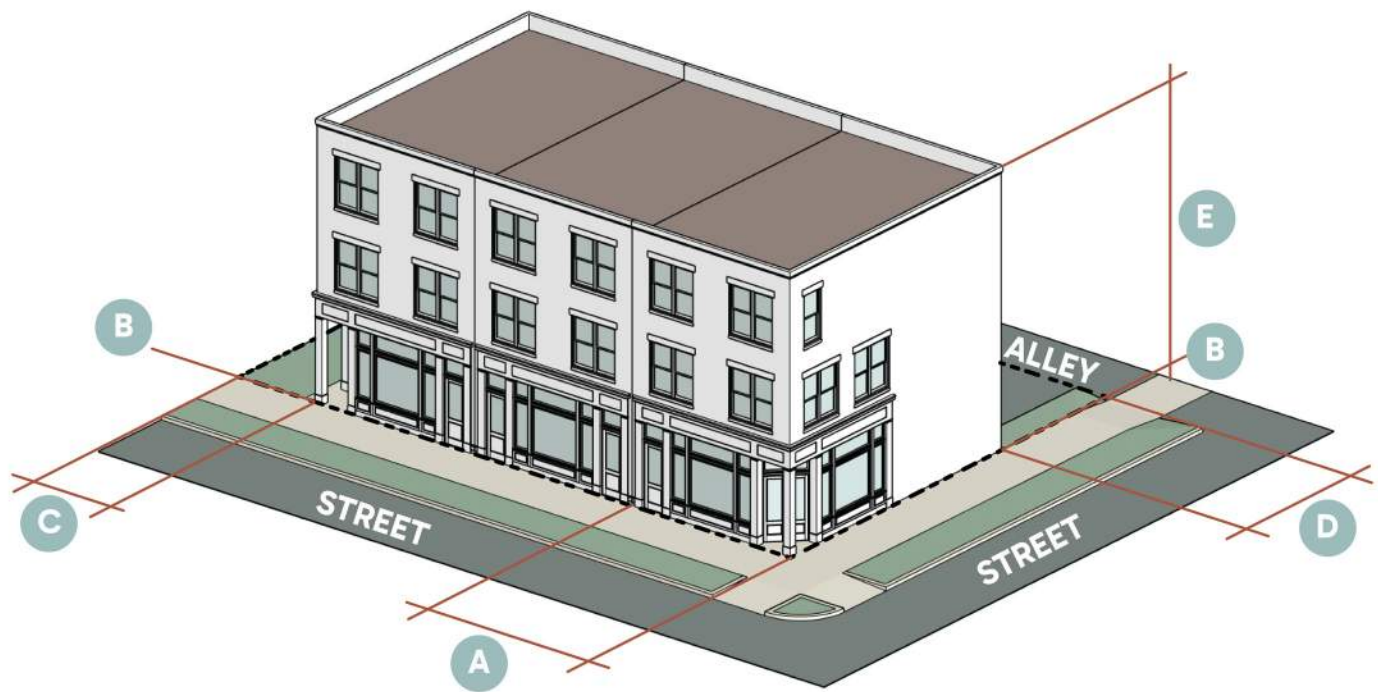
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. There are no min. square footage requirements for residential units over commercial, however no more than 50% of units will be less than 650 sf.



# BUILDING TYPE: LIVE-WORK



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	18 ft. min. per unit	18 ft. min. per unit
B Minimum Street Setback	0 ft.	0 ft.
C Minimum Side Setback	0 or 5 ft.	0 or 5 ft.
D Minimum Rear Setback	0 ft.	0 ft.
E Maximum Height	4 stories	4 stories

## 04. PARKING ACCESS & LOCATION

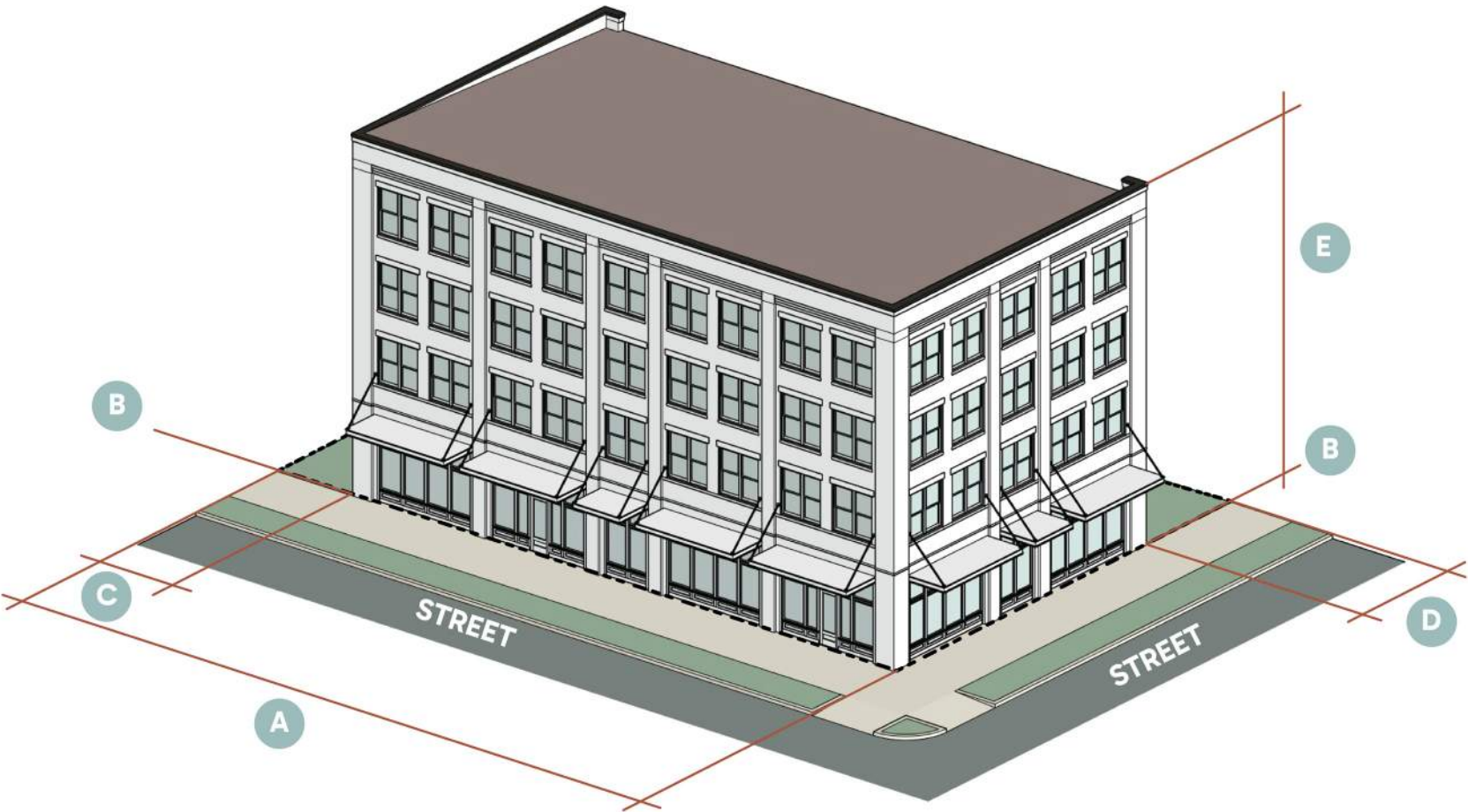
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.



# BUILDING TYPE: COMMERCIAL



## 01. BUILDING TYPE DIAGRAM



## 02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	Not Permitted	N/A
B Minimum Street Setback		0 ft.
C Minimum Side Setback		0 ft.
D Minimum Rear Setback		0 ft.
E Maximum Height		8 stories

## 04. PARKING ACCESS & LOCATION

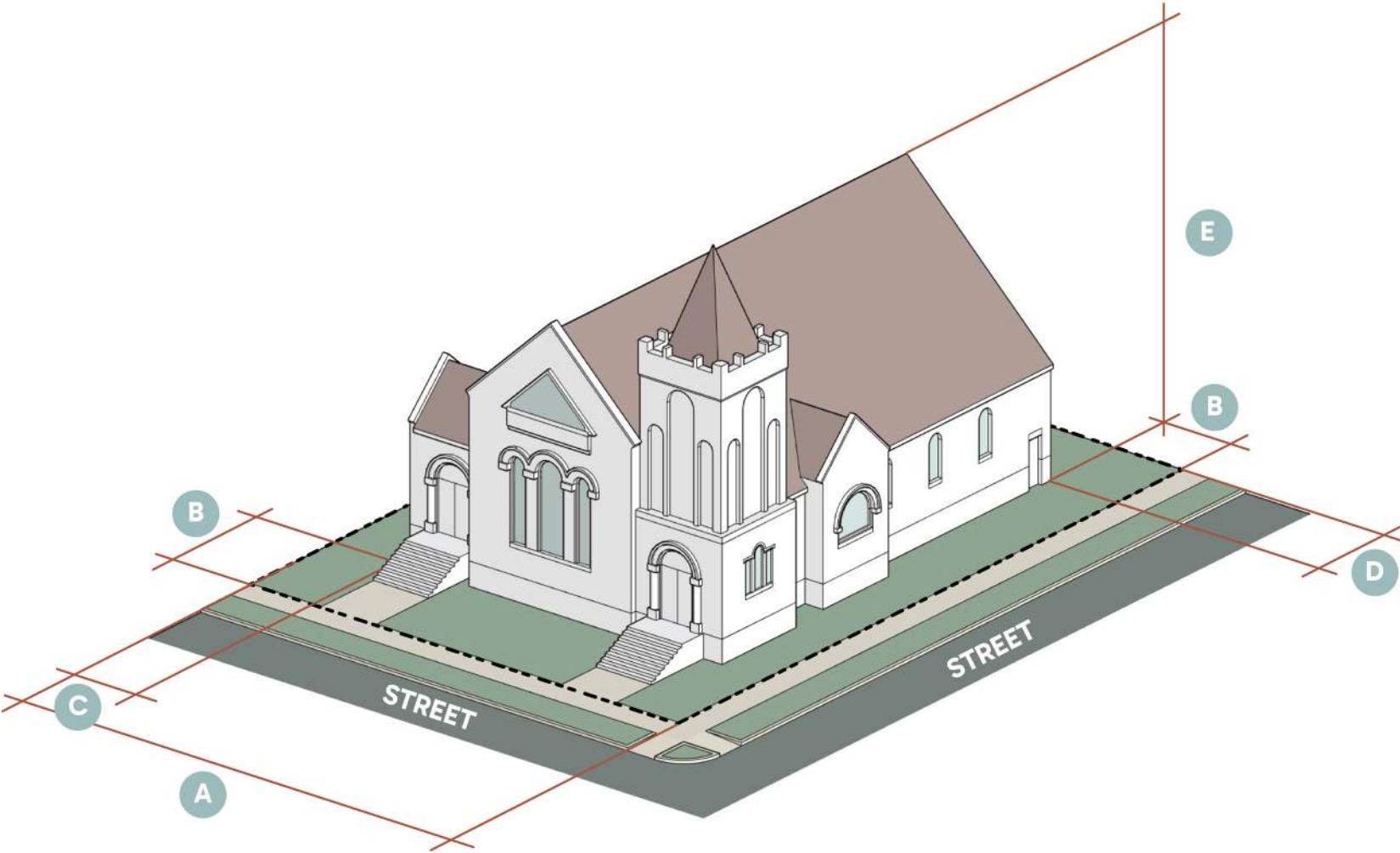
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

## 05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.



# BUILDING TYPE: CIVIC



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	8 stories	8 stories

04. PARKING ACCESS & LOCATION

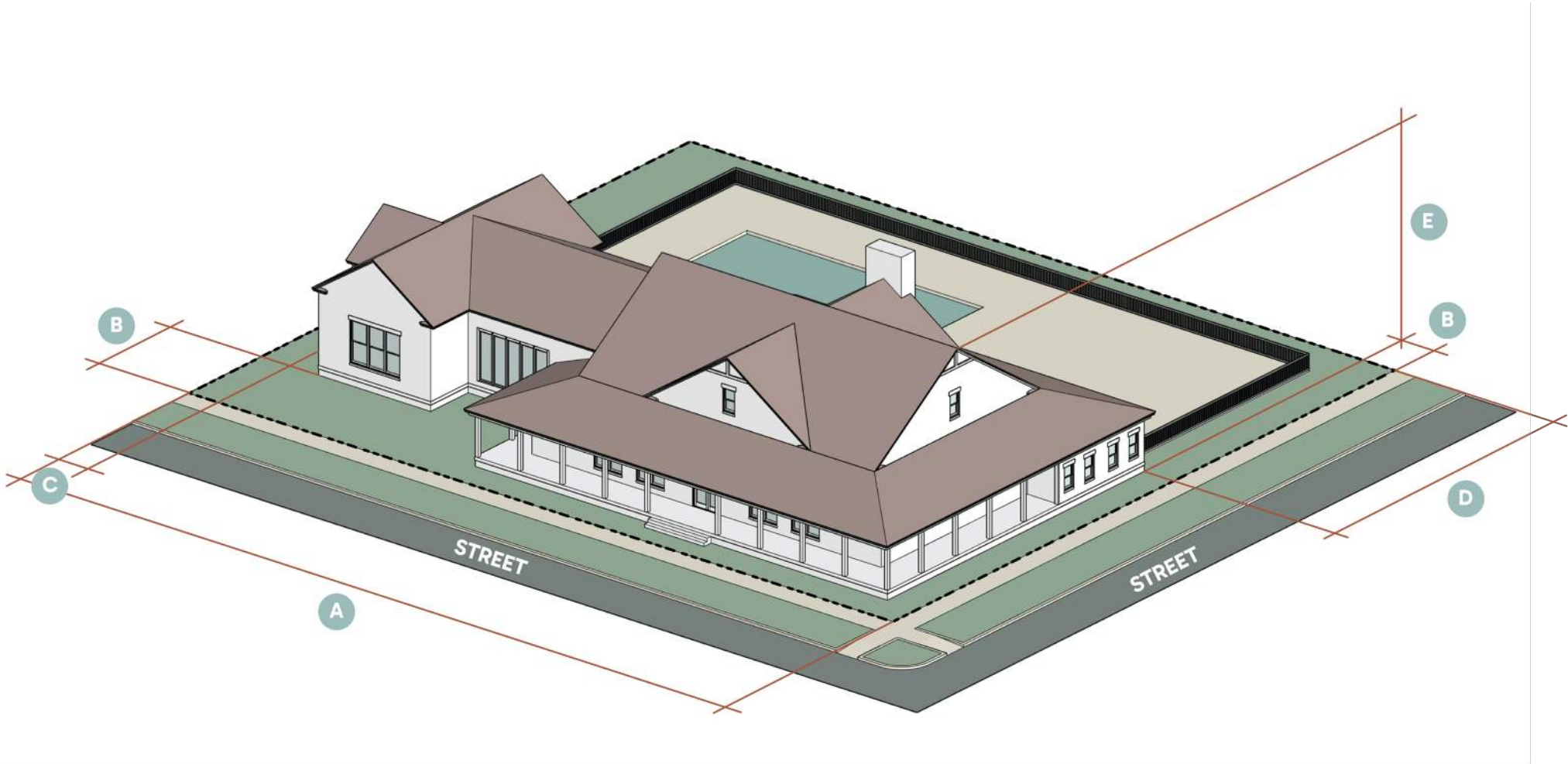
- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# BUILDING TYPE: AMENITIES



01. BUILDING TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. BUILDING TYPE STANDARDS		
	T5 - N	T5 - TC
A Lot Width	N/A	N/A
B Minimum Street Setback	N/A	N/A
C Minimum Side Setback	N/A	N/A
D Minimum Rear Setback	N/A	N/A
E Maximum Height	8 stories	8 stories

04. PARKING ACCESS & LOCATION

- A. Rear loaded with no street access allowed if alley access is available, side and / or rear surface parking allowed.

05. NOTES

- A. Frontage types may encroach into a required street setback as specified in G.96.01.A.
- B. Raised foundations shall be a minimum of 18” with the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.





# SUPPLEMENTARY DESIGN STANDARDS

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GENERAL DESIGN STANDARDS	96
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# GENERAL DESIGN STANDARDS

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**01. SETBACK ENCROACHMENT ZONE (T3-N, T3-H, T4-N, T4-V, T5-N, T5-TC)**

- A. Frontage types may encroach per regulations on pages 97 - 105.
- B. Porches or stoops may encroach into the setback to within 2 feet of the property line (steps may extend to property line) provided that such encroachment may not extend into a utility easement and may not adversely impact drainage.
- C. Upper floor encroachments may extend to property line.
- D. Architectural elements such as bay windows, pilasters, chimneys, roof overhangs, cornices, window and door surrounds, sills, chimneys flues, and other architectural elements may encroach into setback to within 2 feet of the property line (steps may extend to property line) provided that such encroachment may not extend into a utility easement and may not adversely impact drainage.

**02. HEIGHT (T3-N, T3-H, T4-N, T4-V, T5-N, T5-TC)**

- A. Minimum Floor-to-Floor Height
  - 01. Ground Floor Non-Residential: 9 feet
  - 02. Ground Floor Residential: 9 feet
    - a. Mezzanines shall not be considered a story for the purposes of calculating overall number of stories when it is less than 65% of the floor area below it.

**03. SITE LAYOUT (T3-N, T3-H, T4-N, T4-V, T5-N, T5-TC)**

- A. Buildings shall provide a functional entry onto the street / sidewalk network or other public space to promote activity at the street level.
- B. Pedestrian paseos are permitted between buildings, fee simple lots, parking areas, public common open spaces, and amenities. See standards on pages 124 - 125 for specific regulations.

**04. SCREENING (T3-N, T3-H, T4-N, T4-V, T5-N, T5-TC)**

- A. Garbage collection areas (dumpsters) shall be enclosed by opaque materials on all four sides with doors to remove containers. Where dumpsters are enclosed the screening shall be at least 12 inches taller than the dumpster.
- B. Ground-mounted air-conditioning or mechanical units shall be hidden from public view by screening.

**05. MECHANICAL AND LOADING (T3-H, T4-V, T5-TC)**

- A. Service elements, such as loading docks and trash collection locations, shall not be accessible from primary streets, unless a primary street is the only frontage. These site elements shall be located along an alley, along an interior property line, or internal to the site.
- B. Restaurant uses shall have outdoor trash enclosures or conditioned trash storage rooms.



# FRONTAGES OVERVIEW

## 01. INTENT

- A. Building frontage types provide a transition and appropriate interface between the public realm and the private realm. The allowed frontage type standards supplement the standards for each building type.
- B. Frontage is the specific way in which the building facade addresses the street. It is the transition and interaction between the private and public realms. Building frontages define character and form of the public spaces within the development.

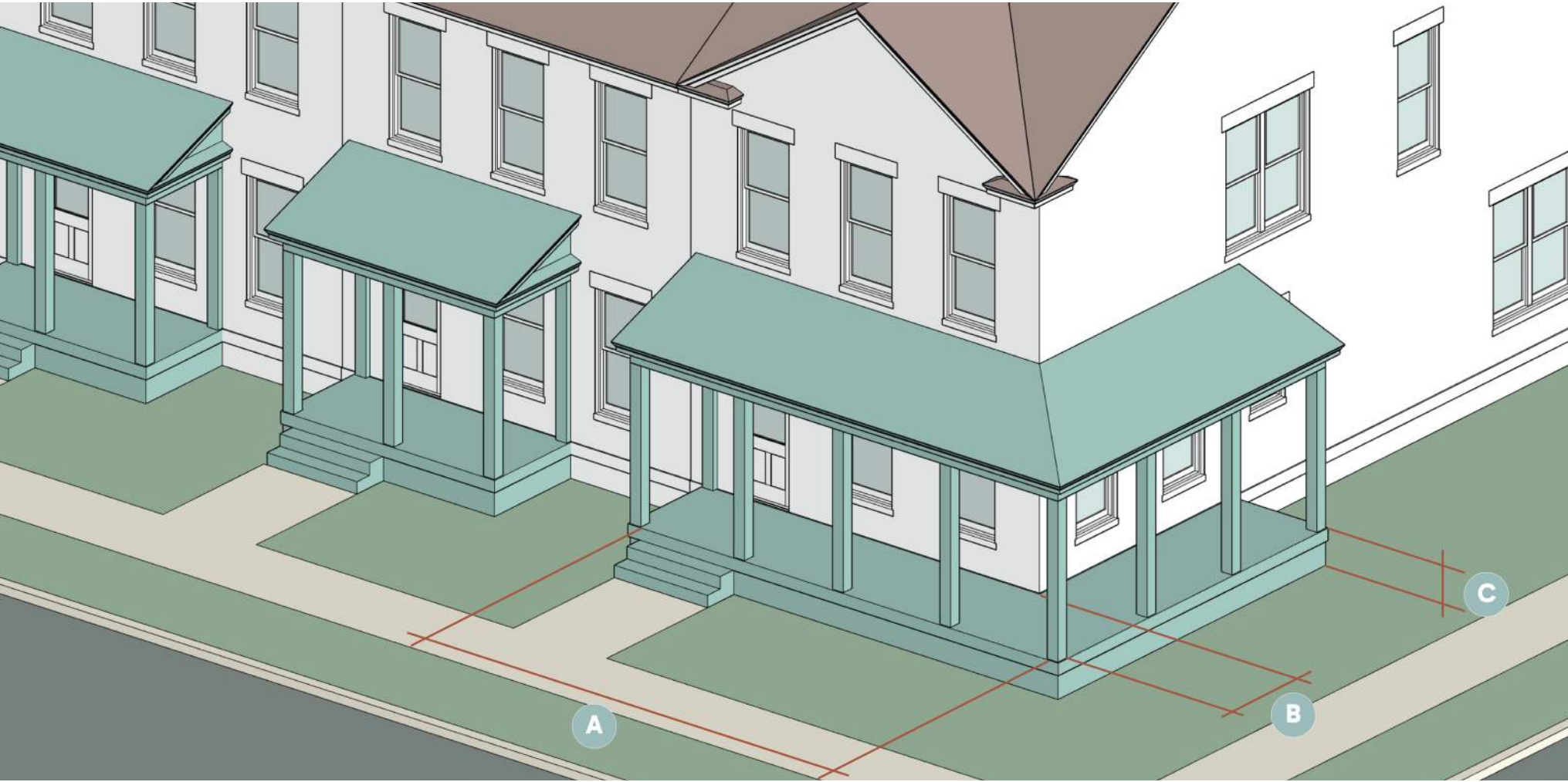
## 02. APPLICABILITY

- A. Building frontage types shall be applied to all primary buildings. If any of the building frontage types are included on a building, even if not required, they shall comply with the standards of this section.
01. Frontages may encroach into specified front or corner setback subject to any applicable regulations.
02. Civic uses are exempt from the regulatory standards of the frontage types section.

	Porch	Stoop	Balcony	Gallery	Arcade	Forecourt	Storefront	Terrace
Single Family Detached								
Cluster Housing	■	■	■					■
Single Family Rear Entry	■	■	■					■
Single Family Front Entry	■	■	■					■
Accessory Dwelling Unit	■	■	■					■
Single Family Attached								
Duplex	■	■	■					■
Townhouse	■	■	■					■
Multifamily								
Carriage House Flat	■	■	■					■
Manor House	■	■	■			■		■
Stacked Flats	■	■	■			■		■
Mixed-Use		■	■	■	■	■	■	■
Live-Work	■	■	■	■	■	■	■	■
Commercial				■	■	■	■	■
Civic	■	■	■	■	■	■	■	■
Amenities	■	■	■	■	■	■	■	■

■ Permitted

# FRONTAGE TYPE: PORCH



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	10 ft. min.
B	Depth	6 ft. min.
C	Finished Level above Sidewalk	18 in. min.

04. PERMITTED TRANSECTS

- A. T3 - N

B. T3 - H

C. T4 - N
- D. T4 - V

E. T5 - N

F. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Cluster Housing

B. Single Family Rear Loaded

C. Single Family Front Loaded

D. Accessory Dwelling Unit

E. Duplex

F. Townhouse

G. Carriage House Flats
- H. Manor House

I. Stacked Flats

J. Mixed-Use

K. Live-Work

L. Civic

M. Amenities

06. NOTES

- A. Definition: A roofed open structure connected to the front of the building that may be projecting, engaged, or integral to a building.

B. Porch shall have a roof.

C. If unenclosed and open on three sides, a porch may encroach into a required front or corner setback.

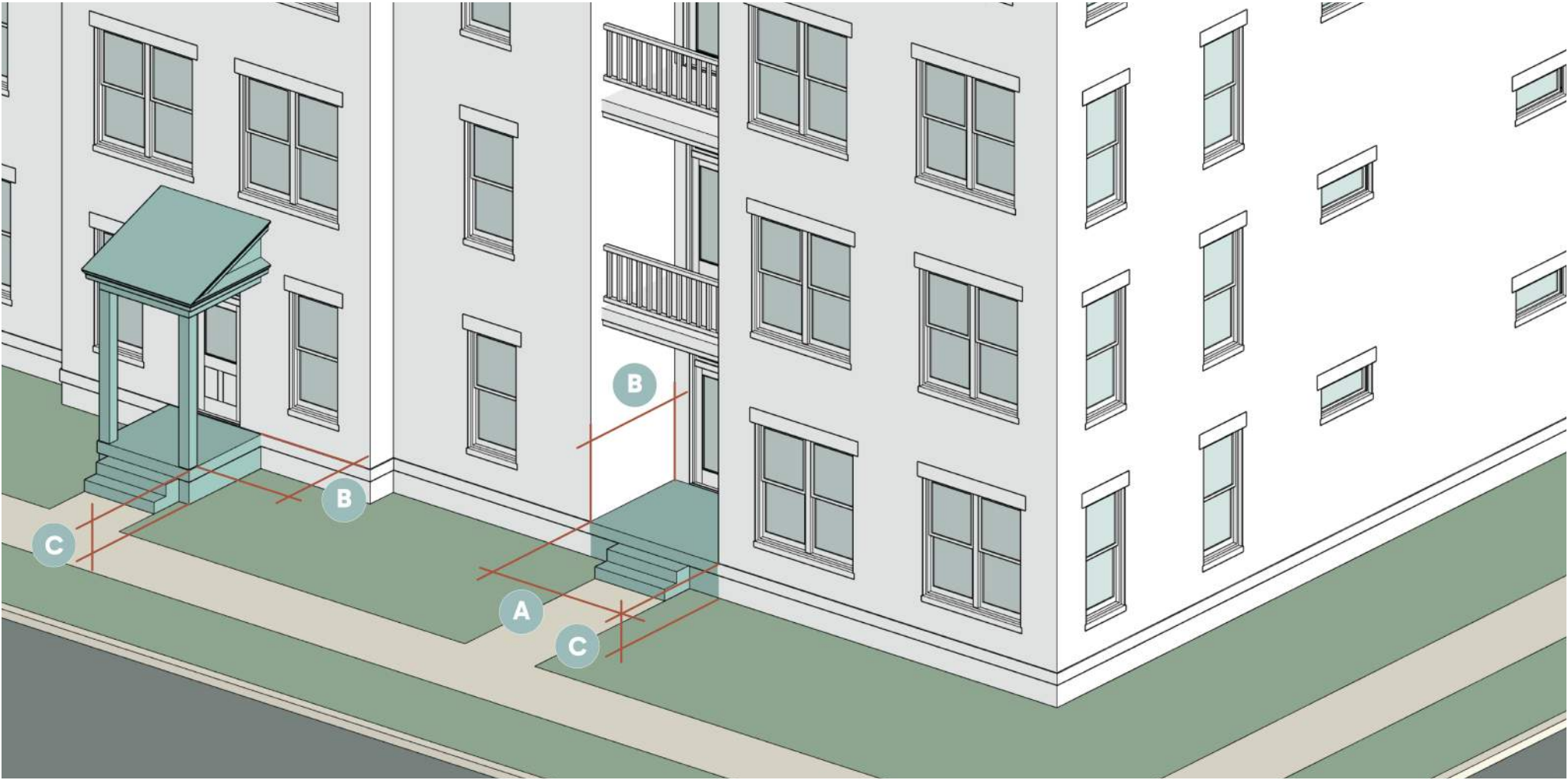
D. Open porch bases shall be enclosed with latticework or similar material, or visually concealed with landscape.

E. With the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36" along public streets, easements, and open spaces.

F. On a corner condition, wrap porches are encouraged. When present, a porch wrap shall extend a minimum of 6 ft. past the front facade.



# FRONTAGE TYPE: STOOP



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	3 ft. min.
B	Depth	3 ft. min.
C	Finished Level above Sidewalk	18 in. min.

## 04. PERMITTED TRANSECTS

- A. T3 - N

B. T3 - H

C. T4 - N
- D. T4 - V

E. T5 - N

F. T5 - TC

## 05. PERMITTED BUILDING TYPES

- A. Cluster Housing

B. Single Family Rear Loaded

C. Single Family Front Loaded

D. Accessory Dwelling Unit

E. Duplex

F. Townhouse

G. Carriage House Flats
- H. Manor House

I. Stacked Flats

J. Mixed-Use

K. Live-Work

L. Civic

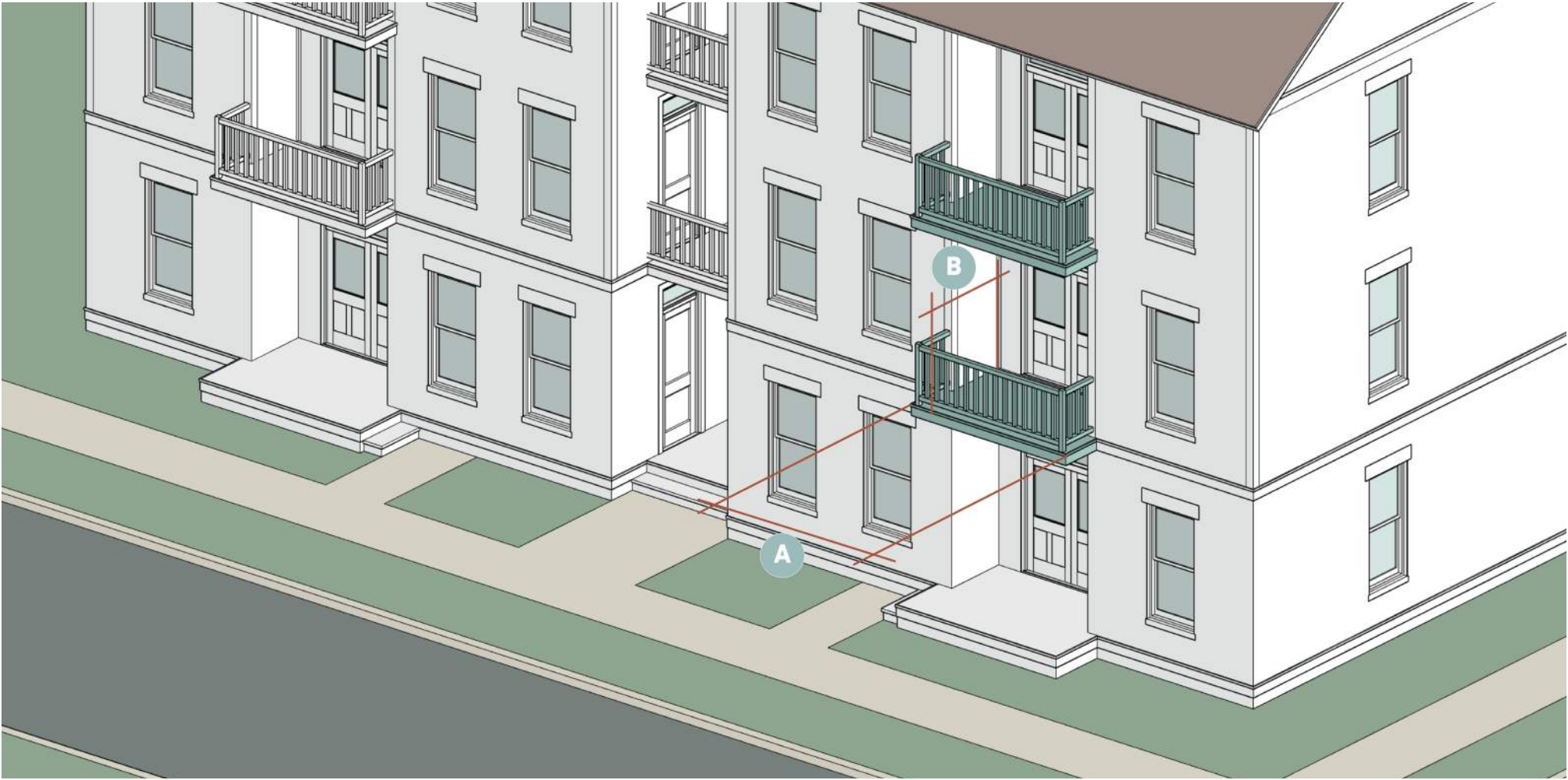
M. Amenities

## 06. NOTES

- A. Definition: A roofed one-story open structure connected to the front of the building that may be projecting, engaged, or integral to a building.
- B. A stoop shall be at least 18” above the sidewalk and shall directly connect to the sidewalk, walkway, or driveway.
- C. If unenclosed and open on three sides, a stoop may encroach into a required front or corner setback.
- D. With the exception of amenity uses, accessible units, visitable units, and topographically challenged units; challenging site topography may result in raised/lowered foundations at strategic locations. Screening is required when raised foundations exceed 36” along public streets, easements, and open spaces.



# FRONTAGE TYPE: BALCONY



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	8 ft. min.
B	Depth	6 ft. min.

04. PERMITTED TRANSECTS

- A. T3 - N

B. T3 - H

C. T4 - N
- D. T4 - V

E. T5 - N

F. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Cluster Housing

B. Single Family Rear Loaded

C. Single Family Front Loaded

D. Accessory Dwelling Unit

E. Duplex

F. Townhouse

G. Carriage House Flats
- H. Manor House

I. Stacked Flats

J. Mixed-Use

K. Live-Work

L. Civic

M. Amenities

06. NOTES

- A. Definition: A raised platform that provides space for upper floor uses, with access solely from the interior of the building. May be roofed or uncovered and may be projecting or integral to the building.

B. Balconies shall be supported by brackets, corbels, tie backs, or beams along its full depth.

C. Juliette balconies are exempt from the requirements listed in Frontage Standards.



# FRONTAGE TYPE: GALLERY



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	10 ft. min.
B	Depth	6 ft. min.
C	Overhead Clearance Height	12 ft. min.

04. PERMITTED TRANSECTS

- A. T3 - H

B. T4 - V
- C. T5 - N

D. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Mixed-Use

B. Live-Work

C. Commercial
- D. Civic

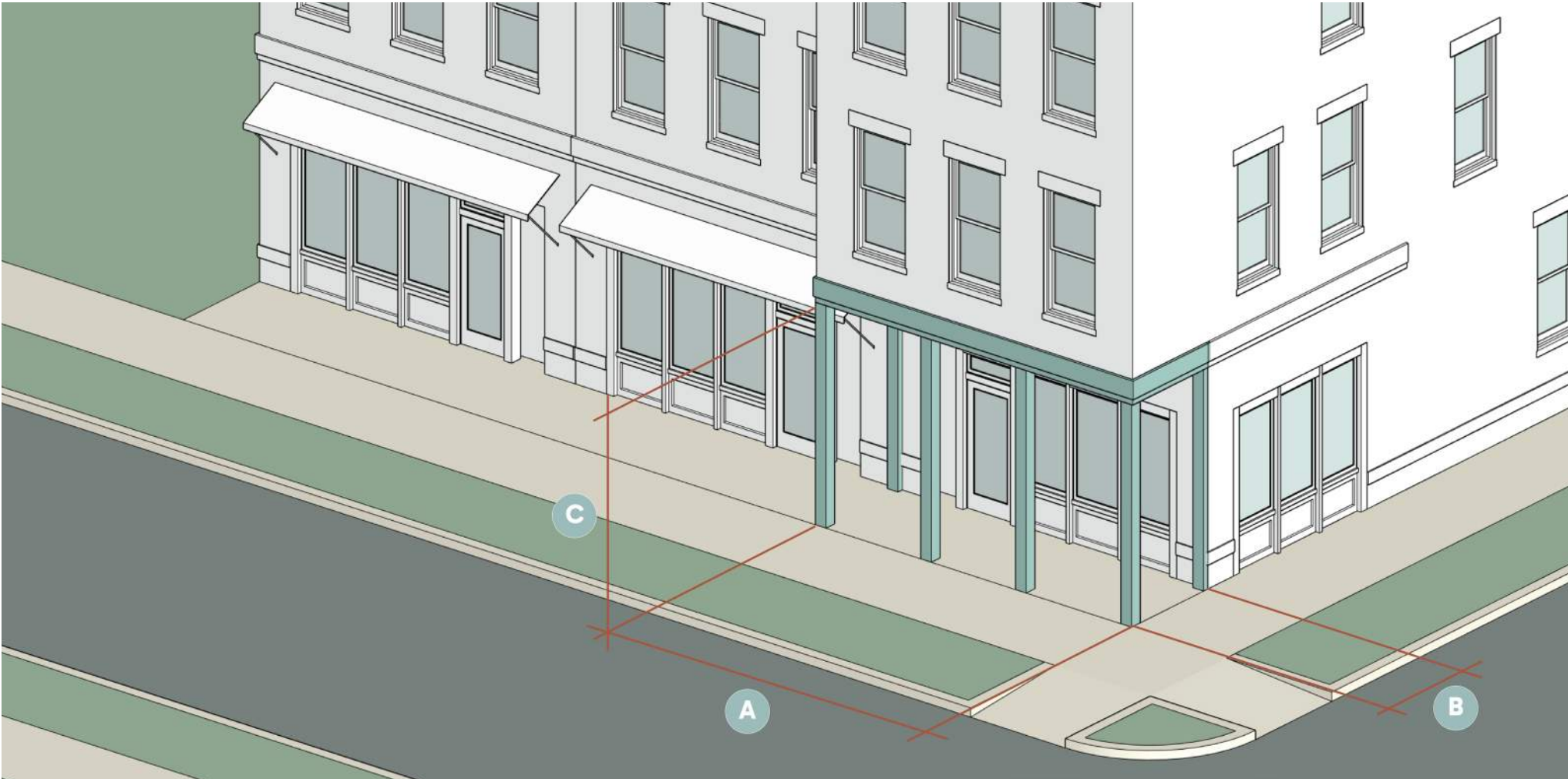
E. Amenities

06. NOTES

- A. Definition: A covered ground floor walkway attached to a building and supported on at least one side by columns or brackets with no enclosed building space above. Upper levels may be roofed or uncovered.
- B. Galleries shall be supported from below by columns; or visually supported by brackets, corbels, or beams along its full depth.
- C. 6 ft. min. clear path from furnishings and architectural supports / elements.
- D. Galleries shall not extend within the public R.O.W.



# FRONTAGE TYPE: ARCADE



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	10 ft. min.
B	Depth	6 ft. min.
C	Overhead Clearance Height	12 ft. min.

04. PERMITTED TRANSECTS

- A. T3 - H

B. T4 - V
- C. T5 - N

D. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Mixed-Use

B. Live-Work

C. Commercial
- D. Civic

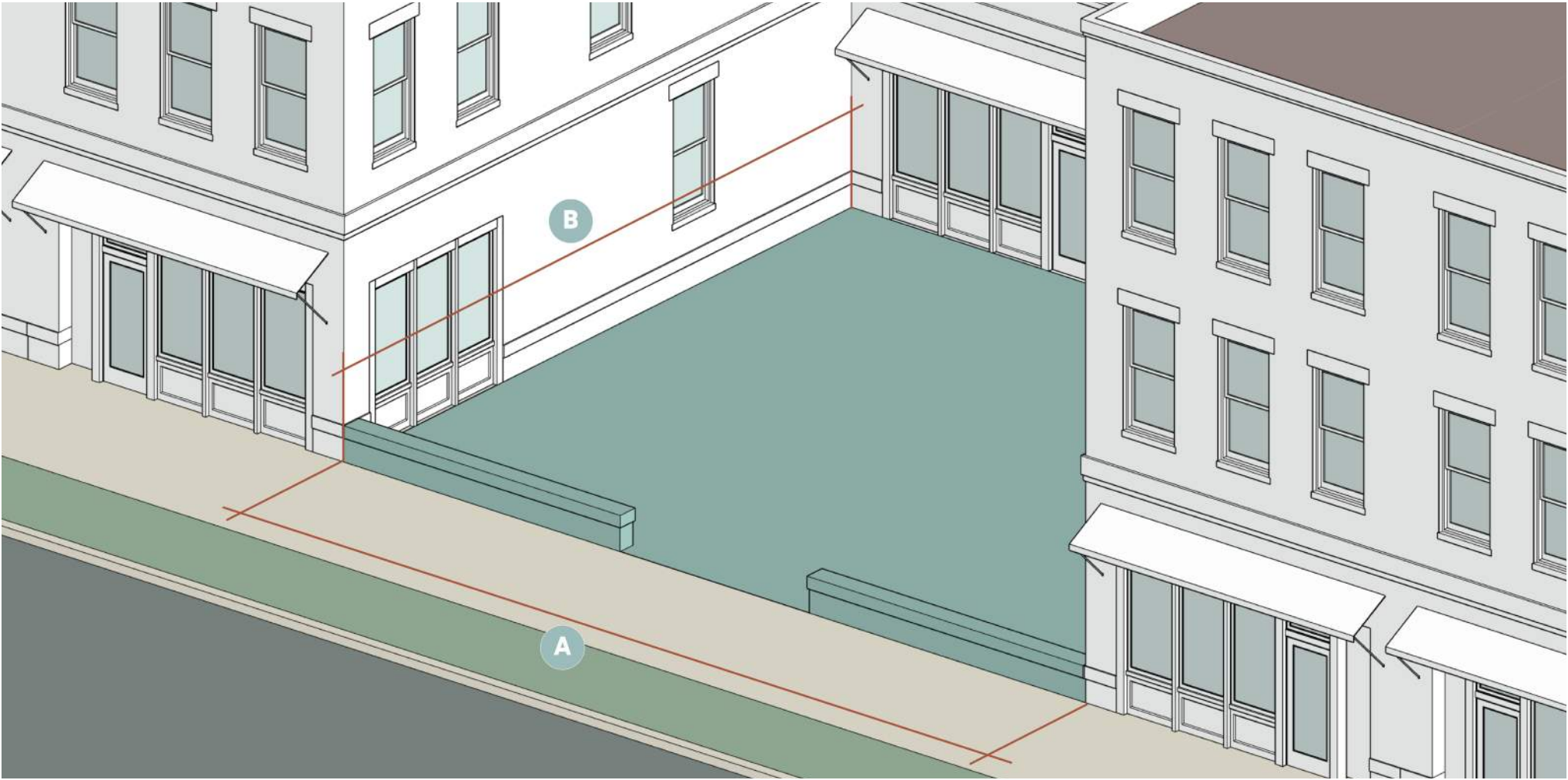
E. Amenities

06. NOTES

- A. Definition: A covered ground floor walkway attached to a building and supported on at least one side by columns with enclosed building space above.
- B. 6 ft. min. clear path from furnishings and architectural supports / elements.



# FRONTAGE TYPE: FORECOURT



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Width	15 ft. min.
B	Depth	10 ft. min.

04. PERMITTED TRANSECTS

- A. T3 - N

B. T3 - H

C. T4 - N
- D. T4 - V

E. T5 - N

F. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Manor House

B. Stacked Flats

C. Mixed-Use

D. Live-Work
- E. Commercial

F. Civic

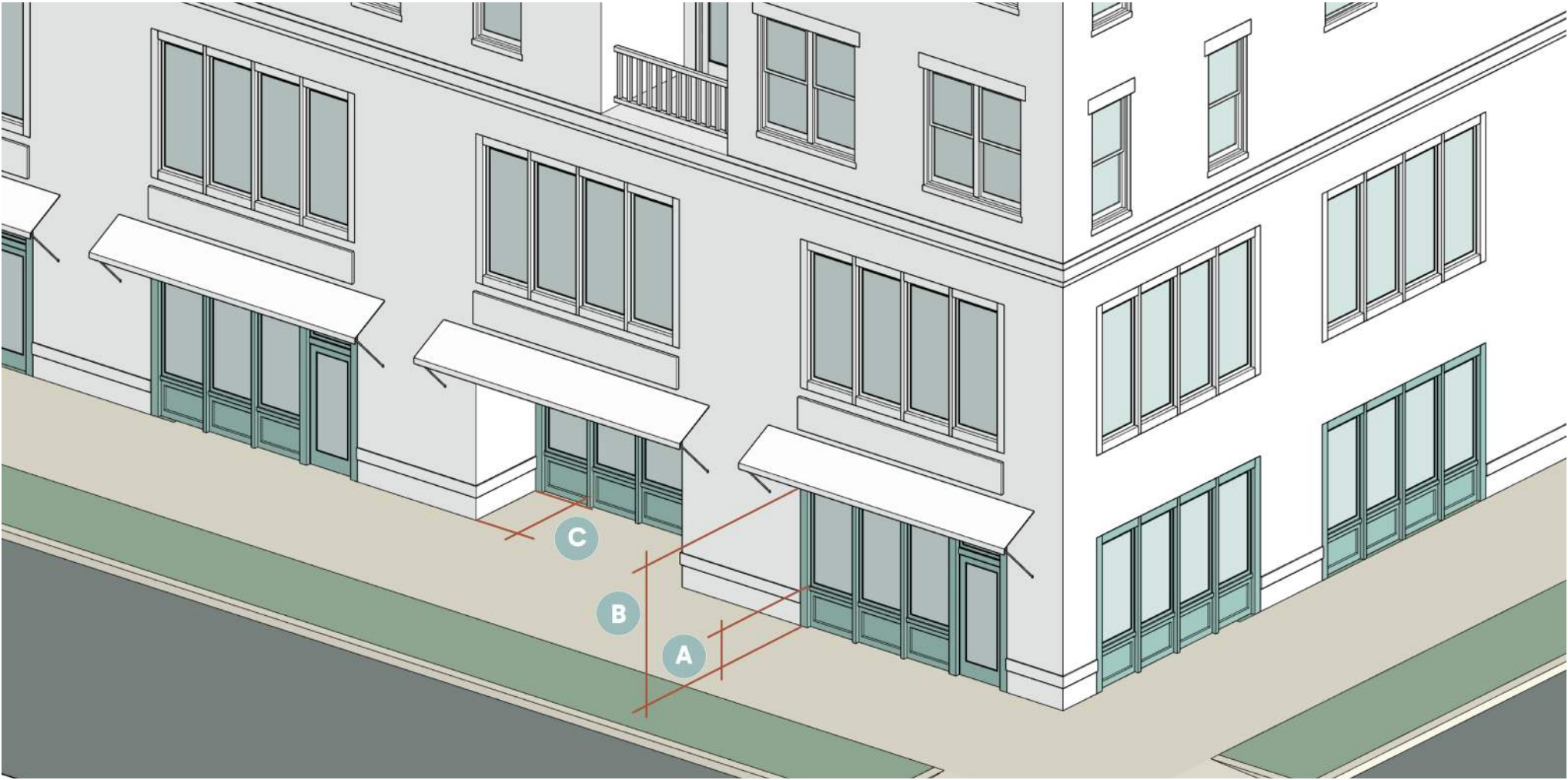
G. Amenities

06. NOTES

- A. Definition: An uncovered open space formed by a recess in the central portion of the building facade. Porches, stoops, and galleries may encroach into the forecourt.
- B. A forecourt shall be enclosed on three sides and give consideration to solar orientation.



# FRONTAGE TYPE: STOREFRONT



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Sill Height	3 ft. max.
B	Clear Height under Awning / Canopy	10 ft. min.
C	Depth of Recessed Entry	8 ft. max.

04. PERMITTED TRANSECTS

- A. T3 - H

B. T4 - V
- C. T5 - N

D. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Mixed-Use

B. Live-Work

C. Commercial
- D. Civic

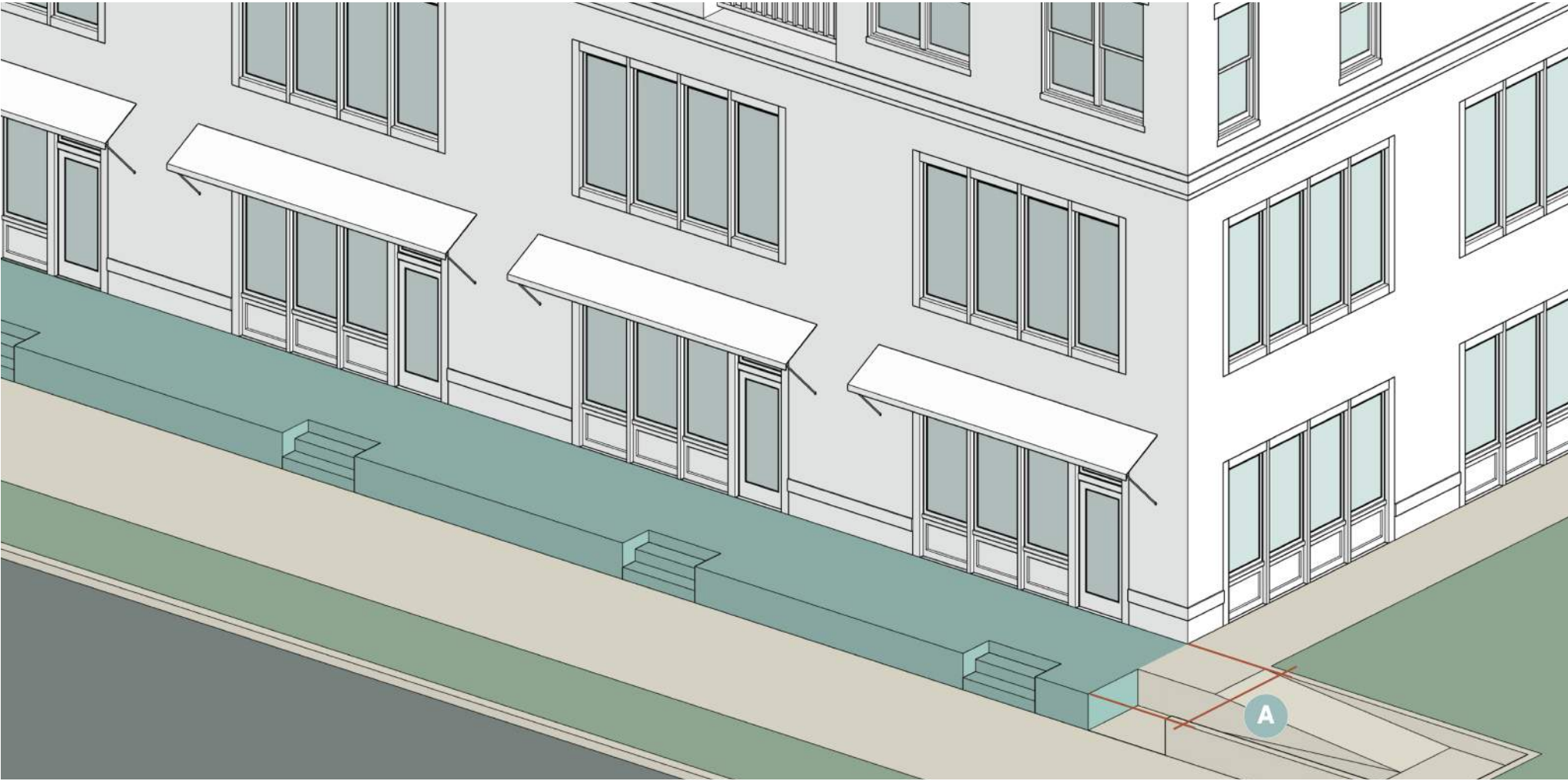
E. Amenities

06. NOTES

- A. Definition: A primary entrance or building facade located on the ground floor or street level for display of goods, services, and signs.
- B. Awnings or canopies are encouraged for shade and weather protection.
- C. Bi-fold glass windows and doors that allow the space to open to the sidewalk are encouraged.
- D. Exceptions to Depth of Recessed Entry may be made to accommodate outdoor seating or dining areas.



# FRONTAGE TYPE: TERRACE



01. FRONTAGE TYPE DIAGRAM



02. CONCEPTUAL EXAMPLES

03. FRONTAGE STANDARDS		
A	Clear Depth	6 ft. min.

04. PERMITTED TRANSECTS

- A. T3 - N

B. T3 - H

C. T4 - N
- D. T4 - V

E. T5 - N

F. T5 - TC

05. PERMITTED BUILDING TYPES

- A. Cluster Housing

B. Single Family Rear Loaded

C. Single Family Front Loaded

D. Accessory Dwelling Unit

E. Duplex

F. Townhouse

G. Carriage House Flats
- H. Manor House

I. Stacked Flats

J. Mixed-Use

K. Live-Work

L. Commercial

M. Civic

N. Amenities

06. NOTES

- A. Definition: An elevated platform, used to accommodate a change in grade, that provides circulation and access across the front of the building.

B. Frequent steps or ramps up to the terrace are encouraged to maximize access and avoid blank walls along the sidewalk.

C. Low walls used as seating are encouraged.

D. For residential uses, Finished Level above Sidewalk shall be 18” min.

# PARKING REQUIREMENTS

## 01. GENERAL STANDARDS

- A. When alleys are present, vehicular access from alleys is preferred.
- B. Site access and internal circulation in developments shall promote safety, efficiency, and convenience. Vehicular entrances shall be clearly identified and be easily accessible to minimize pedestrian / vehicle conflict.
- C. Shared parking areas and cross-access easements are encouraged to improve access and reduce curb cuts along streets.
- D. Driveways shall have appropriate access aprons that transition street grade to sidewalk grade.

## 02. PRIVATE RESIDENTIAL GARAGES

- A. Detached garages and parking shelters are permitted to the rear of buildings.
- B. Lots served by alleys shall only access garages from the alley.

## 03. SURFACE PARKING

- A. Screening and landscaping standards shall apply to all surface parking lots including, but not limited to, public and private parking facilities, driveways and access aisles.
- B. Generally, off-street parking shall be located internal to the block and not located along the street frontage.
- C. Parking areas adjacent to a public street or public common open space shall be separated from the edge of the property line or R.O.W. with a perimeter landscape strip 5 ft. minimum in width and / or screening wall. Perimeter landscape strips adjacent to public streets and public common open spaces are encouraged to include a fence or wall in accordance with this document.

## 04. PARKING GARAGE STRUCTURES

- A. Vehicular openings to parking structures shall not exceed 35 ft. in width.
- B. Parking garage structures shall be located to the rear of buildings when feasible.
- C. Upper level facade treatments / cladding is required on all public street frontages.
- D. Facade treatments shall integrate or complement the architectural characteristics of the habitable portion of the building and the surrounding built context.
- E. All parking structures with parking available to the public shall have a clearly marked pedestrian entrance, separate from vehicular access, on street frontages. A publicly accessible building lobby may meet this requirement.

- F. Underground parking that is completely below grade may extend beyond the facade of the building, but it may not encroach into the R.O.W.

## 05. SHARING FACTOR

- A. For all single tenant, individual retail trade and multi-tenant retail trades, mixed use, and multi-family developments, shared parking is strongly encouraged. Off-street parking and loading requirement shall be provided as set forth below:

Required Parking	
T3 Hamlet & Neighborhood	
Residential	2.0 / dwelling
Lodging	0.33/ room
Office	2.5 / 1,000 sq ft.
Retail	4.0 / 1,000 sq ft.
Civic	See City of Huntsville Zoning
Other	See City of Huntsville Zoning
T4 Village & Neighborhood	
Residential	1.5 / dwelling
Lodging	0.33/ room
Office	2.5 / 1,000 sq ft.
Retail	4.0 / 1,000 sq ft.
Civic	See City of Huntsville Zoning
Other	See City of Huntsville Zoning
T5 Town Center & Neighborhood	
Residential	1.0 / dwelling
Lodging	0.33/ room
Office	2.5 / 1,000 sq ft.
Retail	3.0 / 1,000 sq ft.
Civic	See City of Huntsville Zoning
Other	See City of Huntsville Zoning

Shared Parking Factor

Function	with		Function
Residential			Residential
Lodging			Lodging
Office			Office
Retail			Retail
		1	
	1.1	1.1	
	1.4	1	1.4
	1.2	1.7	1.2
	1.3	1	1.3
	1.2	1.2	
	1		

- B. A professional parking study that indicates that the parking requirements can be reduced will be accepted by the City of Huntsville with the review and approval of the report by the appropriate City departments including, but not limited to, Traffic Engineering.
- C. Any available on-street and public parking, which correlates with the applicable site or development, shall be counted towards the overall parking and loading requirements. These parking spaces must be provided within 1,200 ft. of the intended site or development.
- D. The Sharing Factor may be used to calculate the required parking for any shared parking agreement or mixed use development (defined as two dissimilar functions occurring within the same development or property, as determined by the Planning department). The actual parking required is calculated by adding the total number of spaces required by each separate function and dividing the total by the appropriate factor from the Sharing Factor Table. When 3 or more uses share parking, the highest factor shall be used to calculate the required number of spaces.



# SIGNAGE

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**01. SIGNAGE STANDARDS WITHIN WESTMOORE LANDING WILL FOLLOW THE FOLLOWING REGULATIONS WITHIN THE CITY OF HUNTSVILLE ZONING ORDINANCE:**

- A. 72.2 Definitions (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)
- B. 72.5.2. Residential Subdivisions (T3-N, T4-N, T5-N)
- C. 72.5.2. Apartment Complexes (T3-N, T4-N, T5-N)
- D. 75.5.4 Apartment Buildings (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)
- E. 72.6 Temporary Signs (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)
- F. 72.6.2 Temporary Sign Removal (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)
- G. 72.6.3 Temporary Signs on Residential Premises (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)
- H. 72.6.4 Temporary Signs on Nonresidential Premises (T3-V, T4-V, T5-TC)
- I. 72.7.5 Permanent Signs in General Business C-3, Central Business C-B, Research Park 2, and Village Business C-6 Districts (T3-V, T4-V, T5-TC)
- J. 72.8 General Sign Standards (T3-N, T3-V, T4-N, T4-V, T5-N, T5-TC)



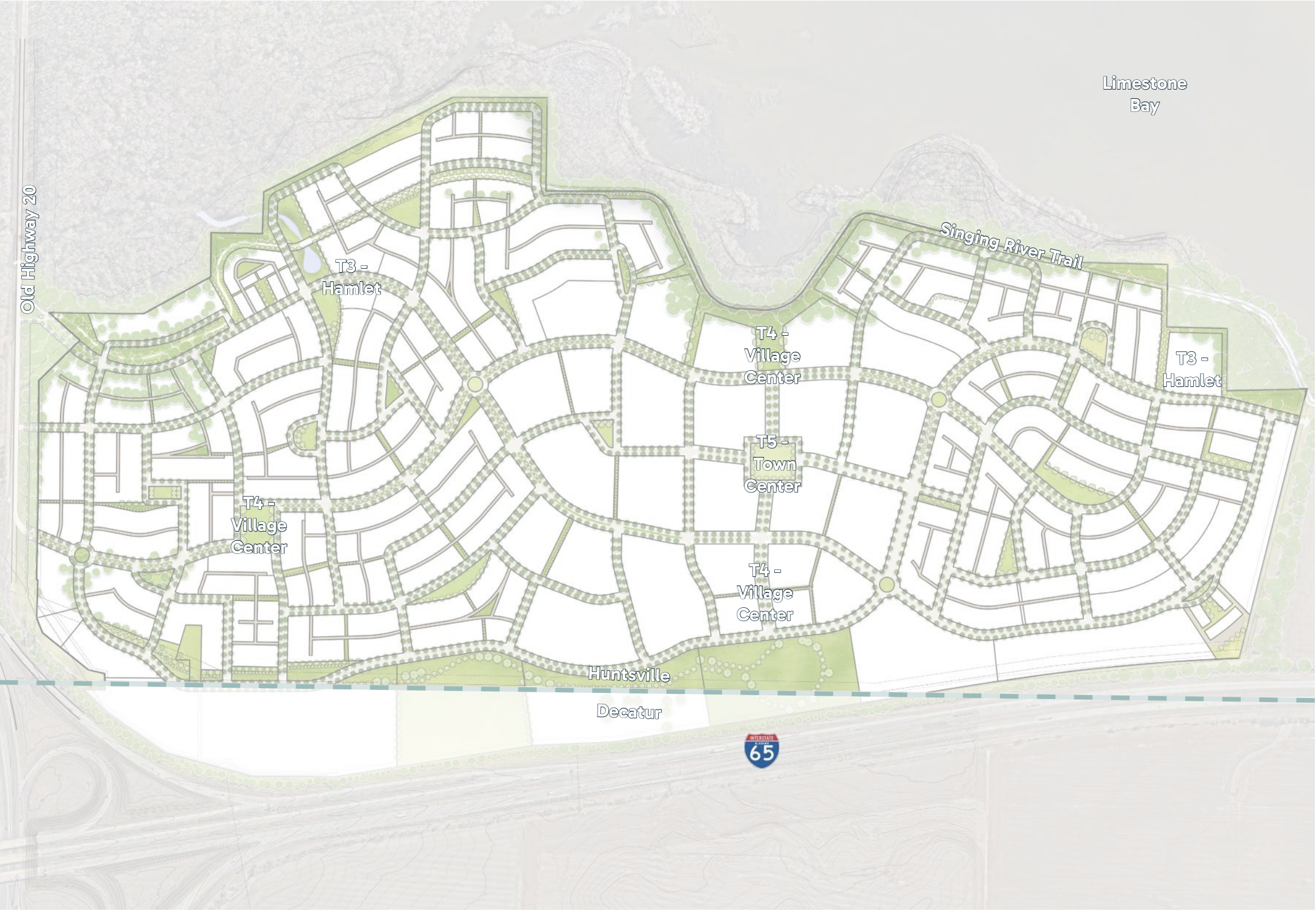
# OPEN SPACE STANDARDS

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# OPEN SPACE OVERVIEW



The Open Space Overview outlines the distribution of defined public open spaces throughout the community, ensuring that residents and visitors alike have access to a variety of both programmed and unprogrammed open spaces of different scales. These open spaces are essential to enhancing the community’s quality of life by offering numerous benefits.

Open spaces provide a natural escape from the built environment and act as community hubs that encourage social interaction and foster a sense of belonging. These spaces will offer opportunities for recreation, exercise, and relaxation, supporting both physical and mental well-being. The open spaces are vital to promoting health, happiness, and community.

The pedestrian shed overview provides information on the various open spaces and access for residents within their designated pedestrian shed, which is defined as a 1/4 mile radius. Within each pedestrian shed, open space amenities include a diverse range of gathering spaces to accommodate community events, leisurely evening walks, and a variety of outdoor activities.

**NOTE:**  
Green space is an approximate location, not prescriptive.





# PEDESTRIAN SHED OVERVIEW



The Pedestrian Shed Overview ensures that both residents and visitors have access to a diverse range of programmed and unprogrammed open spaces of varying sizes throughout the development. These open spaces play a vital role in improving the community's quality of life by providing numerous benefits.

Pedestrian sheds are integral to creating vibrant, walkable communities where residents and visitors can easily access essential services and public spaces. By focusing on a compact, accessible area, these spaces promote walking as a primary mode of transportation, encouraging a healthier and more sustainable lifestyle. They foster an environment where daily needs, like shops, schools, and local services, as well as squares, greens, parks, and paseos, are all within a short, 5-minute walk (1/4 mile), reducing the dependence on cars. This design approach enhances street life, promotes local businesses, and contributes to a safer, more connected community. Prioritizing pedestrian-friendly infrastructure not only improves mobility but also creates a more enjoyable and inclusive urban experience.

Within each pedestrian shed, planting and infrastructure enhances the walking experience by providing shade, improving air quality, and adding aesthetic value. Well-designed sidewalks, seating areas, and lighting ensure comfort and safety, while green spaces and permeable surfaces support sustainability and enhance the overall quality of life. These elements together foster a more inviting and livable environment for pedestrians.





# OPEN SPACE TYPES OVERVIEW



### TOWN SQUARE

Central public spaces surrounded by buildings, typically incorporating a mix of hardscape and landscape elements. Squares serve as focal points for civic activities and social interaction.



### LINEAR GREEN

Public green spaces that are designed for recreational, social, and environmental purposes. These parks typically feature open lawns, seating, public art, and fountains.



### LARGE PASEO

A large paseo is a broad pathway or walkway designed to accommodate more foot traffic and provide a more prominent route for pedestrians and cyclists. They often lead to or connect open spaces to key destinations, offering a safe alternative to streets.



### VILLAGE GREEN

Large, open, greens located in the heart of urban centers, designed to accommodate public gatherings, events, and daily pedestrian traffic. Village greens often feature seating, public art, and fountains.



### CLUSTER HOUSING OPEN SPACE

Cluster housing open spaces provide a range of activities from relaxing and socializing to gardening and recreation. These spaces foster a sense of community, connection to nature, and allow residents to engage in outdoor activities within their neighborhood.



### SMALL PASEO

A small paseo is a narrow pathway or walkway designed to provide a convenient and pleasant route for pedestrians. They often lead to or connect open spaces, offering a quiet and safe alternative to streets for walking and social interaction.



### COMMUNITY PARK

Public green spaces that are designed for recreational, social, and environmental purposes. These parks typically include playgrounds, walking trails, sports fields, picnic areas, benches, and outdoor event spaces.



### SMALL COMMONS

Shared open spaces within hamlets that serve as communal areas for events, socializing, and local activities. These commons are typically minimally developed and maintain a natural feel.



### NEIGHBORHOOD PARK

Medium-sized parks designed to serve the recreational needs of nearby residents. These parks typically include playgrounds, dog parks, picnic areas, and walking paths.



### POCKET PARK

Small, urban green spaces, usually less than an acre, designed to provide a respite from the city environment. Pocket parks often include seating, greenery, and sometimes play areas.



# T3 OPEN SPACE INTENT

## T3 NEIGHBORHOOD (T3 - N)



### 01. INTENT

The intent for open space within a T3 Neighborhood is to create well-designed, accessible areas that enhance the overall quality of life for residents by creating environments that support physical and mental well-being. They are intended to promote social interaction among residents, encourage a range of outdoor activities, and foster a connection with nature. Whether it's children playing in a park, neighbors enjoying a peaceful walk, or residents participating in community affairs, these spaces contribute to a vibrant, active, and engaged community as a whole.

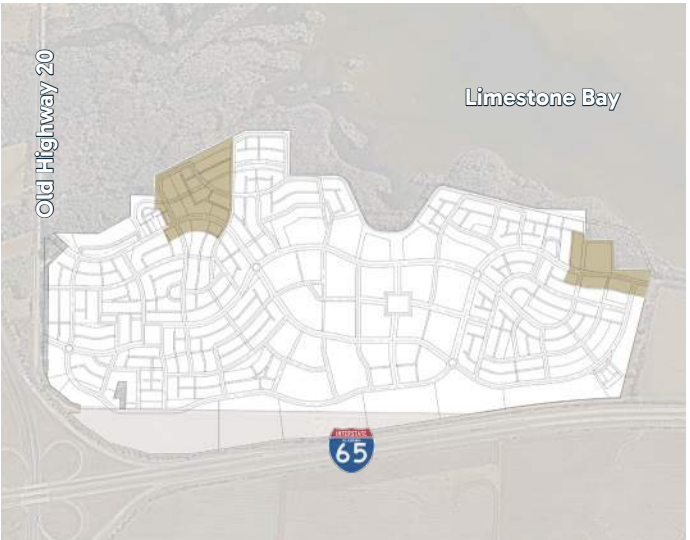


## T3 HAMLET (T3 - H)



### 02. INTENT

The intent of open space within a T3 Hamlet is to create small, community-focused areas within a hamlet that promote gathering, socializing, relaxation, recreation, and a strong connection to nature. These space aim to enhance quality of life by fostering a sense of belonging and community. Additionally, T3 Hamlet open spaces are often designed to integrate natural elements, such as ponds, trails, and large open green areas, that enhance environmental sustainability and promote a balanced lifestyle. They are intended to be adaptable to the needs of the community to create a cohesive and thriving hamlet.





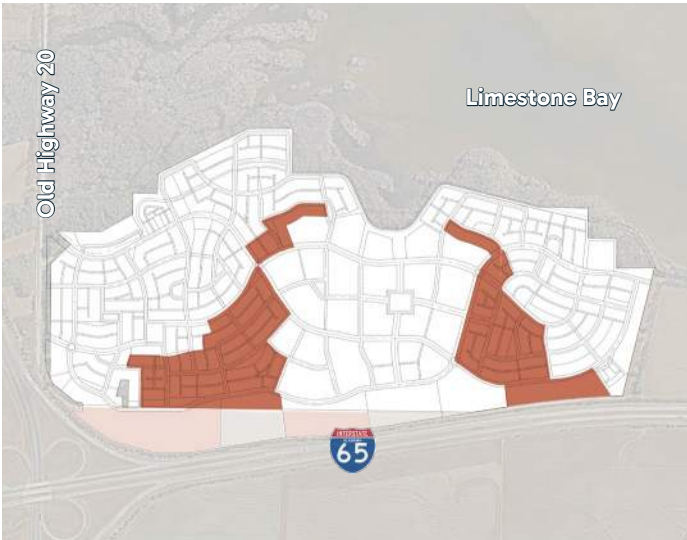
# T4 OPEN SPACE INTENT

## T4 NEIGHBORHOOD (T4 - N)



### 01. INTENT

The intent of open space in a T4 Neighborhood is to create accessible, functional, and enjoyable outdoor areas that enhance the quality of life for residents. Open spaces in T4 Neighborhoods are intended to provide places for recreation, relaxation, and social interaction, such as neighborhood parks, community parks, linear greens, cottage courts, pocket parks, and paseos. Ultimately, open spaces in T4 Neighborhoods should contribute to a healthy, sustainable, and vibrant urban environment, where people can engage with nature while supporting social and recreational needs.

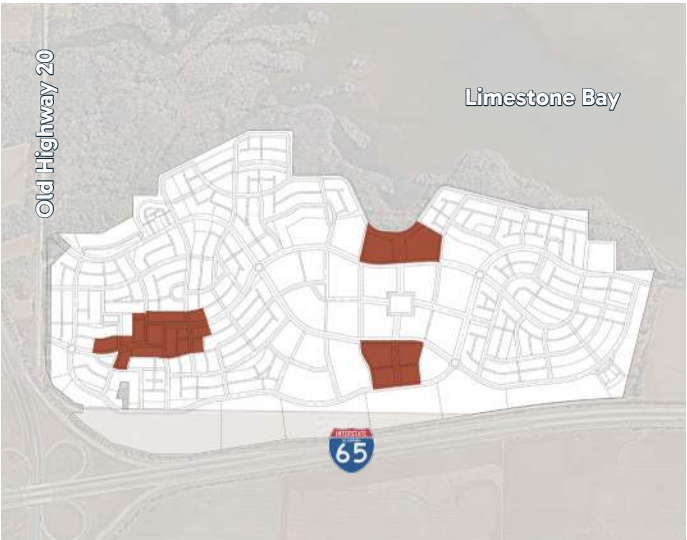


## T4 VILLAGE (T4 - V)



### 02. INTENT

The intent of open space in a T4 Village is to create high-quality, accessible, and multi-functional outdoor areas that foster community interaction and enhance the overall quality of life. Open spaces in T4 Villages are intended to serve as communal gathering spaces and provide places for recreation, relaxation, and cultural events, such as village and linear greens, cottage courts, pocket parks, and paseos. Ultimately, the emphasis is on providing accessible, walkable, and safe open spaces that are well-integrated into the village's urban fabric while enhancing the surrounding natural environment.





# T5 OPEN SPACE INTENT

## T5 NEIGHBORHOOD (T5 - N)



### 01. INTENT

The intent of open space in a T5 Neighborhood is to provide accessible, well-maintained areas that promote public health, enhance the quality of life, and encourage social interaction within a densely developed urban environment. These spaces are designed bring balance to the built environment by offering communal, recreational, aesthetic, and environmental benefits, while creating a sense of place that fosters a vibrant, inclusive community. Additionally, well-designed open spaces can improve the neighborhood’s overall livability by offering residents a respite from the urban hustle.

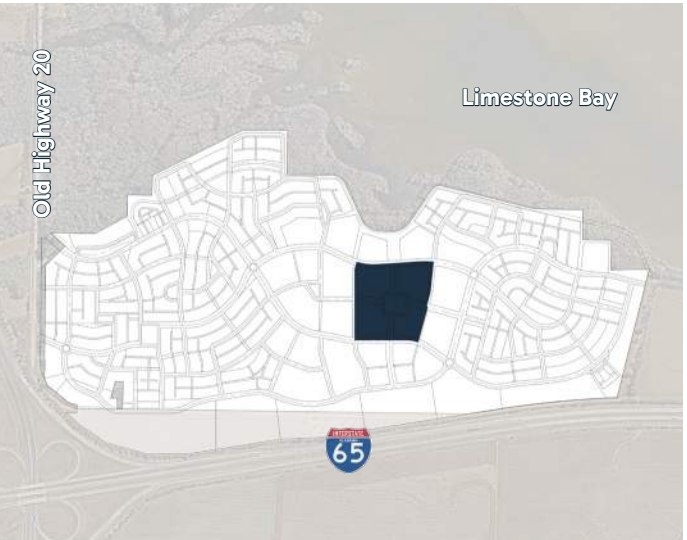


## T5 TOWN CENTER (T5 - TC)



### 02. INTENT

The intent of open space in a T5 Town Center is to create accessible, vibrant areas that foster community engagement, enhance public health, and provide a balance between the built and natural environment. These spaces are designed to serve as focal points for social interaction, relaxation, and cultural activities, while also contributing to a pedestrian-friendly atmosphere. They help create a sense of place that strengthens the neighborhood’s identity and promotes a high quality of life for all. Additionally, these spaces can serve as venues to enrich the cultural fabric of the community.





# PERMITTED OPEN SPACE BY TRANSECT

	T3 - N	T3 - H	T4 - N	T4 - V	T5 - N	T5 - TC
TOWN SQUARE						●
VILLAGE GREEN				●		
COMMUNITY PARK	●		●			
NEIGHBORHOOD PARK	●	●	●		●	
LINEAR GREEN	●		●	●	●	
CLUSTER HOUSING OPEN SPACE	●	●	●	●	●	●
SMALL COMMONS		●				
POCKET PARK	●	●	●	●	●	●
LARGE PASEO	●	●	●	●	●	●
SMALL PASEO	●	●	●	●	●	●
UNPROGRAMMED OPEN SPACE	●	●	●	●	●	●

01. **NOTES:**

A. Unprogrammed open space shall not have any size requirements.

● Permitted

# TOWN SQUARE



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T5 - Neighborhood	T5 - Town Center
Pervious	N/A	Min. 50%
A Minimum Size	N/A	9,000 sf
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# VILLAGE GREEN



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T4 - Neighborhood	T4 - Village
Pervious	N/A	Min. 40%
A Minimum Size	N/A	15,000 sf
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# COMMUNITY PARK



## 01. OPEN SPACE DIAGRAM



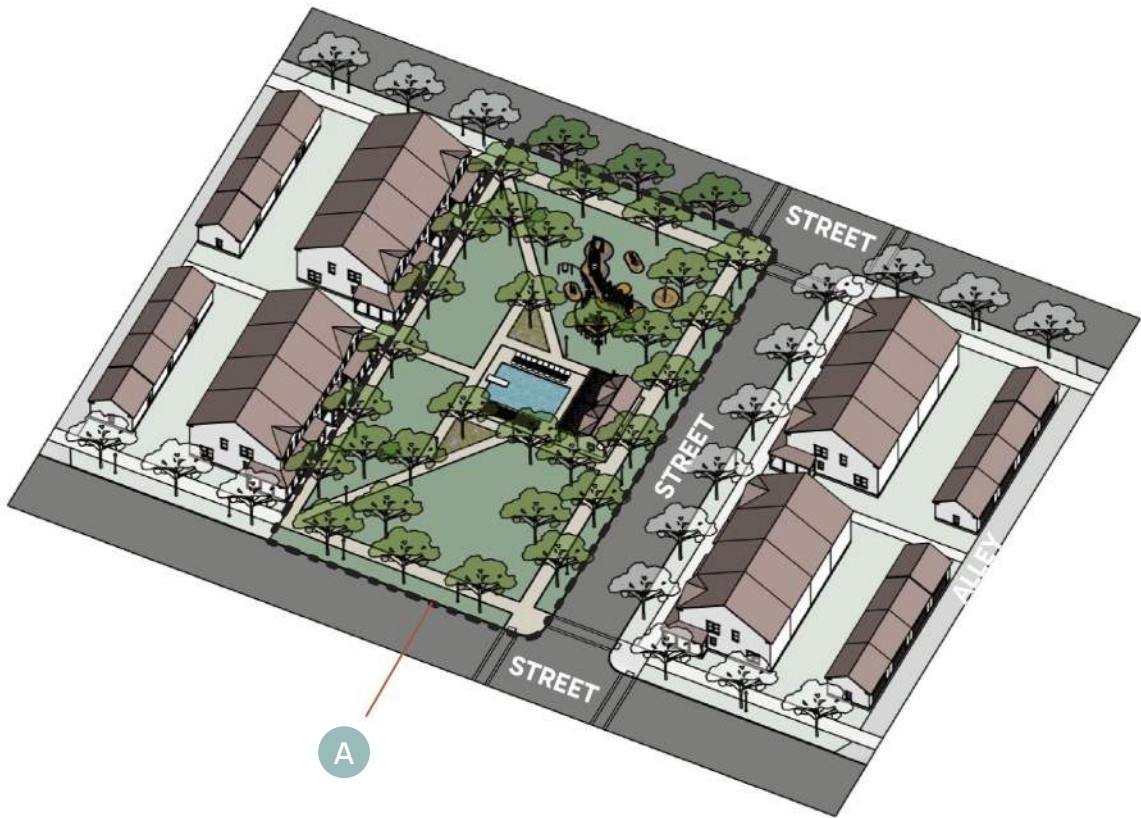
## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 80%	N/A
A Minimum Size	65,340 sf (1.5 acres)	N/A
	T4 - Neighborhood	T4 - Village
Pervious	Min. 80%	N/A
A Minimum Size	43,560 sf (1 acre)	N/A
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# NEIGHBORHOOD PARK



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 50%	Min. 50%
A Minimum Size	14,000 sf	12,500 sf
	T4 - Neighborhood	T4 - Village
Pervious	Min. 50%	N/A
A Minimum Size	11,000 sf	N/A
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 50%	N/A
A Minimum Size	9,500 sf	N/A
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 5' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# LINEAR GREEN



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 80%	N/A
A Minimum Size	9,000 sf	N/A
	T4 - Neighborhood	T4 - Village
Pervious	Min. 80%	Min. 70%
A Minimum Size	7,000 sf	5,000 sf
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 70%	N/A
A Minimum Size	3,000 sf	N/A
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# CLUSTER HOUSING OPEN SPACE



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 80%	Min. 80%
A Minimum Size	150 sf per lot in cluster	150 sf per lot in cluster
	T4 - Neighborhood	T4 - Village
Pervious	Min. 80%	Min. 80%
A Minimum Size	150 sf per lot in cluster	150 sf per lot in cluster
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 80%	Min. 80%
A Minimum Size	150 sf per lot in cluster	150 sf per lot in cluster
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 5' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# SMALL COMMONS



03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	N/A	N/A
A Minimum Size	N/A	5,000 sf
	All Transects	
Access	Shall have at least one direct access to a public right-of way	
Walkway Standards	Min. 6' wide	

04. **NOTES:**
- A. Walkway standards to apply to perimeter and primary designated paths.

## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY



# POCKET PARK



## 01. OPEN SPACE DIAGRAM



## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 40%	Min. 30%
A Minimum Size	5,000 sf	3,000 sf
	T4 - Neighborhood	T4 - Village
Pervious	Min. 40%	Min. 30%
A Minimum Size	4,500 sf	2,500 sf
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 40%	Min. 30%
A Minimum Size	4,000 sf	2,000 sf
	All Transects	
Access	Shall provide a sidewalk connection to a sidewalk along any adjacent street	
Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.



# LARGE PASEO



## 01. OPEN SPACE DIAGRAM



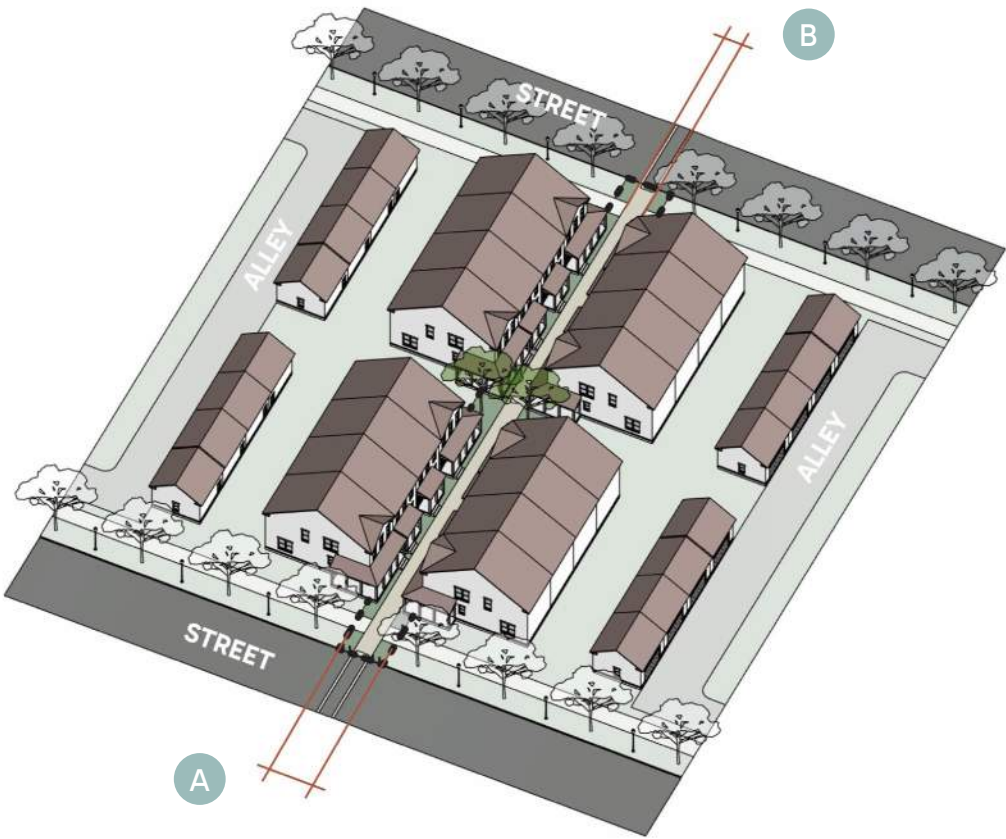
## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 20%	Min. 20%
A Minimum Size	15' Min. Wide	15' Min. Wide
	T4 - Neighborhood	T4 - Village
Pervious	Min. 20%	Min. 20%
A Minimum Size	15' Min. Wide	15' Min. Wide
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 20%	Min. 20%
A Minimum Size	15' Min. Wide	15' Min. Wide
	All Transects	
Access	Shall provide a sidewalk connection to a sidewalk along an adjacent street	
B Walkway Standards	Min. 6' wide	

04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.
  - B. Paseo paths can bisect a block as shown, but can also vary to ensure connectivity.



# SMALL PASEO



## 01. OPEN SPACE DIAGRAM



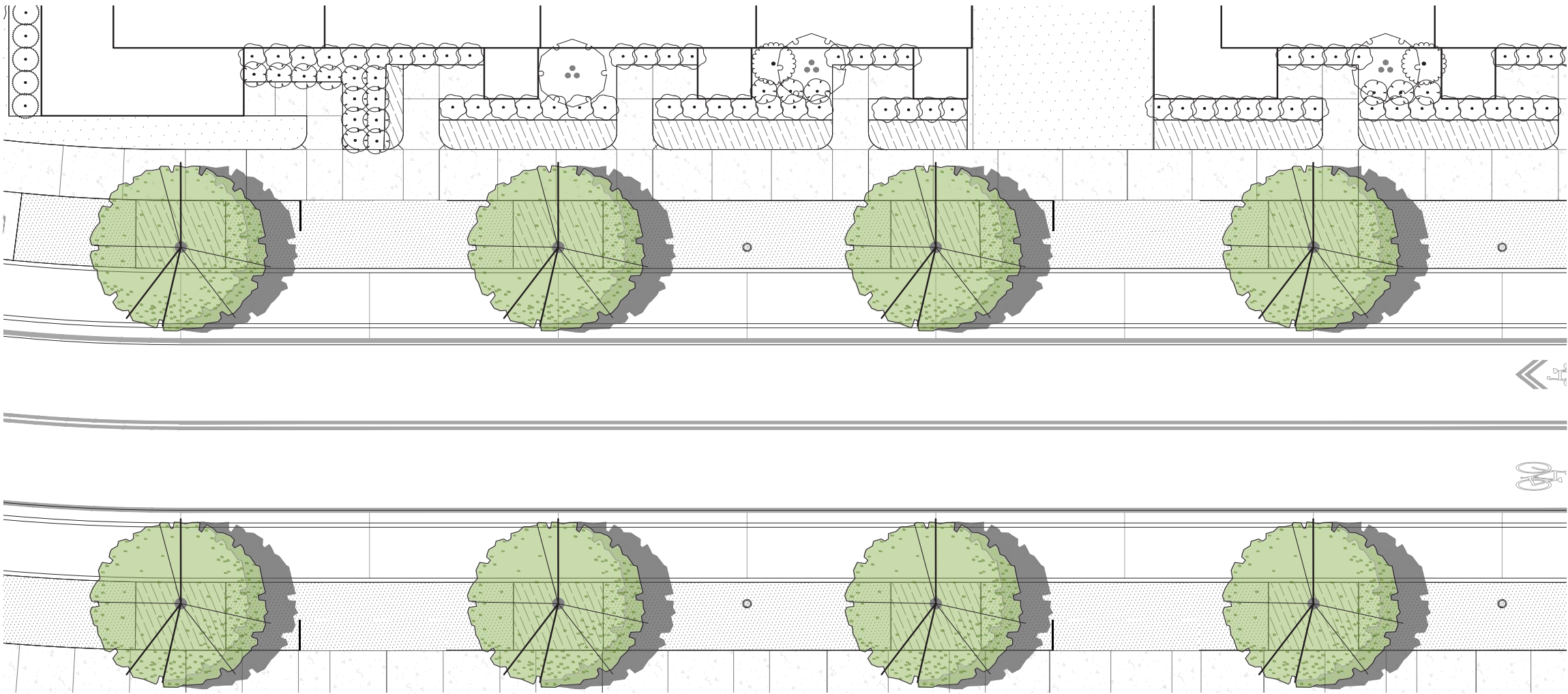
## 02. CONCEPTUAL IMAGERY

03. BULK STANDARDS		
	T3 - Neighborhood	T3 - Hamlet
Pervious	Min. 20%	Min. 20%
A Minimum Size	10' Min. Wide	10' Min. Wide
	T4 - Neighborhood	T4 - Village
Pervious	Min. 20%	Min. 20%
A Minimum Size	10' Min. Wide	10' Min. Wide
	T5 - Neighborhood	T5 - Town Center
Pervious	Min. 20%	Min. 20%
A Minimum Size	10' Min. Wide	10' Min. Wide
	All Transects	
Access	Shall provide a sidewalk connection to a sidewalk along an adjacent street	
B Walkway Standards	Min. 6' wide	

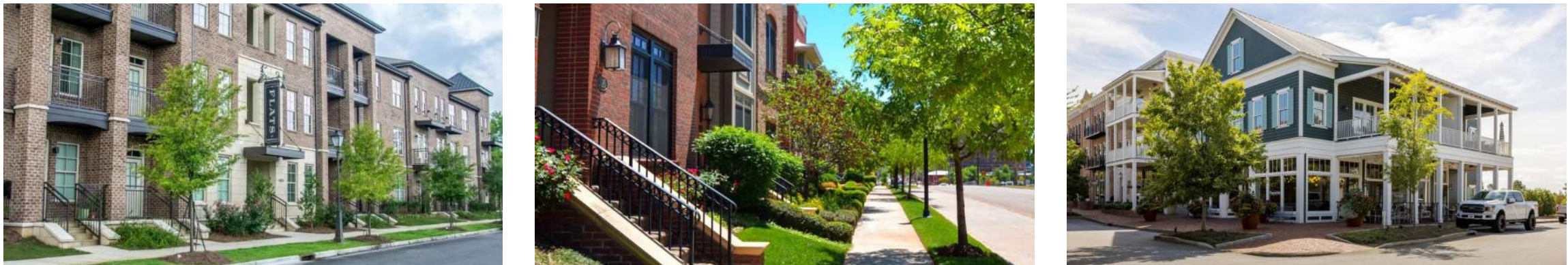
04. NOTES:
- A. Walkway standards to apply to perimeter and primary designated paths.
  - B. Paseo paths can bisect a block as shown, but can also vary to ensure connectivity.



# STREET TREE STANDARDS



01. DIAGRAM



02. CONCEPTUAL IMAGERY

These guidelines help ensure that street trees are selected, planted, and maintained in a way that enhances the public realm, supports sustainability, and minimizes conflicts with infrastructure.

**03. TREE SELECTION**

- A. Species: Tree species to be non-invasive, suited to the climate, and pest/disease resistance. Ensure diversity to prevent species loss.

**04. PLANTING REQUIREMENTS**

- A. Spacing: Trees should be spaced to allow room for canopy growth. Trees shall not block architectural elements, such as front doors and stoops.
- B. Tree Wells vs. Tree Grates: Wells for larger trees, with grates providing openings for air/ water to roots.
- C. Clearances: Maintain 8-foot streetlight and regulatory sign clearance.

**05. NOTES**

- A. Street tree planting to adhere to Huntsville Municipal Code Chapter 27 - Vegetation section 27-53.

**Legend**

Canopy Tree



# PLANTING STANDARDS



Planting guidelines are essential for ensuring that plants are integrated into the landscape in a way that promotes health, longevity, aesthetic value, and environmental benefits. These standards guide the selection, installation, and maintenance of plants in urban, suburban, or rural settings.

## 01. PLANT SELECTION

- A. Species: Plant species to be non-invasive, suited to the climate, and pest/disease resistance. Ensure diversity to prevent species loss.

## 02. PLANTING LAYOUT

- A. Recommended Arrangement: Building frontages shall have a minimum of one layer of evergreen shrubs. If space allows, provide deciduous shrubs, grasses, perennials, and groundcovers.
- B. Spacing: Space plants based on their mature size to avoid overcrowding.
- C. Grouping: Group plants with similar sun, water, and soil needs.

## 03. SOIL PREPARATION

- A. Soil Quality: Amend soil for good drainage and nutrients.
- B. Depth: Ensure soil depth for root growth.
- C. Drainage: Use solutions for proper drainage.





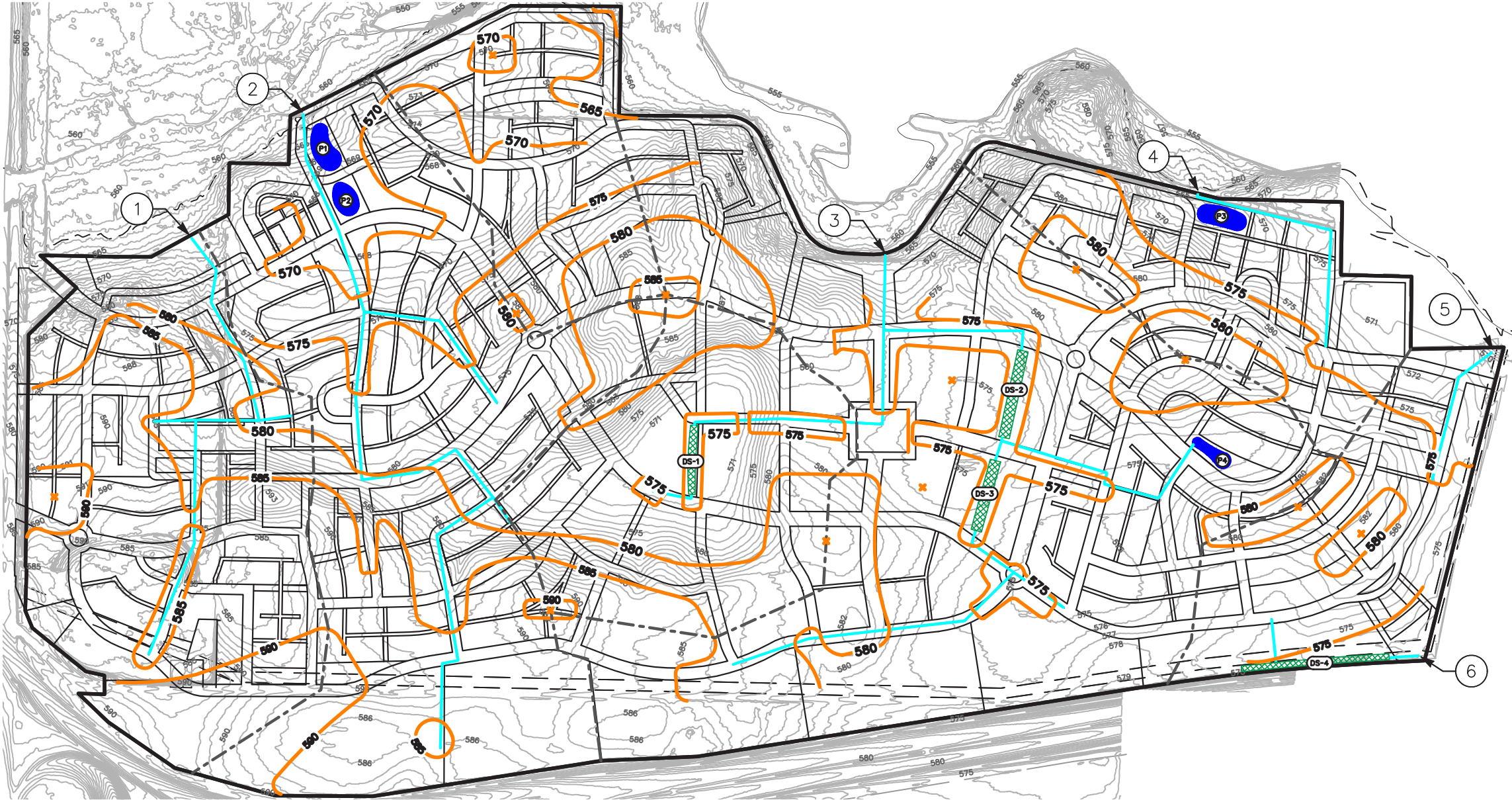
# CIVIL CONCEPT ANALYSIS

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CONCEPTUAL MASS GRADING & STORM OUTFALL	128
CONCEPTUAL SANITARY SEWER	129
CONCEPTUAL UTILITY - WATER	130



# CONCEPTUAL MASS GRADING & STORM OUTFALL

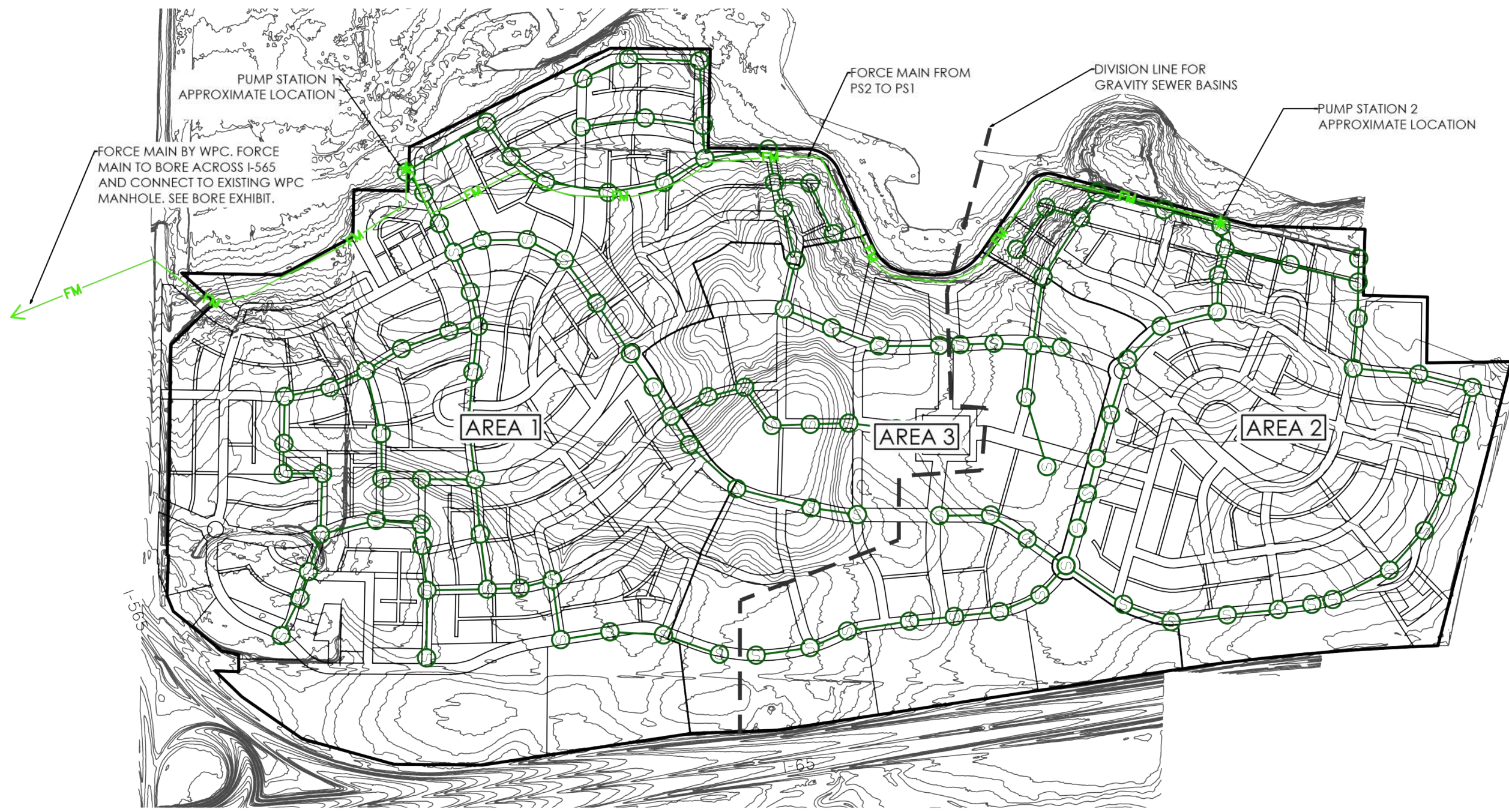


**01. NOTE:**

A. This is only to demonstrate that the site can drain and does not represent the final design. The team will work with the City of Huntsville on a phase-by-phase approach.



# CONCEPTUAL SANITARY SEWER

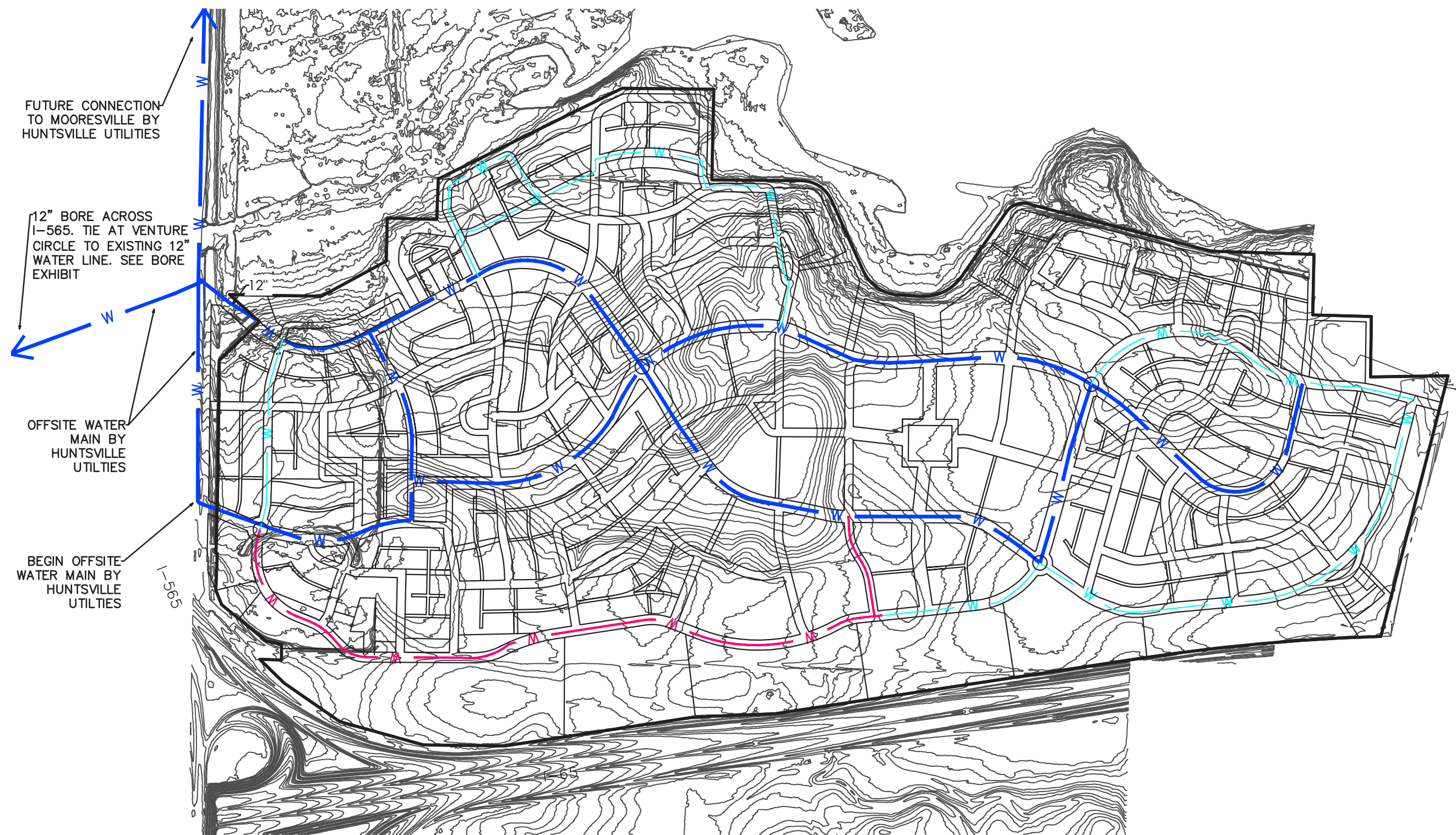


**01. NOTE:**

A. This is a conceptual layout to identify potential locations of sewer mains, manholes, and lift stations. A final design will be reviewed by the City of Huntsville as the project is developed.



# CONCEPTUAL UTILITY - WATER



**01. NOTE:**

A. This is a conceptual layout to identify potential locations of the site water mains. A final design will be reviewed by the City of Huntsville as the project is developed.



# RESOURCES

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WESTMOORE LANDING CODE	133
DEFINITIONS	134
CODE MODIFICATIONS	135



# WESTMOORE LANDING CODE

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**01. INTENT**

- A. As part of the Westmoore Landing form-based code the following definitions, transect descriptions, and modification requests are provided as supplementary regulations for each transect community type (T3, T4, and T5). Modifications to the underlying regulations as articulated in the City of Huntsville Zoning Ordinance and City of Huntsville Subdivision Regulations are identified and articulated within these definitions, transect descriptions, and modification requests.

**02. DEFINITIONS**

- A. See page 134 for additional modification clarification.

**03. TRANSECT DESCRIPTIONS**

- A. T3 Neighborhood (T3 - N)
- B. T3 Hamlet (T3 - H)
- C. T4 Neighborhood (T4 - N)
- D. T4 Village (T4 - V)
- E. T5 Neighborhood (T5 - N)
- F. T5 Town Center (T5 - TC)

**04. CODE MODIFICATION REQUESTS**

- A. See page 135 for zoning ordinance and subdivision regulations modification clarifications.

**05. LAND USE INTENSITY**

- A. The Gross Land Area for Westmoore Landing PD-H recognizes that mixed-use areas (T3-Hamlet, T4-Village, and T5-Town Center) are inherently different than the housing or residential areas of the project, similar to the TND philosophy of having more intentional and meaningful programmed and unprogrammed open space.

**06. FIRE ACCESS**

- A. Where the aerial apparatus staging area is not provided in front of a building on-street, it may be provided off the alley in the rear of the property, subject to City of Huntsville Engineering and Fire Department approval.

# DEFINITIONS

## ACCESSIBLE UNITS

A dwelling or facility designed to provide full, uncompromising access and usability for people with disabilities from the point of first occupancy.

## BEST MANAGEMENT PRACTICES (BMPS)

Effective techniques or methods used to prevent or reduce environmental impacts, particularly related to stormwater runoff, erosion control, and water quality management. BMPs can include a wide range of physical solutions to accomplish these goals; such as detention ponds, vegetated swales, and filtration systems.

## BUILDING HEIGHT

The vertical extent of a building as measured in stories, building elements including chimneys or flues, elevator or stair bulkheads, roof water tanks, or cooling towers, flagpoles, parapet walls not more than four feet high, ornamental towers, spires or belfries having no floor area within that portion of the tower shall be exempt from meeting the maximum height.

## BUILD-TO-ZONE

An area established within a given lot indicating where the building frontage must be located. Depending on particular circumstances, the build-to zone may be measured from the property line, the back of an easement, R.O.W. edge, or the back of the sidewalk.

## CIVIC

A general term defining services dedicated to the public, typically including culture, education, recreation, religion, government, transit, and municipal uses.

## COMMERCIAL

A use intended to collectively define non-residential uses not limited to office, retail, food establishments, entertainment and hospitality establishments.

## COMMERCIAL BUILDING TYPE

A structure that integrates one or more permissible commercial uses per the PD-H document. Commercial buildings shall follow the regulations found in the Building Types - Commercial for each Transect Community Type.

## DIAGRAM(S)

Similar to Framework, Concept, Images, Plans, Floor Plans, Layout, etc.; are terms used to describe various elements throughout the Westmoore Landing PD-H document intended to illustrating and/or visualize thoughts, phrases, and/or words. These illustrations do not represent a final solution. These are representative by nature and subject to change.

## ELDERLY HOUSING

Residential developments designed for people 55 and older, offering features that accommodate aging-related needs such as accessibility, safety, and social/community support.

## FURNISHING ZONE

A designated portion of the public right-of-way, typically located between the edge of the travel or parking lanes and the sidewalk or edge of right-of-way for alleys, used to accommodate street furniture, trees, utilities, signage, lighting, bike racks, and other streetscape elements.

## GAME ROOM

A commercial or accessory space designed or used for indoor recreational purposes involving games of skill or chance, table tennis, arcade games, pinball, or similar amusement.

## HELISTOP

A minimal landing and takeoff area for a variety of potential manned and unmanned services intended only for pick-up or drop-off with no prolonged stay.

## JULIETTE BALCONY

A shallow, non-occupiable exterior architectural feature attached to the facade of a building, consisting of a railing or guard installed in front of a door or window. A Juliette Balcony may project no more than 18 inches from the building wall. Juliette balconies are not subject to frontage type requirements.

## LIVE-WORK

A unit which contains separate residential and non-residential areas intended for both residential and non-residential uses in either commercial or residentially zoned areas.

## LOW IMPACT DESIGN (LID)

An environmentally sensitive approach to land development and stormwater management that aims to minimize disturbances to the natural landscape. It emphasizes preserving natural drainage patterns, reducing impervious surfaces, and integrating small-scale solutions—such as permeable pavements, rain gardens, and biofiltration—to effectively manage runoff, enhance water quality, and support healthier ecosystems.

## MEZZANINE

An intermediate, partial floor that projects between 2 floors of a building. Mezzanine floors that cover more than 65% of the story below shall be counted as a full story.

## MICRO RETAIL

A retail model that includes small-scale pop-up shops and boutique storefronts that use innovative, downsized activities.

## MIXED-USE

A combination of two or more of the following uses: residential, office, retail, institutional, light industrial, recreational, and other miscellaneous uses. Mixed-use may be applied horizontally or vertically to a building, lot, and/or transect.

## MOTORCOURT

A front entry driveway providing access and maneuverability space between the driveway and primary structure.

## PASEO

A pedestrian-oriented walkway or pathway, typically landscaped and designed to provide connectivity and recreational space within neighborhoods or developments. Paseos are intended to enhance community interaction, facilitate easy pedestrian access, and integrate open space elements with surrounding buildings and natural environments. When a paseo is used to provide public access to a lot, the public access easement shall be recorded on the final plat and specifically designated as a public access easement.

## TEMPORARY USES

An activity or event occurring at a specific location for a limited and defined duration, which is planned or reasonably anticipated to attract gatherings of people, and is not otherwise classified within this Ordinance. Examples include but are not limited to farmers markets, pop-up markets, street fairs, and similar short-term events.

## TOPOGRAPHICALLY CHALLENGED UNITS

Units where existing or design grade does not allow for entitlement standards to be met without undue hardship.

## TRADITIONAL NEIGHBORHOOD DEVELOPMENT (TND)

A design technique that replicates historic development patterns found in American towns and applies their basic elements to new development projects. TND includes compact, pedestrian friendly development with a mix of land uses (detached, attached, mixed use, etc.) in a town setting.

## VISITABLE UNITS

A dwelling or facility designed with minimal accessibility features designed for ease of future adaptation for accessibility, and/or those visiting a dwelling or facility.



# CODE MODIFICATIONS

## 01. SUBDIVISION REGULATION MODIFICATIONS

- A. Street cross sections are in accordance with the Street Sections (pages 21 - 27) per the PD-H submittal packet. Where the aerial apparatus staging area is not provided on-street, it may be provided off the alley in the rear of the property.
- B. Streets with ribbon curb shall be allowed in T3 Neighborhood & T3 Hamlet as identified within the Westmoore Landing PD-H street sections as an alternative.
- C. Mountable, Header (ribbon) and/or stand up (post) curbs shall be allowed where streets abut parks, common open space, and/or alleys in all Transects and may be used when design conditions permit.
- D. Alleys as shown in the Westmoore Landing PD-H street sections may have an inverse crown, ribbon curbing, and a 5 ft. furnishing zone beyond the back of curb if public. Final layout and design will be developed during the construction plan phase.
- E. Modification of minimum design standards.
  - 01. Minimum pavement widths, including travel lanes and on-street parking vary and are prescribed in the Street Sections. Minimum pavement widths to include travel lanes and on-street parking when applicable.
  - 02. Minimum R.O.W. vary and are prescribed in the Street Sections.
- F. Pedestrian ways and paseos shall provide access and will be labeled on the final plat. These private easements for ingress, egress, and public access will exclude any portion of a lot, but can include utility easements. All paseos, whether they provide private or public access, will be maintained by the Westmoore Landing Owners Association.
- G. Intersections shall be allowed to have 15' radii, as long as the route isn't required to serve fire apparatus, that encourage traffic to slow while encouraging pedestrian safety and traffic calming.
- H. Raised tables using different materials (brick, stamped concrete, etc.), brick crosswalks, brick / paver sidewalks, will be permitted within the public R.O.W. in T3 - HC, T4 - VC, and T5 - TC for the purpose of promoting and encouraging traffic calming and enhanced pedestrian safety, with final design to be reviewed and approved by Huntsville Engineering and Fire Marshal.
- I. Non-radial side lot lines are permitted without the necessity of approval by the Planning Commission.
- J. Traffic Control Devices, street lights, and street name signs may deviate from Huntsville standards as long as the Westmoore Landing Owner's Association maintains such and, if applicable,

MUTCD standards must be followed, otherwise, the City of Huntsville will replace with standard materials.

- K. Public utility, public drainage, and private easements for ingress, egress, and public access will serve as both access and utility corridors to lot frontages facing common open space / paseos within the Westmoore Landing PD-H.

## 02. R2B AND C4 MODIFICATIONS

- A. Eliminate the requirement for perimeter landscaping along common lot lines for parking areas where the common lot lines fall within or adjacent to the shared parking areas.
- B. Lot sizes, lot widths, yard setbacks, lot coverage, lot frontage, building setbacks, density, open space requirements, and height regulations are modified to meet those standards as defined for each Transect and Building Type as identified in the Building Type Standards in place of provisions of City of Huntsville Zoning Ordinance and the Subdivision Regulations. Corner lot front yard setback will be determined by the front door location thereby designating the secondary frontage as a side yard.
- C. Multifamily shall follow the Building Type Standards of the Westmoore Landing PD-H, eliminating window view setbacks and density requirements.
- D. Fee simple buildings may be accessed from public streets or public alleys as defined in the Westmoore Landing PD-H Document as long as fire and building codes are met.
- E. Townhouse regulations per City of Huntsville Zoning Ordinance (13.2.3) are modified to meet those standards as defined for the Townhouse in the Building Type Standards. Townhouses may be 3-8 units in a row. Townhouses may have a contiguous front facade. There shall be no min. private open space requirements for Townhouses meeting the 1/4 mile radius of a formal open space type.
- F. Accessory dwelling units: each single family rear loaded, single family front loaded, townhouse, manor house, and live/work lot may contain one accessory dwelling unit in addition to the principal dwelling, with full plumbing, not to exceed 60% of primary building habitable floor area in a separate outbuilding and shall have no minimum street frontage requirements, and no max. lot coverage in the rear yard unless there is no alley, in which a 10' minimum setback must be maintained. Accessory structures shall be subservient in style and massing to the

primary dwelling and follow the bulk and height standards in the Westmoore Landing PD-H document.

- G. Parking Requirements are modified to meet those standards as defined in the Supplementary Design Standards - Parking Requirements of the Westmoore Landing PD-H Submittal Packet in place of provisions of City of Huntsville Zoning Ordinance. Required parking spaces may be reduced in accordance with G.106.05.A. One or more required parking spaces may be located on-street and adjoining the lot frontage for which the space is required, as long as the street is designated as a cross section with the heading "dedicated parking". Parking is permitted in the first 5 ft. of a rear yard when it is accessed from an Alley, and extends at least 20' from alley curb to building or sidewalk. Shared parking is encouraged per Table G.106.05.A. and can be further refined by a professionally prepared parking study.
- H. Permitted land uses are modified to meet those standards as defined for each Transect Community Type in place of provisions of City of Huntsville Zoning Ordinance.
- I. Frontage Types are modified to meet the standards as defined for each Transect and Building Type and identified in the Frontage Type Standards and associated Supplementary Design Standards. Covered porches having a roof, bay windows, and other architectural elements may encroach within the yard setbacks per the Frontage Type Standards.
- J. The Phasing Plan for development shall be waived as part of the PD-H submittal packet.
- K. Streets serving over 50 dwelling units may have vehicular access to off-street parking directly to the street from the sites of individual dwelling units provided the buildings follow the building type standards per transect.
- L. Lot sizes, dimensions, frontage and setbacks shall correspond to the Westmoore Landing PD-H, for all Building Types in order to promote TND design and function.